

Attachments

Council Meeting

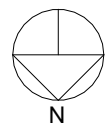
Monday 11 May 2026

| | | |
|-------------|--|-----|
| 10.3 | 3/6 Redland Drive, MITCHAM (Lot 3 PS 417841)– Use the land for the purpose of an indoor recreational facility. | |
| | Attachment 1 WH/2025/881 - 3/6 Redland Drive, MITCHAM VIC 3132 - Plans | 3 |
| 10.4 | Council Owned Land - 37 Bennett Street, Forest Hill & 25 Mountainview Road, Nunawading | |
| | Attachment 1 Community Feedback and Officer Response | 12 |
| 10.6 | Forest Hill Activity Centre (69-79 Mahoneys Road, Forest Hill and adjoining car park) - request for planning scheme amendment | |
| | Attachment 1 Planning Report..... | 30 |
| | Attachment 2 Explanatory Report, instruction sheet, draft DPO1 and DDO12 | 120 |
| | Attachment 3 Traffic Report | 138 |
| 10.7 | Records of Informal Meetings of Councillors | |
| | Attachment 1 Pre Council Meeting Briefing - 20 April 2026... | 177 |
| | Attachment 2 Strategy Meeting - 27 April 2026 | 178 |
| | Attachment 3 Strategy Meeting - 4 May 2026 | 179 |

10.3 **3/6 Redland Drive, MITCHAM (Lot 3 PS 417841)–
Use the land for the purpose of an indoor
recreational facility.**

Attachment 1 WH/2025/881 - 3/6 Redland Drive, MITCHAM VIC
3132 - Plans

10.3 – ATTACHMENT 1. WH/2025/881 - 3/6 Redland Drive, MITCHAM VIC 3132 - Plans



LOCATION PLAN



CANTER TOWN PLANNING
 23 Ellis Street, Frankston VIC 3199
 T+ +61 3 7043 7843
 info@cantertownplanning.com.au
 TURNING PLANS INTO PLACES, DREAMS INTO SPACES

PROJECT ADDRESS:
 6 REDLAND DRIVE, MITCHAM VIC 3132

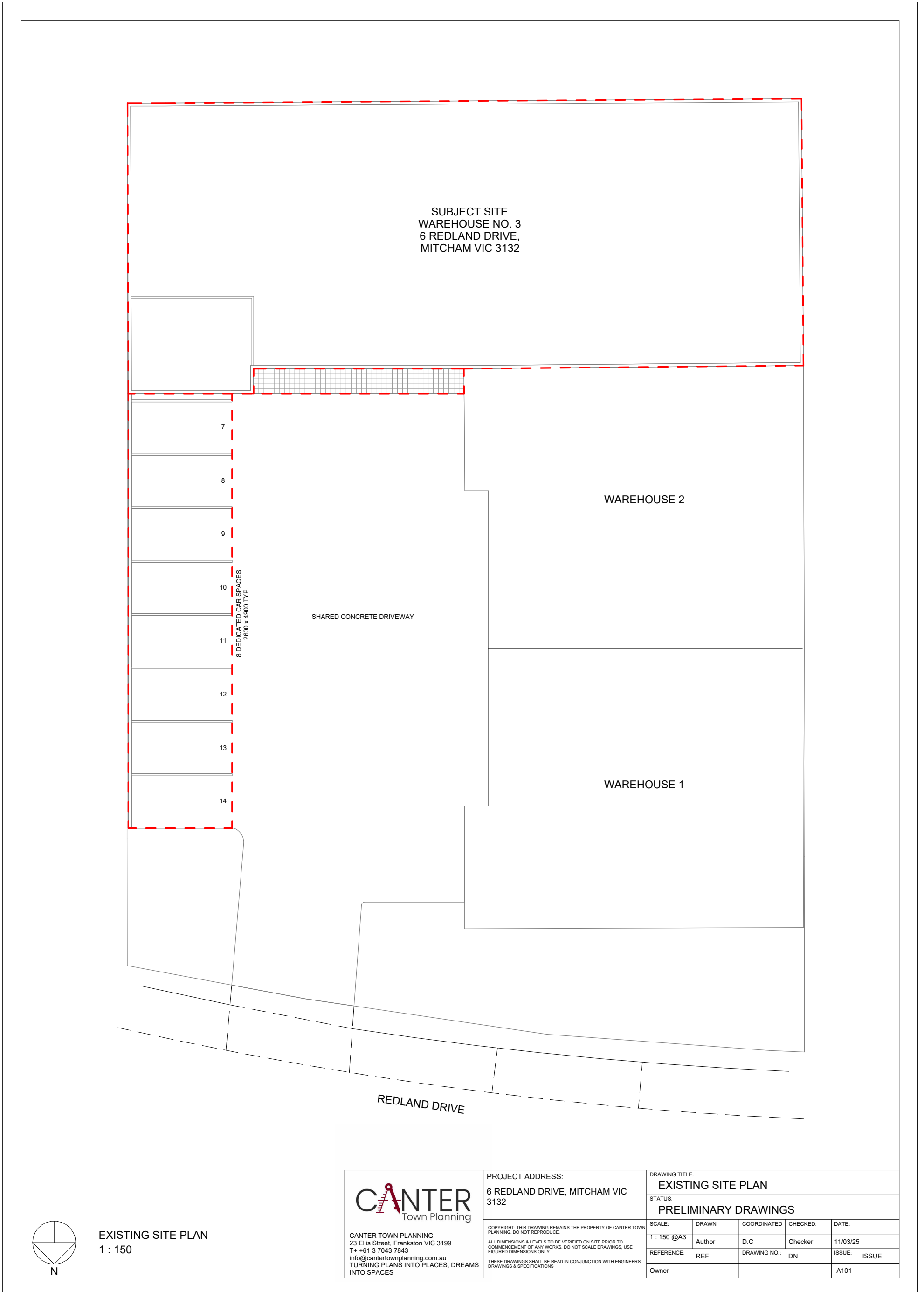
COPYRIGHT: THIS DRAWING REMAINS THE PROPERTY OF CANTER TOWN PLANNING. DO NOT REPRODUCE.
 ALL DIMENSIONS & LEVELS TO BE VERIFIED ON SITE PRIOR TO COMMENCEMENT OF ANY WORKS. DO NOT SCALE DRAWINGS. USE FIGURED DIMENSIONS ONLY.
 THESE DRAWINGS SHALL BE READ IN CONJUNCTION WITH ENGINEERS DRAWINGS & SPECIFICATIONS

DRAWING TITLE:
 LOCATION PLAN

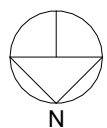
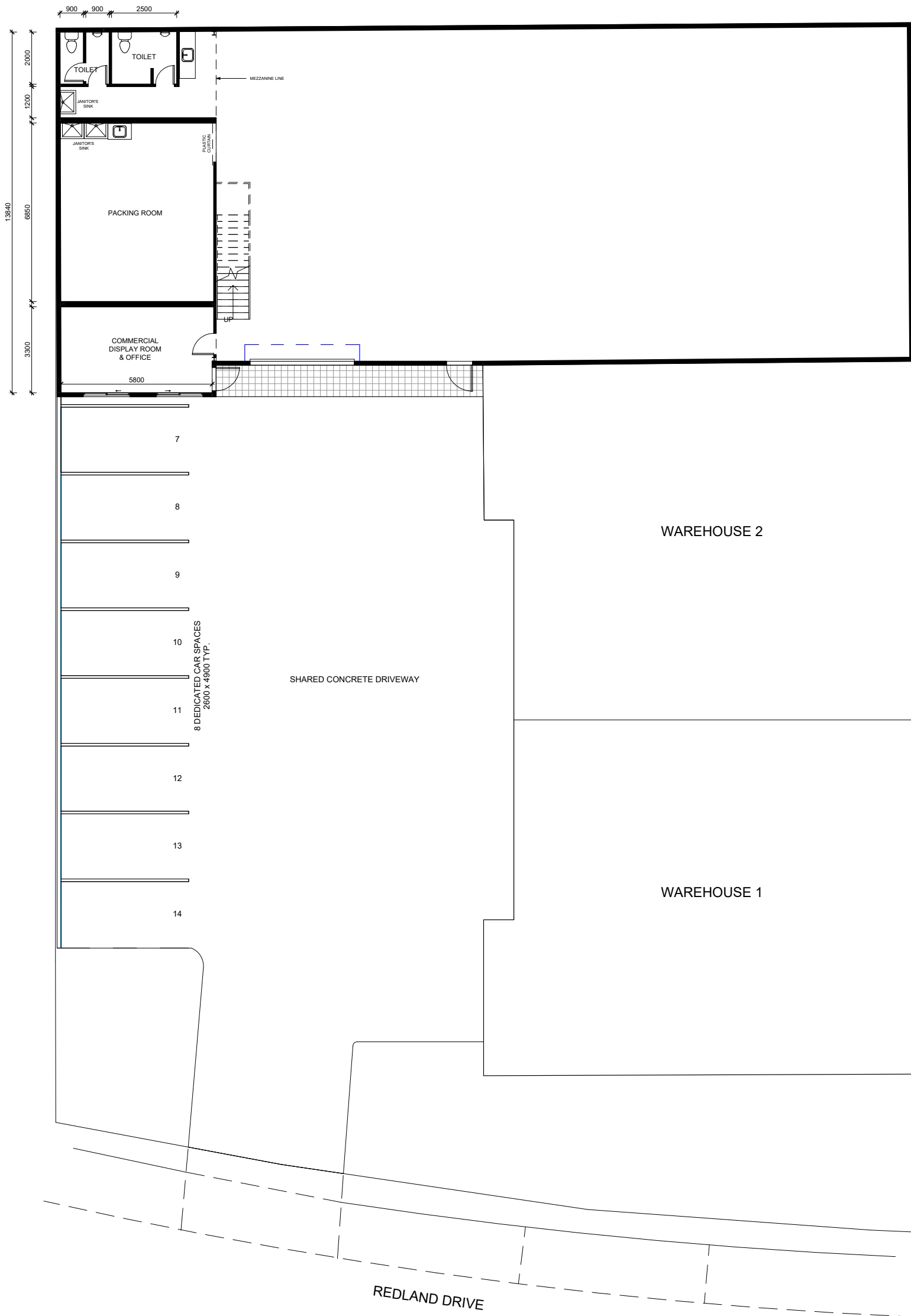
STATUS:
 PRELIMINARY DRAWINGS

| SCALE: | DRAWN: | COORDINATED | CHECKED: | DATE: |
|------------|--------|--------------|----------|--------------|
| @A3 | Author | D.C | Checker | 11/03/25 |
| REFERENCE: | REF | DRAWING NO.: | DN | ISSUE: ISSUE |
| Owner | | | | A100 |

10.3 – ATTACHMENT 1. WH/2025/881 - 3/6 Redland Drive, MITCHAM VIC 3132 - Plans



10.3 – ATTACHMENT 1. WH/2025/881 - 3/6 Redland Drive, MITCHAM VIC 3132 - Plans



EXISTING GROUND FLOOR PLAN
1 : 150



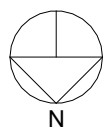
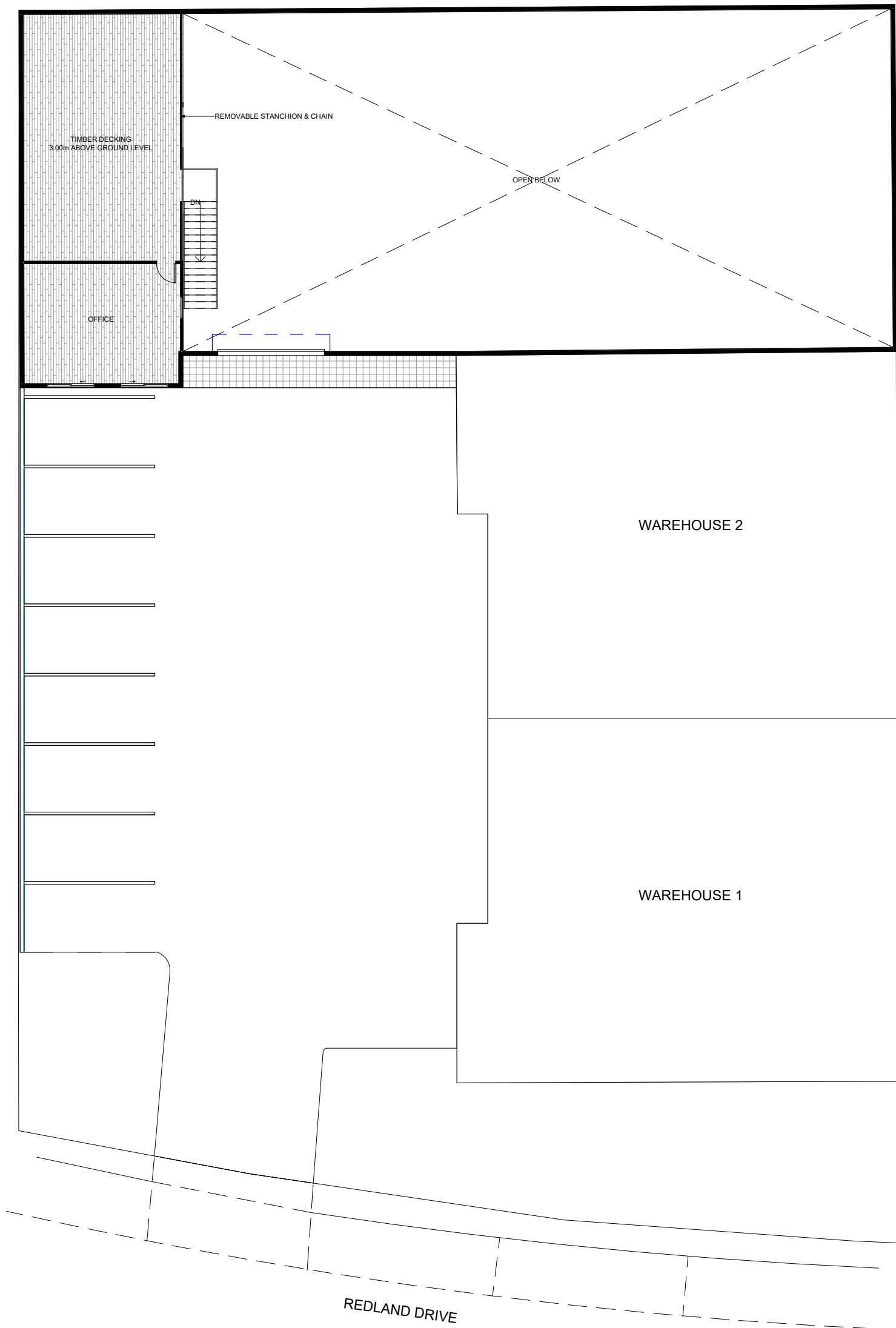
CANTER TOWN PLANNING
23 Ellis Street, Frankston VIC 3199
T+ +61 3 7043 7843
info@cantertownplanning.com.au
TURNING PLANS INTO PLACES, DREAMS INTO SPACES

PROJECT ADDRESS:
6 REDLAND DRIVE, MITCHAM VIC
3132

COPYRIGHT: THIS DRAWING REMAINS THE PROPERTY OF CANTER TOWN PLANNING. DO NOT REPRODUCE.
ALL DIMENSIONS & LEVELS TO BE VERIFIED ON SITE PRIOR TO COMMENCEMENT OF ANY WORKS. DO NOT SCALE DRAWINGS, USE FIGURED DIMENSIONS ONLY.
THESE DRAWINGS SHALL BE READ IN CONJUNCTION WITH ENGINEERS DRAWINGS & SPECIFICATIONS

| | | | | |
|---|------------------|---------------------|---------------------|-------------------|
| DRAWING TITLE: EXISTING GROUND FLOOR PLAN | | | | |
| STATUS: PRELIMINARY DRAWINGS | | | | |
| SCALE: 1 : 150 @A3 | DRAWN: Author | COORDINATED: D.C | CHECKED: Checker | DATE: 11/03/25 |
| REFERENCE: REF | DRAWING NO.: | DN | ISSUE: | ISSUE |
| Owner | | | | A102 |

10.3 – ATTACHMENT 1. WH/2025/881 - 3/6 Redland Drive, MITCHAM VIC 3132 - Plans



EXISTING MEZZANINE PLAN
1 : 150



CANTER TOWN PLANNING
23 Ellis Street, Frankston VIC 3199
T+ +61 3 7043 7843
info@cantertownplanning.com.au
TURNING PLANS INTO PLACES, DREAMS INTO SPACES

PROJECT ADDRESS:
6 REDLAND DRIVE, MITCHAM VIC
3132

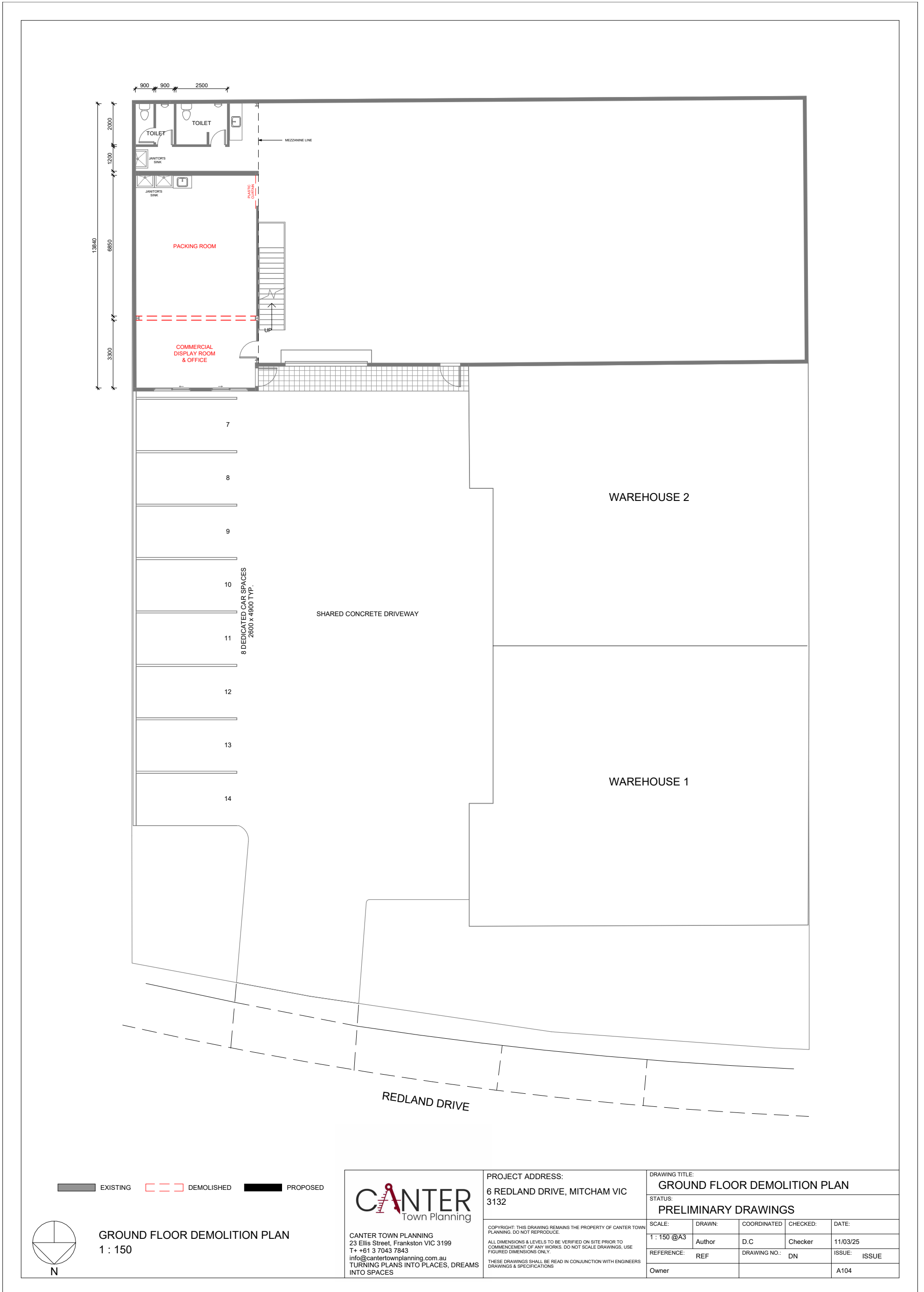
COPYRIGHT: THIS DRAWING REMAINS THE PROPERTY OF CANTER TOWN PLANNING. DO NOT REPRODUCE.
ALL DIMENSIONS & LEVELS TO BE VERIFIED ON SITE PRIOR TO COMMENCEMENT OF ANY WORKS. DO NOT SCALE DRAWINGS. USE FIGURED DIMENSIONS ONLY.
THESE DRAWINGS SHALL BE READ IN CONJUNCTION WITH ENGINEERS DRAWINGS & SPECIFICATIONS

DRAWING TITLE:
EXISTING MEZZANINE PLAN

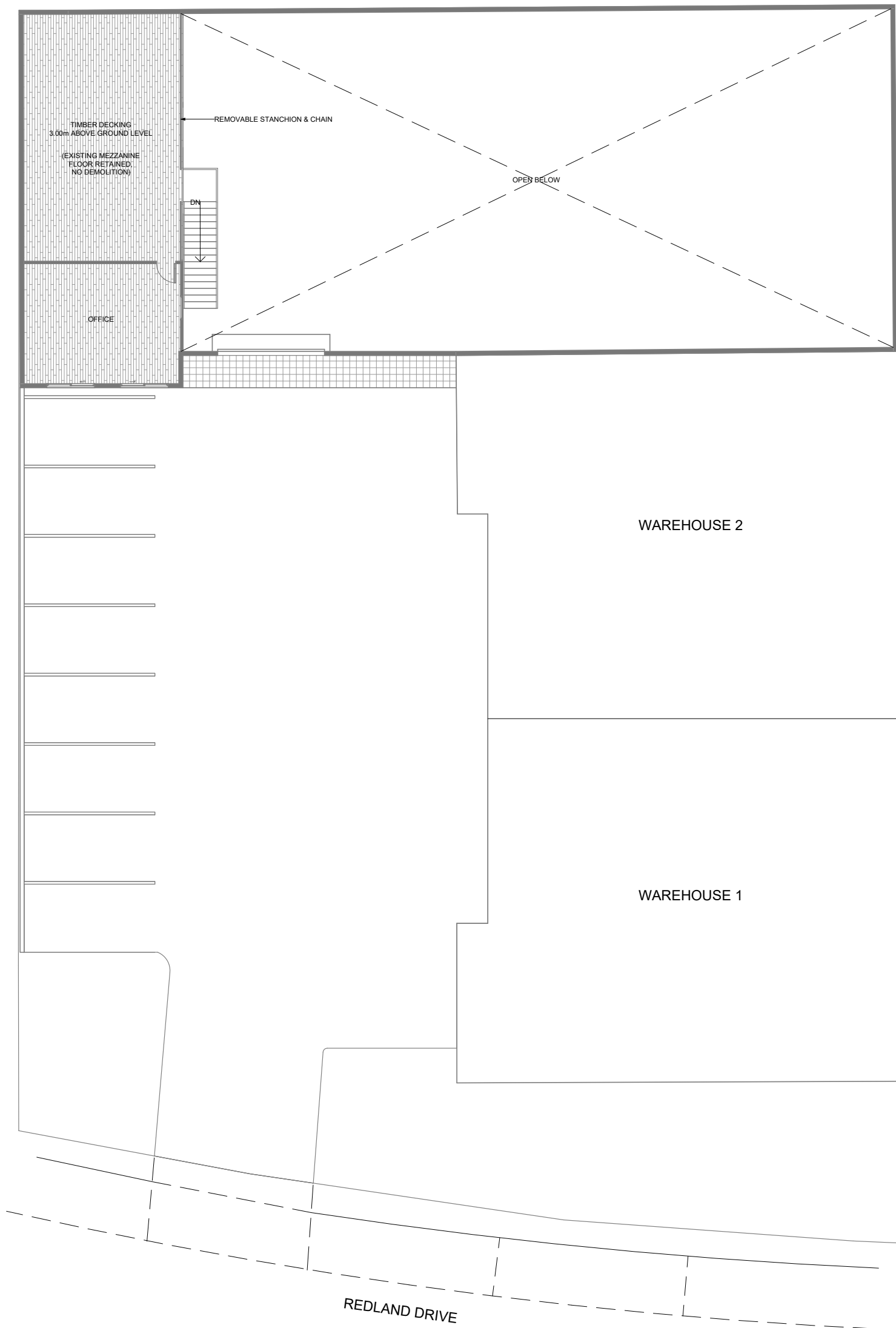
STATUS:
PRELIMINARY DRAWINGS

| SCALE: | DRAWN: | COORDINATED: | CHECKED: | DATE: |
|----------------|-----------------|--------------|----------|----------|
| 1 : 150 @A3 | Author | D.C | Checker | 11/03/25 |
| REFERENCE: REF | DRAWING NO.: DN | | ISSUE: | ISSUE |
| Owner | | | | A103 |

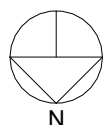
10.3 – ATTACHMENT 1. WH/2025/881 - 3/6 Redland Drive, MITCHAM VIC 3132 - Plans



10.3 – ATTACHMENT 1. WH/2025/881 - 3/6 Redland Drive, MITCHAM VIC 3132 - Plans



EXISTING
 DEMOLISHED
 PROPOSED



MEZZANINE DEMOLITION PLAN
1 : 150



CANTER TOWN PLANNING
 23 Ellis Street, Frankston VIC 3199
 T+ +61 3 7043 7843
 info@cantertownplanning.com.au
 TURNING PLANS INTO PLACES, DREAMS INTO SPACES

PROJECT ADDRESS:
 6 REDLAND DRIVE, MITCHAM VIC
 3132

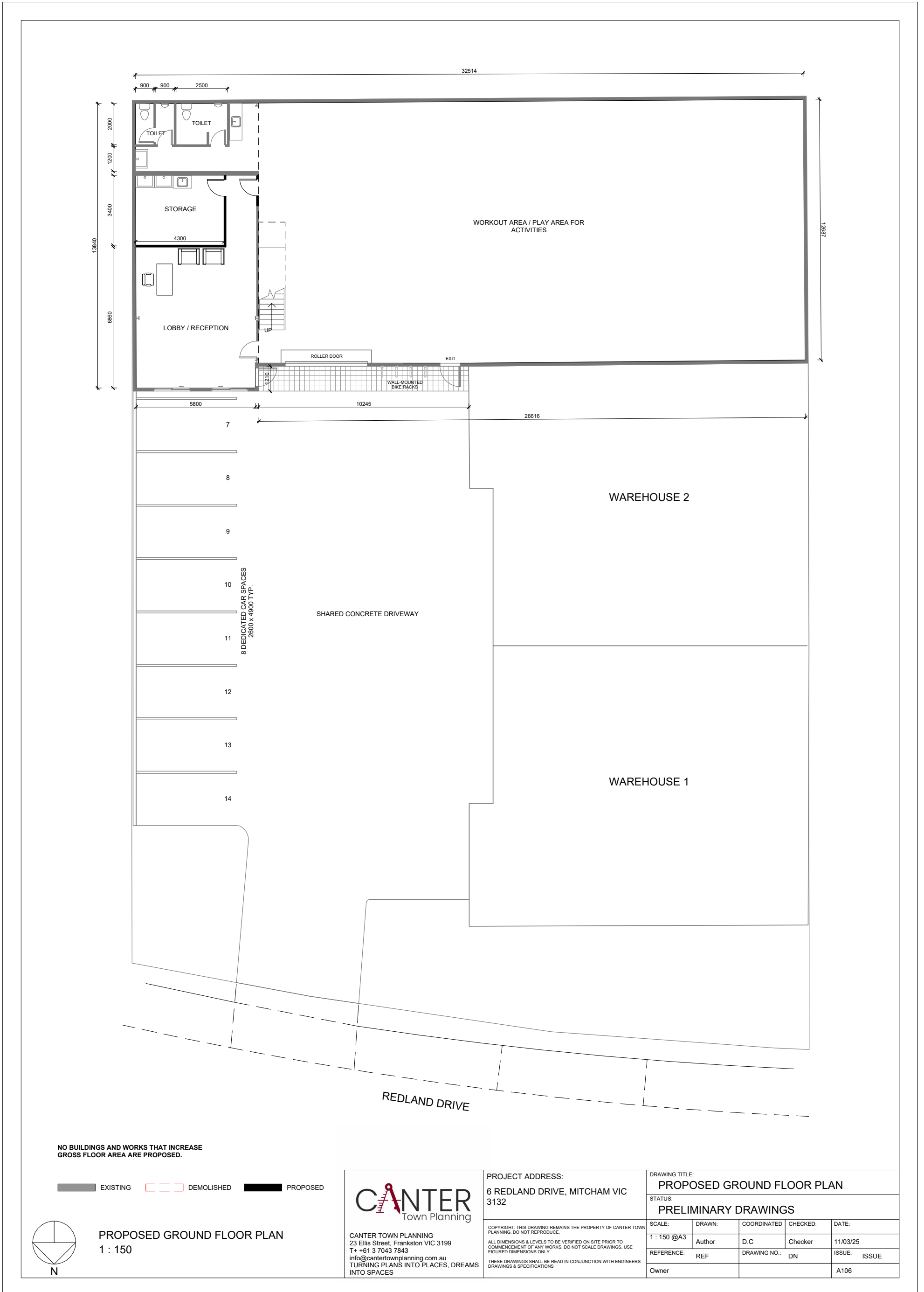
COPYRIGHT: THIS DRAWING REMAINS THE PROPERTY OF CANTER TOWN PLANNING. DO NOT REPRODUCE.
 ALL DIMENSIONS & LEVELS TO BE VERIFIED ON SITE PRIOR TO COMMENCEMENT OF ANY WORKS. DO NOT SCALE DRAWINGS. USE FIGURED DIMENSIONS ONLY.
 THESE DRAWINGS SHALL BE READ IN CONJUNCTION WITH ENGINEERS DRAWINGS & SPECIFICATIONS

DRAWING TITLE:
MEZZANINE DEMOLITION PLAN

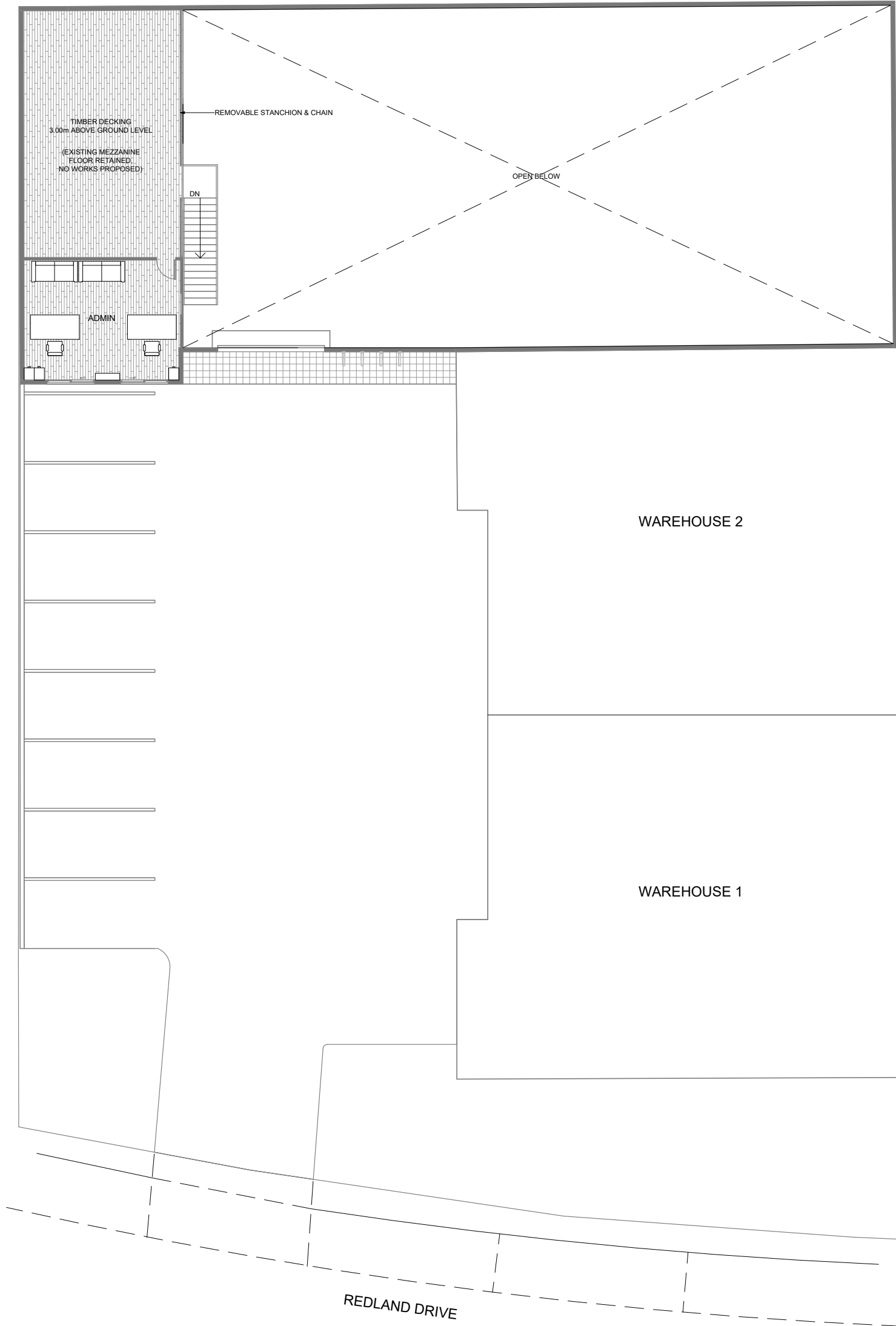
STATUS:
PRELIMINARY DRAWINGS

| SCALE: | DRAWN: | COORDINATED: | CHECKED: | DATE: |
|----------------|-----------------|--------------|----------|----------|
| 1 : 150 @A3 | Author | D.C | Checker | 11/03/25 |
| REFERENCE: REF | DRAWING NO.: DN | | ISSUE: | ISSUE |
| Owner | | | | A105 |

10.3 – ATTACHMENT 1. WH/2025/881 - 3/6 Redland Drive, MITCHAM VIC 3132 - Plans

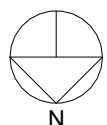


10.3 – ATTACHMENT 1. WH/2025/881 - 3/6 Redland Drive, MITCHAM VIC 3132 - Plans



NO BUILDINGS AND WORKS THAT INCREASE GROSS FLOOR AREA ARE PROPOSED.

EXISTING DEMOLISHED PROPOSED



PROPOSED MEZZANINE PLAN
1 : 150



CANTER TOWN PLANNING
23 Ellis Street, Frankston VIC 3199
T+ +61 3 7043 7843
info@cantertownplanning.com.au
TURNING PLANS INTO PLACES, DREAMS INTO SPACES

PROJECT ADDRESS:
6 REDLAND DRIVE, MITCHAM VIC 3132

COPYRIGHT: THIS DRAWING REMAINS THE PROPERTY OF CANTER TOWN PLANNING. DO NOT REPRODUCE.
ALL DIMENSIONS & LEVELS TO BE VERIFIED ON SITE PRIOR TO COMMENCEMENT OF ANY WORKS. DO NOT SCALE DRAWINGS. USE FIGURED DIMENSIONS ONLY.
THESE DRAWINGS SHALL BE READ IN CONJUNCTION WITH ENGINEERS DRAWINGS & SPECIFICATIONS

DRAWING TITLE:
PROPOSED MEZZANINE PLAN

STATUS:
PRELIMINARY DRAWINGS

| SCALE: | DRAWN: | COORDINATED: | CHECKED: | DATE: |
|----------------|-----------------|--------------|----------|----------|
| 1 : 150 @A3 | Author | D.C | Checker | 11/03/25 |
| REFERENCE: REF | DRAWING NO.: DN | ISSUE: ISSUE | | |
| Owner | | | A107 | |

**10.4 Council Owned Land - 37 Bennett Street, Forest Hill
& 25 Mountainview Road, Nunawading**

Attachment 1 Community Feedback and Officer Response

10.4 – ATTACHMENT 1. Community Feedback and Officer Response**Engagement Results – 25 Mountainview Road**

| | Very supportive | Supportive | Unsure | Unsupportive | Very unsupportive | Total |
|-------------------|-----------------|------------|--------|--------------|-------------------|-------|
| Count | 20 | 9 | 9 | 15 | 61 | 114 |
| Percentage | 17.5% | 7.9% | 7.9% | 13.2% | 53.5% | 100% |

| | Whiteh orse residen t | Work in Whiteh orse | Run a busines s in Whiteh orse | Study in Whiteh orse | Volunte er in Whiteh orse | Particip ate in/belon g to a local club or group in Whiteh orse | Use local parks and walking tracks in Whiteh orse | Use Council sport, recreati on or cultural facilitie s in Whiteh orse | Oth er | Tot al |
|--------------|--------------------------------|---------------------------|--|-------------------------------|------------------------------------|--|---|--|-----------|-----------|
| Count | 98 | 27 | 7 | 5 | 33 | 46 | 44 | 31 | 4 | 295 |
| % | 33.2% | 9.2% | 2.4% | 1.7% | 11.2% | 15.6% | 14.9% | 10.5% | 1.4 % | 100 % |

| Postcode | Suburb | Count | Percentage |
|----------|--|-------|------------|
| 3101 | Kew | 1 | 0.9% |
| 3108 | Doncaster | 1 | 0.9% |
| 3109 | Doncaster East | 3 | 2.6% |
| 3111 | Donvale | 3 | 2.6% |
| 3128 | Box Hill | 4 | 3.5% |
| 3129 | Box Hill North | 4 | 3.5% |
| 3130 | Blackburn/Blackburn North/Blackburn South | 24 | 21.1% |
| 3131 | Nunawading/Forest Hill | 29 | 25.4% |
| 3132 | Mitcham | 9 | 7.9% |
| 3133 | Vermont/Vermont South | 23 | 20.2% |
| 3136 | Croydon | 1 | 0.9% |
| 3151 | Burwood East | 3 | 2.6% |

10.4 – ATTACHMENT 1. Community Feedback and Officer Response

| | | | |
|--------------------|----------------|------------|---------------|
| 3152 | Wantima | 1 | 0.9% |
| 3153 | Bayswater | 1 | 0.9% |
| 3156 | Ferntree Gully | 1 | 0.9% |
| (blank) | | 6 | 5.3% |
| Grand Total | | 114 | 100.0% |

| Verbatim Responses (Personal Identifiers Redacted) | |
|---|--|
| 1 | <p>I am a resident of Mountainview Road and wish to object to the proposed sale of the council-owned property at 25 Mountainview Road, Nunawading. The site is on the edge of the proposed Nunawading activity centre and, if sold, is likely to be targeted for higher-density or high-rise development, which would be inconsistent with the quiet, low-scale character of our street and its small local shops. Although the land is currently zoned Neighbourhood Residential Zone – Schedule 4 (NRZ4), emerging state planning directions suggest these controls could be overridden in future to enable more intensive development, which increases the risk of a substantial change to the streetscape and amenity for existing residents.</p> <p>Our street is currently a quiet environment of low-scale housing with a small cluster of local shops, and a childcare that serve the community and contribute to a village feel. Large-scale or taller built form on this particular site would likely increase traffic and parking pressure, overshadow neighbouring homes and the public realm, and erode the intimate local-shopping character that residents value.</p> |
| 2 | I am concerning whether high levels buildings are |
| 3 | <p>The Vermont Men's Shed must move from their current location, and this property would allow the Shed avoid closure. The Vermont Men's Shed provides an important meeting place and a community point of contact and fellowship for men with common interests in Whitehorse and also provides services to the Whitehorse community. I request that this property be used to assist the Vermont Men's Shed continue with its important Whitehorse community support and services.</p> |
| 4 | <p>I believe that house would be so good as a home for a Men's Shed. A very useful present group has lost its home and this would provide a wonderful alternative. Please consider this as it's brilliant for so many residents.</p> |
| 5 | <p>The Vermont Men's Shed must move from their current location and this property would allow the Shed avoid closure.</p> <p>I request that this property be used to assist the Vermont Men's Shed</p> |
| 6 | <p>This would be a suitable interim accommodation for Vermont Men's shed as it looks to transition to a permanent home following its eviction from the current Uniting Church tenancy</p> |

10.4 – ATTACHMENT 1. Community Feedback and Officer Response

| | |
|----|---|
| 7 | I am very unsupportive of sale of this property at this time given the urgent need for property to support the operation (and prevent the closure of) the Vermont Men's Shed. This Men's Shed engages Whitehorse men in creative and satisfying work which directly benefits Whitehorse community. It may be helpful to Council to explore ways in which Men's Shed may also be capable of cost-effectively supporting any essential building maintenance in the form of building labour. |
| 8 | It would be a great place for a men's shed. The current Vermont men's shed has to vacate their premises on Canterbury Road. Men's sheds are such a great place for men to gather and meet for the sake of their mental health as well as having the facilities to work on projects that are too hard to do at home. Please consider this opportunity as a great service to the community. |
| 9 | Renovate and repurpose for use by community groups who require a meeting space |
| 10 | I understand the Vermont men's shed is looking for a new location to meet. The could be used for the men's shed. The men's shed is a terrific organization to support men who are looking for friendship. there is an epidemic of loneliness in Australia and it would be a shame for the men's shed to have this opportunity taken away from the buildings sale |
| 11 | Please consider the needs of the community before going ahead with a sale. It is very important to have Neighbourhood Houses for the wellbeing of residents in the Whitehorse area. As our area becomes more populated, it is important to have a space like this for groups to enjoy. |
| 12 | The Vermont Mens Shed has been operating for 13 years providing an amazing support for men of all ages and serving the community through building equipment and possum boxes. It has to move from its' current location at 640 Canterbury Rd by 30 June, 2026. This property would be suitable for the shed and ensure that it does not need to close. |
| 13 | The Vermont Men's Shed is being forced to move from June next year. There are about eighty men who regularly attend the Shed providing them with fellowship, mental stimulation, a feeling of being wanted and overall contentment. This property would be very good for the Shed to continue its vital role and outreach to the men of Whitehorse. There is also the possibility of the men taking part in a restoration program on the property. |
| 14 | The Vermont Men's Shed serves the community by supporting both the mental and physical wellbeing of its participants - particularly its older and more lonely members who are 'at risk'. In return, the broader community (eg. kindergartens, schools, clubs etc.) benefits by being the recipients of the output of various projects undertaken by the Shed. The Shed must move from its current location and this property would allow the Shed |

10.4 – ATTACHMENT 1. Community Feedback and Officer Response

| | |
|----|--|
| | <p>to avoid closure.</p> <p>I request that this property be used to assist the Vermont Mens Shed</p> |
| 15 | Vermont men's shed needs a new venue |
| 16 | The men's shed need more space and time to be able to relocate. |
| 17 | <p>I am a current member at the Vermont Men's Shed where i attend three times per week.</p> <p>My involvement at the men's shed is very important to me as my wife of 55+ years passed away in January 2024 and since her passing, i have been actively attending the men's shed which keeps me connected to my local community as well as being involved in crafting items for the local community.</p> <p>The site where the men's she currently operates out of, is owned by the Uniting Church on Canterbury Road in Vermont. This site is being sold and we have been told we need to vacate the premises.</p> <p>The proposed site of the new Vermont Men's Shed on Morack Road has not yet even commenced construction and we are seeking to find an alternate location to operate the men's she out of and we believe that this site in Nunawading will provide the location we need.</p> <p>We therefore request that the sale of the building is prolonged in order for the men's shed to use until the site on Morack Road is ready as you will be displacing many men who enjoy participating in activities with other likeminded men which keeps our minds and bodies healthy and moving.</p> <p>I strongly believe that if we were to not be able to set up our shed at an interim location, the men involved will become withdrawn from the community and stop doing the things they find meaningful to continue to fulfil our lives.</p> |
| 18 | <p>The Vermont Men's Shed must move from their current location, due to proposed redevelopment of their current site. Making the use of this council property would allow the shed to avoid closure.</p> <p>I request that this property be used to assist the Vermont Men's Shed, which is a well run resource providing much needed support to men's mental health.</p> |
| 19 | <p>I appreciate that there is always a tension between budget issues and holding assets but if the land is sold then in reality it will never be replaced later as a resource for the community. I am a 65 year old man who has been an office worker who has lived in the Blackburn North area for 40 years and worked in a high stress environment all my working life and I have a very limited range of friends other than through the Churches I have attended in Whitehorse area. I have an uncle whose life was transformed by attending a men's shed in Lorne and I would hope that I can have the same opportunity.</p> |
| 20 | <p>The Vermont Men's Shed must move from their current location and this property would be a marvellous location to move too. The Vermont Men's Shed has to find somewhere and this house is ideal. It will secure the future for The Vermont Men's</p> |

10.4 – ATTACHMENT 1. Community Feedback and Officer Response

| | |
|----|---|
| | Shed to continue to provide for its members and I also believe there are advantages for council to be seen supporting them. Thank you. [Name and address redacted] |
| 21 | The site would provide temporary premises for Vermont Men's Shed and assist it to continue operating |
| 22 | Forest hill woodturners needs a new home. Currently bursting st seams of existing venue |
| 23 | Vermont men's shed are looking for a new premises. They are a great place for men's mental health. |
| 24 | We Seniors who reside in Whitehorse need a place where we could get together for our mental state. At present I belong to the Forest Hill Wood turning Club, and the place is not big enough to accommodate all the turners. So I wish and hope that we can relocate to a much better premise. Hope you'll can consider it. Thanks |
| 25 | The Vermont Men's Shed must move from their current location, and this property would allow the Shed to avoid closure. I request that this property be used to assist the Vermont Men's Shed. I have been a resident in the Whitehorse area for 30 years and a foundation member of Vermont South Men's Shed. As a disabled person I have found the Men's Shed community an integral part of my life. It is a place I feel valued and welcome and would be devastated if it had to close. |
| 26 | With the number of community groups crying out for space and the scarcity of suitable sites in this area I feel it would be remiss of the council to sell off this property. The Vermont Men's Shed are in dire need of a space given the sale of their current premises. Forest Hill Woodturners & Community Shed has for years been asking the council for a safer and more appropriate site than the current premises on Richmond St. |
| 27 | The Vermont Men's Shed needs to move from its present location and is facing closure. This property would allow the Shed to continue and provide a great service to the local men of the area, |
| 28 | I am a resident of Whitehorse City Council, and also a Member of the Vermont Mens Shed. This organisation is vital to many retired and capable Men (mostly well skilled) and allowing them to pass on their knowledge to others. Unfortunately, we have to move out of our present premises, and are looking for a New home in the area. We would like your consideration of our request. [Name redacted] |
| 29 | The Vermont Men's Shed must move from current location due to sale of property. This house would provide interim accommodation until funding becomes available to |

10.4 – ATTACHMENT 1. Community Feedback and Officer Response

| | |
|----|--|
| | <p>build a purpose built building thus not allow the Shed to close. The Shed provides support for men and the community</p> |
| 30 | <p>Yes let the men's shed use the property. Vermont.</p> |
| 31 | <p>The Vermont Men's Shed must move from their current location and this property would allow the Shed avoid closure. I request that this property be used to assist the Vermont Men's Shed</p> |
| 32 | <p>As a long term resident I feel that there is a need for property like this could be used for other local projects like the mens shed that needs a new location as the uniting church wants to sell the present property</p> |
| 33 | <p>Ideal location for Vermont man's shed</p> |
| 34 | <p>The Vermont mens shed must move from their current location in the very near future. This property would be ideal and would possibly prevent the closure of the mens shed</p> |
| 35 | <p>For the Vermont Men's Shed to avoid closure at their current location this property may be of use. I request that this property be used to assist the Vermont Men's Shed.</p> |
| 36 | <p>The Vermont Men's Shed must move from their current location and this property would allow the Shed to avoid closure. I request that this property be used to assist the Vermont Men's Shed. Please help the Vermont Men's Shed to continue its important role as a place of welcome, support and community connection . Thank you [Name redacted]</p> |
| 37 | <p>I believe that this property could be used by a community group within the Whitehorse area. The Vermont Men's Shed is a group that I am involved with and is desperate for a new home within the Whitehorse area. Our existing home is to be sold and we have to vacate the site by mid year. If we have to close, there would be approximately 80 men with out a place to meet with men of a common interest and enjoy their company and companionship. This property would give the shed a home and a sanctuary for men in the Whitehorse area.</p> |
| 38 | <p>It would be great if this property could be used to support Community and volunteer organisations that need to relocate from their current premises rather than being sold.</p> |
| 39 | <p>The Vermont Men's Shed has to move from its present location and is in urgent need of a new home. This property would be suitable.</p> |
| 40 | <p>I am a long term resident of Whitehorse, before my retirement also owned a business in Whitehorse employing 15 persons. I have been an active member of 3 clubs and associations in Whitehorse for more than 20 years plus a member of a Parkland Committee for 40 years.</p> |

10.4 – ATTACHMENT 1. Community Feedback and Officer Response

| | |
|----|---|
| | <p>My request today is to support the Vermont Men's Shed which is about to close due to the sale of our current buildings.</p> <p>Vermont Men's Shed is the only one in Whitehorse and is vital for men's health and well being.</p> <p>Long term we hope to have our own building but we urgently need 25 Mountainview Road Nunawading to provide a home for the Vermont Men's Shed in the short to medium term. Its location is ideal for the majority of members.</p> <p>I implore you to positively consider the needs of Vermont Men's Shed.</p> |
| 41 | <p>The Vermont Men's Shed must move from their current location and this property would allow the Shed avoid closure.</p> <p>I request that this property be used to assist the Vermont Men's Shed</p> |
| 42 | <p>Blackburn North Neighbourhood House is searching for additional activity room space. We would appreciate being able to view the Cottage to see if this space might meet our needs.</p> |
| 43 | <p>Should be used by other community groups, eg neighbourhood houses, U3A as additional space</p> |
| 44 | <p>Given the current great need for social housing, I feel that this site could better serve the local population than be in the hands of a private owner - perhaps as share housing, an emergency shelter, or a domestic violence refuge.</p> |
| 45 | <p>I write to you on the behalf of the Vermont Men's Shed. The Vermont Men's shed has enjoyed the facilities at the United Church Vermont for many years but the property is about to be sold which means that the Shed has to find an alternative site. The church has been very generous in the past but the time has come. And we have to move by June 2026. At this stage we have a partner that has made available to us a section of land in Vermont for us to build a new shed. The problem is that we do not have the resources to build a new shed. Our applications for grants have not had any success and that leaves us without a home for approximately 80 members of the shed. We are also working with the Forest Hill Wood Turners who have 40 members also are in need of a new home.</p> <p>This means that approximately 120 men will be without a place of interest and sanctuary. We need your help to help us make available to all the members of the organizations, a safe and viable place for them to meet and socialize with people of a common interest or a place to just meet and enjoy the company of others. The clock is ticking and June 2026 is coming very quickly which means that action needs to happen very soon if we are to have a place to call home. Any help that you are able to give us will be gratefully appreciated and acknowledged by all concerned.</p> <p>We would like to see these properties used for support the Vermont Men's Shed and Forest Hill Wood Turners so that we can continue approx 120 local residents who use our services for supporting mental health.</p> |

10.4 – ATTACHMENT 1. Community Feedback and Officer Response

| | |
|----|--|
| | [Name and address redacted] |
| 46 | Like as has been applied to 65 Esdale Street, Council must put a Heritage Overlay on the building on this property before selling it so that this original local architecture is not demolished or destroyed, but thoughtfully retained for future generations. |
| 47 | <p>I have been involved with Whitehorse Council properties since I was president of the committee of the Box Hill South Neighbourhood in the 1970' & 80's. My wife worked at the Neighbourhood House for almost 25 years running the creche.</p> <p>Since 2017 and until recently I have been on the management committee of the Forest Hill Woodturners and Community Shed which uses rooms in Richmond St Blackburn South. So, I have a good understanding of the competition for community resources and facilities the council must deal with.</p> <p>During my time on the committee and as Secretary for over 4 years here at the Forest Hill Woodturners I have on many occasions communicated with Council asking (almost pleading) for them to consider new premises for our group. We have used this facility since the mid 1990's and appreciate the support with rental provide by Council. But to now see two very good properties proposed for sale when there is such competition for premises seems incongruous.</p> <p>Our group is more than woodturning, as a community Shed, we support a small membership of male and female members of all ages from 18 to 94. We want to expand our offering and have an appetite to share and make fuller use of a suitable facility. The Council already recognise the group's ongoing efforts to provide a welcoming, inclusive environment where members can learn, share skills, and participate in local community activities. Our work supports broader community objectives including lifelong learning, wellbeing, and the preservation of traditional crafts.</p> <p>If council want to sell a property, sell the building and land we currently use (Warrawong Annex) which is infested with termites and at the end of its useful life. The money raised could be partially used to fund renovations of the Mountain View rd. property.</p> <p>In conclusion, as a long-time user of Whitehorse Council facilities and a previous longtime resident of Whitehorse, I do not support the sale when there is so much need within the municipality. The council apparently already has a healthy financial surplus so the assets should be directed back to the community.</p> |
| 48 | The site would be ideal for local hobby groups such as the Forest Hill Wood Turners Club (of which I am treasurer) and the Vermont Mens Shed. Site security and parking facilities are in place and the refurbishing requirements for groups such as ours are less demanding than for use as a senior citizens community facility. Our existing use of Warrawong Community Hall has been limited by the need to clean and pack equipment away each week and restrictions on installing dust management equipment. We understand the building has termite infestation and may need serious future renovations. Our club provides an opportunity for social interaction whilst |

10.4 – ATTACHMENT 1. Community Feedback and Officer Response

| | |
|----|---|
| | sharing a rewarding hobby all of which contribute to mental health stability. 25 Mountview Rd has the potential to provide us and other like minded clubs with long term council supported accommodation and would free up Warrawong Hall for renovation or redevelopment. It would be a pity to waste the Mountview Rd property when it could be re-purposed for community based activities |
| 49 | Couldn't this property be used by local groups, aged or otherwise. Maybe a men's shed or art studio, possibly a gardening groups. Surely when rented out it would be an asset to the community. |
| 50 | Please consider using the land and or building for a facility for the Forest Hill Wood Turners. In order for the artisan group and men's shed to continue it's amazing work and support of its members and the wider community it needs larger facility where equipment can remain in place between use. Members range from teenager through to those in their 90's, is multicultural, is supportive of groups in need via donations of hand made toys, pens etc. They provide tuition to all members and run monthly demonstration open to the public. Your continued support of the Forest Hill Woodturners is important and greatly appreciated. |
| 51 | There is a dearth of suitable spaces for community groups and that it may be a good idea to have 2 or even 3 groups sharing the property so the space is used 6 days a week. This could be a space where different ages, cultures and sexes can mix and cross pollinate. I'm part of a woodturning club in Whitehorse which is a fantastic and inclusive space. We would love to co habit with other clubs in this property. |
| 52 | Repurpose property, so it can be rented out for functions. |
| 53 | I believe there should be direct reinvestment to community services/similar property or service upgrade and the funds not be a windfall to general revenue |
| 54 | Makes sense to sell. |
| 55 | This is a lovely heritage house that will no doubt be bulldozed by a developer if sold. It's a large block. Can some of the land not be sold off and the funds used to renovate the house? |
| 56 | Why can't it be used for low income and/or short stay accommodation? |
| 57 | Has Council considered use of this facility by the Blackburn North Neighbourhood House who need more space? |
| 58 | How much 'significant investment' would be required? I would prefer this historic building remain in council ownership and a plan to renovate it into a new community centre was put forward rather than selling it to a developer who will chuck units on it and sell them for millions a pop. The location is right next to a shopping strip that is in |

10.4 – ATTACHMENT 1. Community Feedback and Officer Response

| | |
|----|---|
| | need of some form of revival. Would it be possible to rezone this plot for commercial use so it could be integrated with the shopping strip and used to host outdoor dining/additional restaurant/shop space? |
| 59 | <p>Selling assets to balance your books is a short term fix and does not look at the long term needs of residents in the municipality. Once Council sells sites there is no opportunity to provide services or open space which is already at a premium. The state government has and is in the middle of wanting to double the population of Whitehorse.</p> <p>The carpark could be turned into a park and then the original home for the area, with your photos showing the house in place in the 1940's retained as part of the heritage of Nunawading.</p> |
| 60 | Given the land size, you could build at least 2 or 3 houses for the community, either homeless accommodation or a service that helps the community. It looks heritage, are you bound by heritage law? If so restore and use as a community house....again |
| 61 | Sell it, let someone else develop it. Put the money to better use. |
| 62 | Yes my husband and I are in desperate need of housing. Why doesn't the council rent them out as a NRAS property? |
| 63 | Once these sites are sold off there is no way the community can afford to have them back and if the council decides to provide proper services for people in the community again, rent would be needed. Council should fix it and then use it for services for vulnerable community members like youth with mental health conditions or disabled residents rather than selling off for a tacky housing development. Spend money on this rather than all that money that went to the golf course which is used by those who don't have chronic youth mental health issues or who don't have a disability |
| 64 | <p>Healthability (previously Whitehorse community health service) are soon to be ousted. The activity centre at Blackburn station may result in buildings such as the council run childcare and Blackburn library being without an appropriate site. Homelessness is growing. And the council can't see any need to retain buildings that could be used to aid with any of these issues?</p> <p>Even if there wasn't an immediate need, giving up property in a growing area where the needs of the community will also continue to grow and services will be harder and harder to come by is extremely short sighted, and not remotely community minded.</p> <p>Turn part of it into a community garden, a community pantry, lease it to an organisation that provides emergency housing, lease it to a community health service, turn it into anything else, other than temporary profit for temporary gain</p> |
| 65 | I think it would be a great venue for Blackburn North Community House to move to or make use of. Although, I have never been inside, it looks ideal with lots of car |

10.4 – ATTACHMENT 1. Community Feedback and Officer Response

| | |
|----|---|
| | parking. I live in the street and have always admired the house. Let the community use it. |
| 66 | Much better to sell this than try to fix it up to run services it's not quite right for. I'm sure there are much better bang for buck ways to invest community money and that's why I support selling. |
| 67 | Loss of community property that could be used to benefit to citizens - community/council land is hard to come by now as so much is sold off to property developers who only care about the bottom dollar. Keep it and use it as a community space for those in need |
| 68 | It's a beautiful location equipped with parking lots, which could become a good coffee/cafe for the community to hang out in with historic Whitehorse Council vibes or mini museum of some-sort. OR it could also turn into a community Early Learning Centre? |
| 69 | Land should be kept for community open space. As density increases having land available for community recreation is critical. The land could support the activation of the stores nearby |
| 70 | This one looks more heritage, perhaps it needs to be sold to continue community based services rather than be redeveloped into a concrete slum. |
| 71 | We need more spaces for community groups to meet, and will need more facilities for community as the population around Nunawading increases with the Activity Centre development. Council should be holding on to all community facilities at this point in time. You are about to get a windfall in developer contributions as part of activity centre development - it doesn't make sense to sell. Why would you sell this now, before the Activity Zones are finalised? Surely the value will increase significantly when the rezoning for the activity centres is finalised. Also - if Council sells residential land, it should ensure that affordable housing that is high quality, sustainable design is built on that land. |
| 72 | Feels like this property has some heritage significance which will be lost if sold. Consider selling the adjoining land and re-purposing the home, back into a home |
| 73 | Surely there is a need for this property within the community? Maybe instead of asking should we sell it, you should be asking could you use this property? It's a stunning property and shouldn't be sold (it will be knocked down and over developed) |
| 74 | Could this be better utilised as a Youth and Family hub? Whitehorse lacks support for families and young people in our community. Have a Youth and family hub would allow the council and local service providers to run program to enhance the lives of many |
| 75 | It will be a shame for another bungalow to be redeveloped, the ambience of our lovely suburbs is being ruined by subdivisions |

10.4 – ATTACHMENT 1. Community Feedback and Officer Response

| | |
|----|--|
| 76 | Managing the needs of community requires agility and responsiveness. Having a fixed asset, returning low financial revenue or capital growth is counter productive to the community needs. The sale attached to a ring fenced disbursement strategy including liquid investment and over expectation return use would achieve wonders, the sale to create a cash grab for wage increases, grants, etc is the worst option. |
| 77 | Councils should be protecting period / heritage homes as part our our history |
| 78 | As a resident and rate payer of Whitehorse City Council, I have a couple of questions: 1. What is the proposed method of sale? For full transparency, the properties should be sold via public auction using the most suitable agent for the suburb/ area the property is situated. This is to avoid conflicts of interest and actual or perceived corrupt activity and transacting by council, council officers and 3rd parties. 2. What will council do with the surplus cash derived from the sale? Council should make an explicit commitment that benefits the broader community. |
| 79 | Excellent location for a pocket park, between 2 busy roads, no need to cross busy road to access the site. it would also add to the declining tree canopy in Whitehorse. Add homes for the wildlife in the area. |

| Key Theme Category | Number | Percentage |
|--|-----------|-------------|
| Alternative Community Uses | 21 | 26% |
| Community Group Accommodation Needs | 42 | 52% |
| Density and Development Concerns | 3 | 4% |
| Heritage Considerations | 6 | 7% |
| Strategic and Financial Considerations | 9 | 11% |
| Total | 81 | 100% |

10.4 – ATTACHMENT 1. Community Feedback and Officer Response**Engagement Results – 37 Bennett Street**

| | Very supportive | Supportive | Unsure | Unsupportive | Very unsupportive | Total |
|-------------------|-----------------|------------|--------|--------------|-------------------|-------|
| Count | 25 | 7 | 7 | 7 | 15 | 61 |
| Percentage | 41.0% | 11.5% | 11.5% | 11.5% | 24.6% | 100% |

| | Whitehorse resident | Work in Whitehorse | Run a business in Whitehorse | Study in Whitehorse | Volunteer in Whitehorse | Participate in / belong to a local club or group in Whitehorse | Use local parks and walking tracks in Whitehorse | Use Council sport, recreation or cultural facilities in Whitehorse | Other | Total |
|--------------|---------------------|--------------------|------------------------------|---------------------|-------------------------|--|--|--|-------|-------|
| Count | 56 | 14 | 6 | 5 | 12 | 14 | 20 | 13 | 2 | 142 |
| % | 39.4% | 9.9% | 4.2% | 3.5% | 8.5% | 9.9% | 14.1% | 9.2% | 1.4% | 100% |

| Postcode | Suburb | Count | Percentage |
|----------|---|-------|------------|
| 3127 | Mont Albert | 1 | 1.6% |
| 3128 | Box Hill | 5 | 8.2% |
| 3129 | Box Hill North | 3 | 4.9% |
| 3130 | Blackburn/Blackburn North/Blackburn South | 17 | 27.9% |
| 3131 | Nunawading/Forest Hill | 11 | 18.0% |
| 3132 | Mitcham | 3 | 4.9% |
| 3133 | Vermont/Vermont South | 8 | 13.1% |
| 3149 | Mount Waverly | 1 | 1.6% |
| 3151 | Burwood East | 1 | 1.6% |
| (blank) | | 11 | 18.0% |

10.4 – ATTACHMENT 1. Community Feedback and Officer Response

| | | | |
|--------------|--|-----------|---------------|
| Total | | 61 | 100.0% |
|--------------|--|-----------|---------------|

| Verbatim Responses (Personal Identifiers Redacted) | |
|---|---|
| 1 | I believe it should be made available for purchase by Australian citizens who will be owner occupiers and first time buyers / young family. Good to achieve market value to invest in cleaning and maintenance of the local area. |
| 2 | Given the current great need for social housing, I feel that this site could better serve the local population than be in the hands of a private owner. |
| 3 | <p>I write to you on the behalf of the Vermont Men’s Shed. The Vermont Men’s shed has enjoyed the facilities at the United Church Vermont for many years but the property is about to be sold which means that the Shed has to find an alternative site. The church has been very generous in the past but the time has come. And we have to move by June 2026. At this stage we have a partner that has made available to us a section of land in Vermont for us to build a new shed. The problem is that we do not have the resources to build a new shed. Our applications for grants have not had any success and that leaves us without a home for approximately 80 members of the shed. We are also working with the Forest Hill Wood Turners who have 40 members also are in need of a new home.</p> <p>This means that approximately 120 men will be without a place of interest and sanctuary. We need your help to help us make available to all the members of the organizations, a safe and viable place for them to meet and socialize with people of a common interest or a place to just meet and enjoy the company of others. The clock is ticking and June 2026 is coming very quickly which means that action needs to happen very soon if we are to have a place to call home. Any help that you are able to give us will be gratefully appreciated and acknowledged by all concerned.</p> <p>We would like to see these properties used for support the Vermont Men's Shed and Forest Hill Wood Turners so that we can continue approx 120 local residents who use our services for supporting mental health.</p> <p>[Name and address redacted]</p> |
| 4 | Please consider using the land and or building for a facility for the Forest Hill Wood Turners. In order for the artisan group and men's shed to continue it's amazing work and support of its members and the wider community it needs larger facility where equipment can remain in place between use. Members range from teenager through to those in their 90's. It promotes diversity and multiculturalism and supports other local groups in need of assistance through donations of hand made toys and hand made items. The group runs monthly demonstrations open to all and all members are provided with tuition. Your continued assistance is important and vital for our community. |
| 5 | There is a dearth of suitable spaces for community groups and that it may be a good idea to have 2 or even 3 groups sharing the property so the space is used 6 days a |

10.4 – ATTACHMENT 1. Community Feedback and Officer Response

| | |
|----|---|
| | <p>week. This could be a space where different ages, cultures and sexes can mix and cross pollinate.</p> <p>I'm part of a woodturning club in Whitehorse which is a fantastic and inclusive space. We would love to co habit with other clubs in this property.</p> |
| 6 | <p>I believe that the Council should consider developing this property solely for communal use. Whitehorse residents should be asked to provide suggestions ONCE the Council decides not to sell. A property sold disappears for ever as an asset depriving community of alternative options. I hope this survey serves as a reminder to Council that they are custodians of residents' needs.</p> |
| 7 | <p>Do not sell, repurpose land for venue that can be rented for functions</p> |
| 8 | <p>I believe there should be direct reinvestment to community services/similar property or service upgrade upgrade and the funds not be a windfall to general revenue</p> |
| 9 | <p>If council owns property it should put it to housing for those in need. Sure, it might be costly, but people are crying out for public accommodation and yet we, as a city, a state and the country are offloading public housing at a rate of knots. I've read recently that we're down to approximately 2% of what we supported in the middle of last century. That's an appalling state of affairs, and surely it's time for us (Council, State and Federal levels of government) to increase the public housing ratio. What better way to start than with vacant properties!</p> <p>We really are a callous and greedy lot, aren't we.</p> |
| 10 | <p>Is there an opportunity the property could continue to be used for emergency if offered to another organisation eg. Whitehorse Churches Care</p> |
| 11 | <p>Why can't it be used for low income and /or short term relief housing?</p> |
| 12 | <p>Several points</p> <ol style="list-style-type: none"> 1. This site was used for crisis housing, why can it not still be used for this purpose during the ongoing housing crisis? 2. If the sale must occur, could it be limited to humanitarian/nonprofit/charitable buyers? 3. Would it be possible to renovate or convert into a community kitchen/space or similar given the proximity to Forest Hill? |
| 13 | <p>If this property have already been for emergency accommodation. Why sell it????</p> <p>When we have unprecedented homelessness and family in crisis.</p> <p>KEEP FOR HOUSING!!!!!!</p> |
| 14 | <p>Yes my husband and I are in desperate need of housing. Why doesn't the council rent them out as a NRAS property?</p> |
| 15 | <p>Accommodation</p> |
| 16 | <p>I'm concerned that this will end up being sold for another hideous mansion in the area and the money will be used for things that benefit only a few. Surely there is a</p> |

10.4 – ATTACHMENT 1. Community Feedback and Officer Response

| | |
|----|--|
| | community group or volunteer group for our youth who could use this place. One of my sons a was badly impacted by covid lockdowns and never got back to school in person. We ended up having to get support through community groups and all of the ones that provided help were outside where my husband and I work and where our family lives. We were supported by Monash and Maroondah initiatives. Whitehorse needs to do more for young people. |
| 17 | <p>Healthability (previously Whitehorse community health service) are soon to be ousted. The activity centre at Blackburn station may result in buildings such as the council run childcare and Blackburn library being without an appropriate site. Homelessness is growing. And the council can't see any need to retain buildings that could be used to aid with any of these issues?</p> <p>Even if there wasn't an immediate need, giving up property in a growing area where the needs of the community will also continue to grow and services will be harder and harder to come by is extremely short sighted, and not remotely community minded. Turn part of it into a community garden, a community pantry, lease it to another organisation that provides emergency housing, lease it to a community health service, turn it into anything else, other than temporary profit for temporary gain.</p> |
| 18 | Good to see council working to make better use of its properties |
| 19 | Let community groups use this for a nominal cost. |
| 20 | Loss of community property that could be used to benefit to citizens - community/council land is hard to come by now as so much is sold off to property developers who only care about thr bottom dollar. Keep it and use it as a community space for those in need |
| 21 | Should not be sold to developers, perhaps should got to a family in need. |
| 22 | I would prefer that the building be knocked down or leased before being sold. Seems silly to sell off assets that aren't currently used when there may be future need. The cost to purchase in the future will be exorbitant. |
| 23 | <p>we need more community facilities in Nunawading and adjacent suburbs to cater for increasing populations as the Activity Centres are developed.</p> <p>We also need facilities for young people - there's very little for them to do outside of organised sport, and hanging arond in shopping centres (privatised space). we also have a housing crisis and need emergency accomodation.</p> <p>As a member of a local community group, there are not enough low cost meeting rooms available for our group to meet. These hosues could be used to provide free meeting space for local groups.</p> |
| 24 | Please consider either turning this into parkland or better still purchasing property in Lyndhurst Cres Box Hill North to give better access to the new bridge to Eram Park. |
| 25 | do not sell for more townhouses. |

10.4 – ATTACHMENT 1. Community Feedback and Officer Response

| | |
|----|---|
| 26 | <p>Pity the council no longer supports people in need, instead wishes to focus on \$\$\$\$.</p> <p>Perhaps think of combining with the Forest Hill Family Centre and providing a great facility to support families.</p> <p>Once land is sold, it is harder and harder to get it back.</p> <p>It is my belief that the short term thinking of this council is pathetic. Start thinking long term and try to attract families into the area. Council could even think of providing space for Headspace, Orange Door etc.</p> |
| 27 | Barns St |
| 28 | If MCHN needs renovation or updates you could temporarily use this property prior to selling. |
| 29 | <p>As a resident and rate payer of Whitehorse City Council, I have a couple of questions:</p> <p>1. What is the proposed method of sale? For full transparency, the properties should be sold via public auction using the most suitable agent for the suburb/ area the property is situated. This is to avoid conflicts of interest and actual or perceived corrupt activity and transacting by council, council officers and 3rd parties.</p> <p>2. What will council do with the surplus cash derived from the sale? Council should make an explicit commitment that benefits the broader community.</p> |
| 30 | Excellent location for pocket park, as council already owns the property would be good for council to use for the good of their residents. |

| Key Theme Category | Number | Percentage |
|---|-----------|-------------|
| Alternative Community Uses | 5 | 17% |
| Community Group Accommodation Needs | 5 | 17% |
| Concerns About Disposal | 7 | 23% |
| Financial Transparency and Reinvestment | 4 | 13% |
| Social Housing and Crisis Accommodation | 8 | 27% |
| Other Comments | 1 | 3% |
| Total | 30 | 100% |

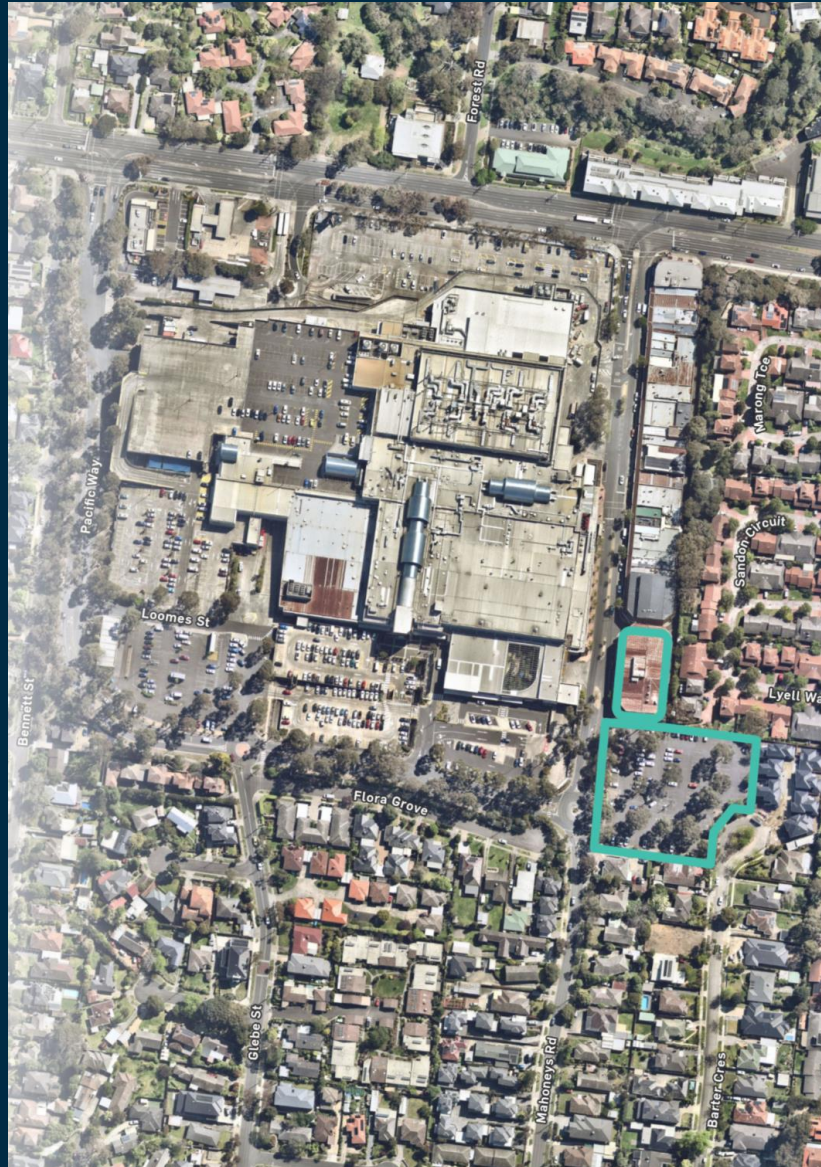
10.6 Forest Hill Activity Centre (69-79 Mahoneys Road, Forest Hill and adjoining car park) - request for planning scheme amendment

Attachment 1 Planning Report

Attachment 2 Explanatory Report, instruction sheet, draft DPO1 and DDO12

Attachment 3 Traffic Report

10.6 – ATTACHMENT 1. Planning Report



Forest Hill Chase

Planning Scheme Amendment

April 2026
Prepared for: Haben

10.6 – ATTACHMENT 1. Planning Report

Acknowledgment of Country



Urbis acknowledges the Traditional Custodians of the lands we operate on.

We recognise that First Nations sovereignty was never ceded and respect First Nations peoples continuing connection to these lands, waterways and ecosystems for over 60,000 years.

We pay our respects to First Nations Elders, past and present.

The river is the symbol of the Dreaming and the journey of life. The circles and lines represent people meeting and connections across time and space. When we are working in different places, we can still be connected and work towards the same goal.

Urbis is committed to incorporating our respect for First Nations cultures, peoples and storytelling in our work across the Country. We are proud to have partnered with Darug Nation artist, **Hayley Pigram**, and to profile her artwork – **Sacred River Dreaming**.

10.6 – ATTACHMENT 1. Planning Report

Urbis staff responsible for this report were:

| | |
|---------------|--------------|
| Director | Laura Thomas |
| Consultant | Sean Michael |
| Project Code | P0043457 |
| Report Number | 02 |

All information supplied to Urbis in order to conduct this research has been treated in the strictest confidence. It shall only be used in this context and shall not be made available to third parties without client authorisation.

Confidential information has been stored securely and data provided by respondents, as well as their identity, has been treated in the strictest confidence and all assurance given to respondents have been and shall be fulfilled.

© Urbis Ltd
50 105 256 228

All Rights Reserved. No material may be reproduced without prior permission. You must read the important disclaimer appearing within the body of this report.

urbis.com.au

Planning Report

Submission documents:

This report is to be read in conjunction with:

- Draft Statutory PSA Documentation including:
 - Explanatory Report
 - Amended Development Plan Overlay Schedule
 - Proposed New Design and Development Overlay Schedule
 - Instruction Sheet
 - Climate Change Considerations Report
- Title Documents
- Concept Architectural Plans prepared by NH Architecture
- Traffic Assessment prepared by Eukai
- Arborist Report prepared by John Patrick Landscape Architects
- Survey Plan prepared by CRA Survey
- Climate Change Supplementary Report prepared by Urbis

10.6 – ATTACHMENT 1. Planning Report

Contents

| | | | |
|---|-----------|---|---|
| Contents | 4 | A.1 Existing FHC Development Plan | Figure 14 Concept Viewline – Barter Crescent..... 36 |
| Executive Summary | 5 | A2. Amendment to FHC Development Plan | Figure 15 Office site – rear interface..... 36 |
| 1 Introduction | 6 | B1. Existing Mahoneys Road Development Plan | Figure 16 Office site – section diagram through rear interface 37 |
| 1.1 Forest Hill Chase Major Activity Centre | 7 | B2. Amendments to Mahoneys Road Development Plan | Figure 17 Carpark site – northern interface 37 |
| 1.2 The Subject Sites | 8 | Figures | Figure 18 Carpark – section diagram through 13 Lyell Walk (Rescode Standard B2-3.1 in green, and CoC building profile in yellow) 38 |
| 1.3 Development Timeline | 10 | Figure 1 Mahoneys Road Massing Study..... 5 | Figure 19 Carpark site – eastern interface 38 |
| 1.4 Urban Context | 13 | Figure 2 Forest Hill Chase, Mahoneys Road Entry Concept..... 7 | Figure 20 Carpark – section diagram through a dwelling at 7 Barter Crescent (Rescode Standard B2-3.1 in green, and CoC building profile in yellow) 39 |
| 2 Planning Control Context | 15 | Figure 3 Mahoneys Road Tenancies..... 13 | Figure 21 Carpark site – Southern interface (Rescode Standard B2-3.1 in green, and CoC building profile in yellow)..... 39 |
| 2.1 Existing Zone | 15 | Figure 4 Barter Crescent and Eastern Residential Area..... 13 | |
| 2.2 Existing Overlays | 16 | Figure 5 Carpark Site Southern Interface..... 14 | |
| 2.3 Policy Context | 19 | Figure 6 Mahoneys Road Interface 14 | |
| 3 The Proposed Amendment | 23 | Figure 7 Endorsed Development Plan, Shopping Centre & Carpark 17 | |
| 4 Assessment | 24 | Figure 8 Endorsed Development Plan, Mahoneys Road Sites..... 17 | |
| 4.1 Proposed Planning Control | 25 | Figure 9 Whitehorse Strategic Framework Plan – .. 22 | |
| 4.2 Built Form/Interface Considerations | 34 | Figure 10 Design Principles 27 | |
| 4.3 Traffic Considerations | 41 | Figure 11 September Equinox Shadow Outcomes (Max. Allowable Building Envelope) 35 | |
| 4.4 Strategic Considerations | 41 | Figure 12 September Equinox Shadow Outcomes (Max. Allowable Building Envelope) 35 | |
| 5 Conclusion | 47 | Figure 13 Concept Viewline – Mahoneys Road..... 35 | |
| Disclaimer | 48 | | |
| Appendices | | | |
| | | | Tables |
| | | | Table 1 – Planning Controls, Existing and Proposed 5 |
| | | | Table 2 – Site Details – Office..... 8 |
| | | | Table 3 – Site Details – Carpark Site..... 9 |
| | | | Table 4 – Proposed DDO Built Form Controls 30 |

10.6 – ATTACHMENT 1. Planning Report

Executive Summary

This report has been prepared on behalf of Haben in support of a planning scheme amendment to the Whitehorse Planning Scheme.

The planning scheme amendment seeks to facilitate the future redevelopment of the site at 69-79 Mahoneys Road (office building) and development of the carpark site (carpark) immediately south of the office building.

It is planned that the land remain in the Commercial 1 Zoning and that the existing Development Plan Overlay (DPO) is removed from the sites. Future development will be controlled through the application of a new Design and Development Overlay.

The planning scheme amendment also seeks to change the existing Development Plan Overlay Schedule 1 to remove reference to the need for provision of 'at least 130 car spaces to the east of Mahoneys Road'.

The table below identifies the current and proposed planning controls applicable to the subject sites.

Table 1 - Planning Controls, Existing and Proposed

| Current Control | Proposed Control |
|-------------------------------|---|
| Commercial 1 Zone | Commercial 1 Zone |
| Development Plan Overlay DPO1 | DPO removed from Subject sites. Schedule amended to remove reference to at least 130 cars to the east of Mahoneys Road. Schedule to remain applicable to the primary Forest Hill Chase site |
| N/A | New Design and Development Overlay Schedule introduced to Subject Sites. |

The existing office building is aged and no longer provides appropriate commercial accommodations. The existing at grade carpark represents an underutilised parcel of land. The ability to redevelop the carpark site is currently constrained by the reference to carparking in the DPO.

The assessment undertaken confirms that this carpark is surplus to the parking needs in the activity centre and that the existing provision of parking on Mahoneys Road together with the shopping centre parking, will more than support the demand.

The planning scheme amendment will unlock investment opportunities for the office and carpark sites, allowing these sites to be redeveloped for commercial or residential uses.

The new DDO seeks to ensure that future development proposals are designed with appropriate consideration of sensitive interfaces whilst looking to take full advantage of this unusual opportunity within this activity centre location.

Figure 1 Mahoneys Road Massing Study



Source: NH Architecture, 2026

10.6 – ATTACHMENT 1. Planning Report

1 Introduction

This report has been prepared on behalf of Haben as owners of the Forest Hill Chase Shopping Centre to support a Planning Scheme Amendment to facilitate the redevelopment of the current office building site at 69–79 Mahoneys Road (**office site**) and the at grade carpark south of the office building, also fronting Mahoneys Road (**carpark site**).

The Planning Scheme Amendment seeks to remove the existing Development Plan Overlay (DPO1) from applying to the subject sites and apply a new Design and Development Overlay over the sites. The amendment also seeks to remove reference within the existing Development Plan Overlay requiring the centre to include at least 130 car spaces to the east of Mahoneys Road.

Whilst the Development Plan Overlay – Forest Hill Activity Centre – has historically provided control for the subject sites, this control is over 25 years old.

The continued use of the carpark site for car parking only is considered to underutilise well located land within an existing Major Activity Centre. Similarly, the existing office building is largely vacant and no longer provides appropriate facilities. The current Development Plan Overlay does not support the opportunity for appropriate redevelopment and upgrading of development outcomes for the subject sites.

The planning scheme amendment introduces a new Design and Development Overlay to the sites, which seeks to provide a planning and design framework heavily guided by state and local planning policy and strategies, to catalyse the appropriate redevelopment of the sites and provide for a range of possible land uses including housing, employment, retail, commercial uses, in line with the Commercial 1 Zone and the role of the Major Activity Centre.

Planning Report



10.6 – ATTACHMENT 1. Planning Report

1.1 Forest Hill Chase Major Activity Centre

Forest Hill Chase Major Activity Centre (MAC) is located on a major east-west arterial road within the eastern suburbs of metropolitan Melbourne, approximately 20 kilometres due east of the CBD.

The Activity Centre is generally bound by Pacific Way to the east and south, Canterbury Road to the north, and residential land to the east of Mahoneys Road.

The Forest Hill MAC is generally defined as the Commercial 1 Zoned land that incorporates the enclosed Forest Hill Shopping Centre on the western side of Mahoneys Road, the PAD sites to Canterbury Road (Service Station, Hungry Jacks) and the commercial tenancies on the eastern side of Mahoneys Road. The activity centre also includes the at grade carpark on the eastern side of Mahoneys Road at the interface with residential properties.

Forest Hill Chase Activity Centre is identified in Plan for Victoria, the State Government's planning strategy, as an *Activity Centre - Housing Choice and Station*, which are 'activity centres with sufficient public transport, facilities and services, to be locations for large number of new homes. The activity centre plays a strong regional retail and entertainment role for the eastern suburbs of Melbourne, with Forest Hill Chase Shopping Centre at its core, serviced by an existing bus interchange.

The shopping centre is developed primarily as an enclosed mall over three levels and now includes significant retail tenants including Woolworths, Coles and ALDI supermarkets, Target discount department store, along with a number of mini-majors and smaller specialty tenancies.

A number of non-retail land uses also form part of the centre, including entertainment (such as Hoyts Cinema and AMF Bowling), cafes/restaurants, offices and services (such as banks, travel agents). A three-court basketball stadium is currently under construction on the carpark at Level 3 of the centre.

Public Transport

Forest Hill benefits from the existing bus interchange located towards the Canterbury Road frontage under the existing shopping centre. The interchange accommodates four bus routes, those being:

- 703 – Middle Brighton to Blackburn
- 735 – Box Hill Station to Nunawading
- 736 – Mitcham to Blackburn via Vermont South and Glen Waverley and Forest Hill
- 765 – Mitcham to Box Hill via Brentford Square, Forest Hill and Blackburn

The report of Infrastructure Victoria of December 2023 titled: *Fast, frequent, fair- How buses can better connect Melbourne* recognises the important role buses can and will continue to play in our public transport network and how there is a great opportunity to elevate the role of buses.

Figure 2 Forest Hill Chase, Mahoneys Road Entry Concept



Source: NH Architecture, 2025

10.6 – ATTACHMENT 1. Planning Report

1.2 The Subject Sites

The planning scheme amendment deals specifically with what we have identified as two sites – the office site and the carpark site.

Key details of the sites are as follows.

Table 2 – Site Details – Office

| Category | Description |
|----------------------------|---|
| Existing Conditions | A three-level office building with frontage to Mahoneys Road. The building has frontages to Council laneways to the north, east and south, and Mahoneys Road to the west. |
| Area | Approximately 1297 sqm |
| Title Detail | <ul style="list-style-type: none"> Lot 101 LP77935 Lot 102 LP77935 <p>There are no covenants or agreements or other encumbrances on title.</p> |
| Vegetation | None |

Picture 1: Office Building



Picture 2: Office Building



10.6 – ATTACHMENT 1. Planning Report

Table 3 – Site Details – Carpark Site

| Category | Description |
|----------------------------|---|
| Existing Conditions | An at grade carpark with access off Mahoneys Road. The carpark includes 177 parking spaces. Pedestrian access connecting Mahoneys Road to Barter Crescent (east) is provided at the southern boundary of the site. |
| Area | Approximately 5759 sqm. |
| Title Details | The carpark site is covered by multiple titles: Lot 125 LP50918, Lot 126 LP50918, Lot 127 LP50918, Lot 128 LP50918, Lot 129 LP50918, Lot 139 LP50918, Lot 140 LP50918 and Lot 1 TP805768 One easement impacts the subject site, relating to a historic road link in the benefit of the City of Nunawading (now City of Whitehorse). This relates to the potential for infrastructure in the former road reservation. |
| Vegetation | The carpark includes existing planted vegetation. 35 existing trees are located on the site, of varying species and sizes. <i>Refer Arborist report.</i> It is notable that the significant landscape overlay that applies to the adjoining residentially zoned land does not apply to this commercially zoned site. |
| Other | Unusually, the pedestrian path along Mahoneys Road is within the site’s title boundaries. |

The following historical photographs show the use of the subject sites over time and show how the sites have been developed as the core retail centre is also changed over time.

Picture 3: Carpark Site



Picture 4: Carpark Site



10.6 – ATTACHMENT 1. Planning Report

1.3 Development Timeline

The images overleaf show the general development timeline for the Forest Hill Activity Centre.

- 1960 – no development shown but former orchard land has been cleared to allow for the new uses. A road is constructed that links Barter Crescent to Mahoneys Road via the carpark site.
- 1963 – new retail development has commenced on the main shopping centre site, albeit the first development is individual tenancies (not an enclosed mall)
- 1970 – land to the north and south of the retail precinct has been clearly developed for carparking purposes. Commercial land uses have been developed on the eastern side of Mahoneys Road, including on the office land. The carpark land is more clearly delineated as a carpark. The road link between Barter Crescent and Mahoneys Road remains, with a residential property developed on the southern portion of the carpark land.
- 1975 – the car park is expanded with delineated car parking to both north and south of the road link. Existing residential properties west of the main centre are demolished to make way for additional carparking.
- 1981 – Further demolition of residential uses west of the centre and additional carparking developed.
- 1987 – road link between Barter Crescent and Mahoneys Road is removed and the former road is now used for carparking. The ring road around the main centre has been constructed. The centre is under construction to shift to an internally enclosed mall.
- 1996 – major redevelopment of the shopping centre has occurred. The centre is enclosed and is multi-level.
- 2005 – additional level added to the central portion of the southern shopping centre carpark. shopping centre is further expanded.
- 2015 – additional carparking level added to the north west of the centre and retail centre expanded towards Canterbury Road.



1960: Source maps.whitehorse.vic.gov.au



1963 Source maps.whitehorse.vic.gov.au

10.6 – ATTACHMENT 1. Planning Report



1970 Source maps.whitehorse.vic.gov.au



1981 Source maps.whitehorse.vic.gov.au



1975 Source maps.whitehorse.vic.gov.au

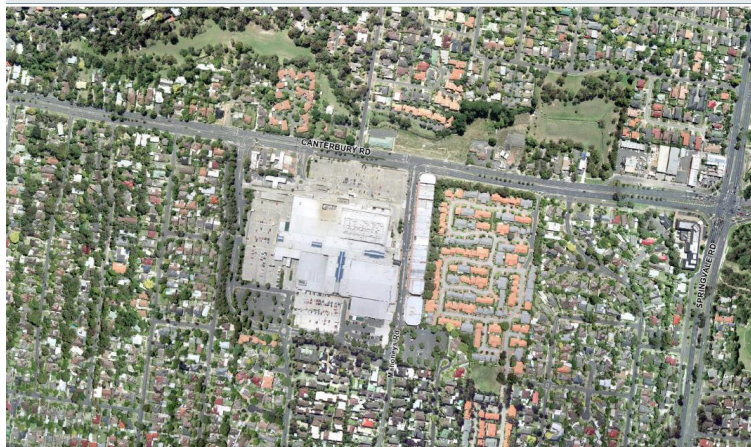


1987 Source maps.whitehorse.vic.gov.au

10.6 – ATTACHMENT 1. Planning Report



1996 Source maps.whitehorse.vic.gov.au



2005 Source maps.whitehorse.vic.gov.au



2015 Source maps.whitehorse.vic.gov.au



2024 Source maps.whitehorse.vic.gov.au

10.6 – ATTACHMENT 1. Planning Report

1.4 Urban Context

North

To the north of the office site are the retail and commercial tenancies along Mahoneys Road. These tenancies are primarily single storey, and most are in individual ownership which has limited their potential for redevelopment or expansion.

Figure 3 Mahoneys Road Tenancies



East

To the east (rear) of the office building is the rear of the Forest Gardens Retirement Village. Forest Gardens is characterised by single and double storey attached dwellings. Some of these dwellings have an interface to the rear of the commercial properties on Mahoneys Road.

There is a Council laneway that runs behind the commercial properties to allow for access and waste removal etc. The residential properties generally have landscaped backyards.

A pedestrian path between Forest Gardens and the activity centre is provided running east-west between the office and carpark. No change to this pedestrian access is proposed, noting the path also runs along the Council laneway.

East of the carpark site are residential properties with Barter Crescent addresses. The site at 7-9 Barter Crescent have been recently redeveloped into a townhouse development of six two storey properties and one single storey property, as well as an associated seven lot subdivision.

Figure 4 Barter Crescent and Eastern Residential Area



10.6 – ATTACHMENT 1. Planning Report

South

To the south, the carpark has an interface to residential properties. These properties are zoned General Residential Zone 1 (GRZ1).

An existing pedestrian path provides separation between the carpark and the residential properties – this path provides access for residents in Barter Crescent to the activity centre.

It is proposed that the path is retained to encourage the ongoing pedestrian access to the activity centre.

Figure 5 Carpark Site Southern Interface



West

The western interface for both subject sites is immediately to Mahoneys Road but beyond that to the Forest Hill Chase Shopping Centre.

For the office building, their interface is to the south eastern portion of the enclosed centre.

For the carpark site, the interface to the shopping centre is to an at grade carpark. It is possible, and subject to planning approval, that the shopping centre could extend into this area. For the purposes of this assessment, we have assumed that the carpark therefore interfaces to the west to a commercial development.

Figure 6 Mahoneys Road Interface



10.6 – ATTACHMENT 1. Planning Report

2 Planning Control Context

2.1 Existing Zone

The subject site is located within the Commercial 1 Zone (C1Z). The purpose of the C1Z is:

- To implement the Municipal Planning Strategy and the Planning Policy Framework.
- To create vibrant mixed use commercial centres for retail, office, business, entertainment and community uses.
- To provide for residential uses at densities complementary to the role and scale of the commercial centre.

No change to this underlying zoning is proposed.

It is considered appropriate that the sites continue to form a buffer to the residentially zoned land to the west and south.

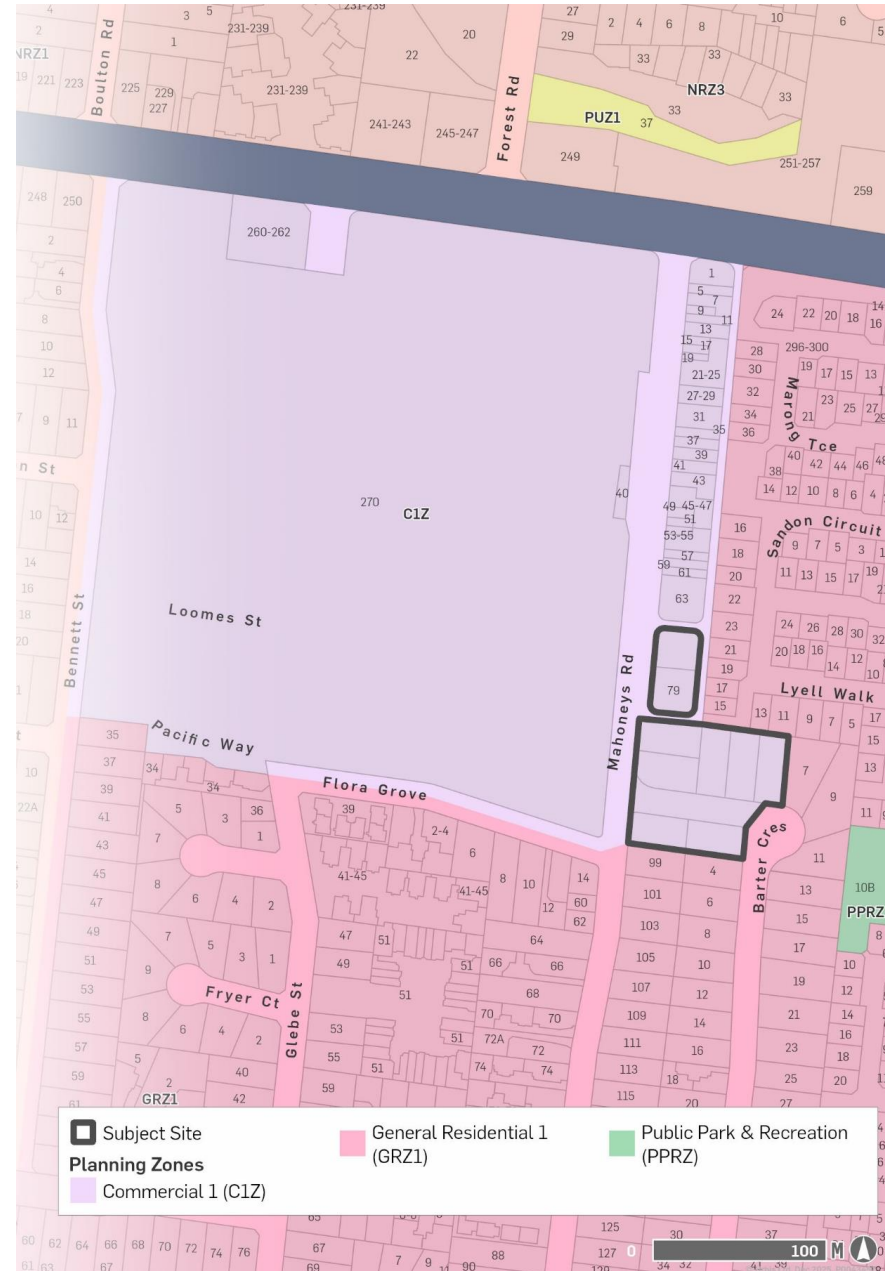
The C1Z is one of the most flexible zones in the Victoria Planning Provisions (VPPs), and permits a wide variety of land uses, including retail, office and residential uses generally as of right.

Pursuant to Clause 34.01-1, a permit is required for use of land for accommodation, where the frontage exceeds 2m at ground level.

Pursuant to Clause 34.01-4, a permit is required for buildings and works.

In retaining the existing zoning, we have sought to retain opportunities for the sites to respond to the market conditions.

Planning Report



10.6 – ATTACHMENT 1. Planning Report

2.2 Existing Overlays

Development Plan Overlay – Schedule 1 (DPO1)

The subject sites are affected by Schedule 1 of the Development Plan Overlay (DPO1). The purpose of the DPO is:

- To implement the Municipal Planning Strategy and the Planning Policy Framework.
- To identify areas which require the form and conditions of future use and development to be shown on a development plan before a permit can be granted to use or develop the land.
- To exempt an application from notice and review if a development plan has been prepared to the satisfaction of the responsible authority.

Pursuant to Clause 43.04-2, a permit must not be granted to use the land, construct a building, or carry out works until a development plan has been prepared to the satisfaction of the responsible authority.

A permit granted must:

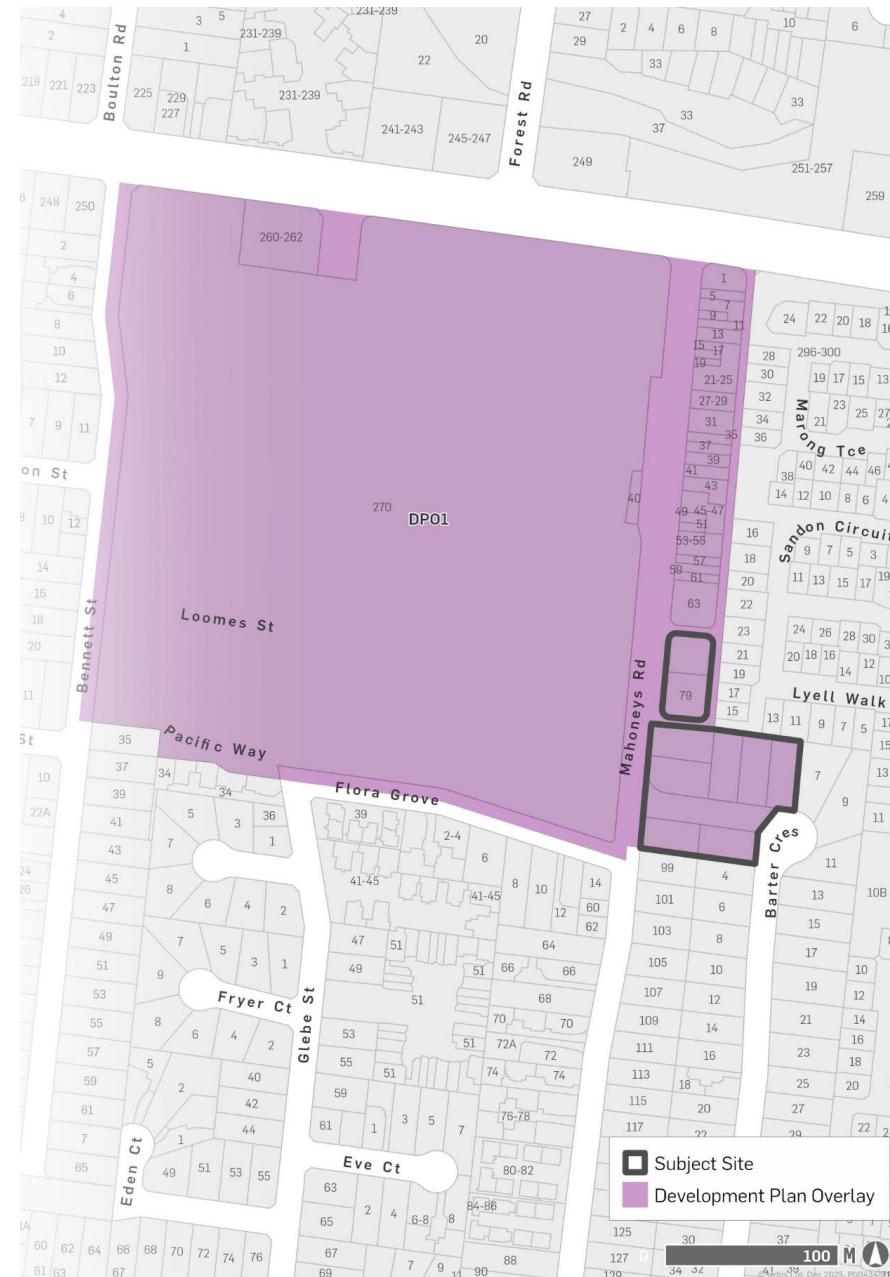
- Be generally in accordance with the development plan.
- Include any conditions or requirements specified in a schedule to this overlay

A Development Plan has been approved by Whitehorse City Council for the Forest Hill Chase Shopping Centre (which for the purposes of the Development Plan includes the carpark site). Another Development Plan has been approved for the Mahoneys Road shops (including the office building but excluding the carpark site).

Pursuant to Clause 43.04-3 of the Development Plan Overlay, if a development plan has been prepared to the satisfaction of the responsible authority, an application is exempt from the notice requirements and review rights under the Act.

This automatic exemption from third party notice and review under the DPO will no longer apply to the carpark or office sites if the amendment is approved.

Planning Report

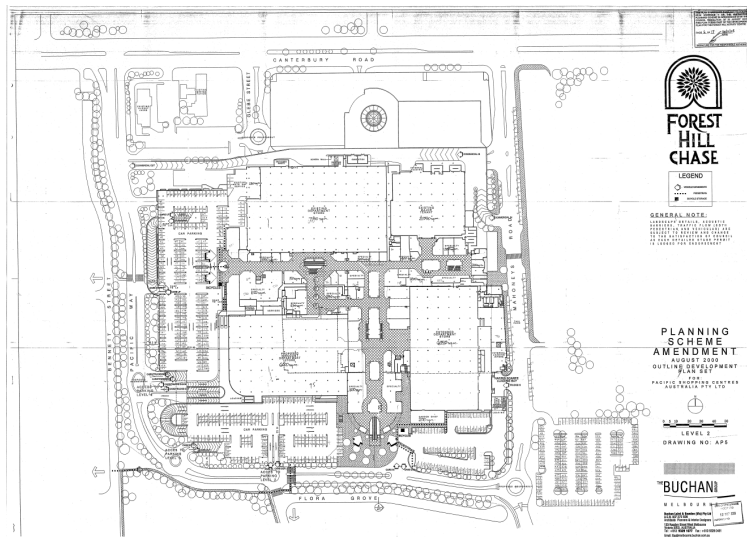


10.6 – ATTACHMENT 1. Planning Report

The existing Development Plan Overlay schedule is over 25 years old and whilst it has served the existing development of the Shopping Centre, it does not provide any future vision for either the office or carpark sites. The retention of these elements as static developments within the existing activity centre is considered an underutilisation of the land.

The current Development Plan was generally endorsed in 2003 (although some pages have more recent updates). The Development Plan comprises multiple pages, with the specific endorsed plan for what is the ground floor at Mahoneys Road shown below.

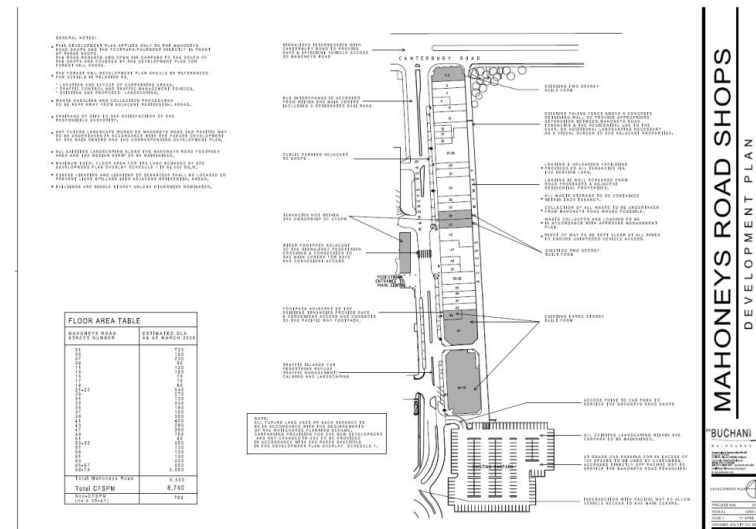
Figure 7 Endorsed Development Plan, Shopping Centre & Carpark



This shows the carpark site as an open carpark.

The endorsed Development Plan for those sites along Mahoneys Road, including sites not owned by Haben, is shown in Figure 8. This was approved in 2006. This shows the office buildings as ‘existing three storey built form’. Under this approved Development Plan a redevelopment of the office building would not be considered to be ‘generally in accordance’ with the plan and therefore without amending the plan, any application would be refused.

Figure 8 Endorsed Development Plan, Mahoneys Road Sites



Furthermore, regarding the carpark site, the Development Plan schedule stipulates that the site must provide a minimum of 130 car spaces to service Mahoneys Road tenancies to the north.

Additionally, the endorsed development Plan includes a number of General Notes. Of relevance, the general notes include the following:

- Maximum Total Floor Area for the Land Bounded by the Development Plan Overlay Schedule 1 is 84,000 SQ.M.

10.6 – ATTACHMENT 1. Planning Report

This notation is a response to considerations of the development of the centre some 20 years ago, when the maximum shop and office floor area in an activity centre was controlled by a schedule to the zone.

Restricting floor space within an activity centre is no longer a feature of planning schemes in established areas. Notwithstanding the appropriateness or otherwise of this restriction, this would no longer apply to the subject sites.

Development Contributions Plan Overlay – Schedule 1 (DCPO1)

The subject sites are affected by Schedule 1 of the Whitehorse Development Contributions Plan Overlay (DCPO). The purpose of the DCPO is:

- To implement the Municipal Planning Strategy and the Planning Policy Framework.
- To identify areas which require the preparation of a development contributions plan for the purpose of levying contributions for the provision of works, services and facilities before development can commence

Pursuant to Clause 45.06, a permit must not be granted to subdivide land, construct a building, or construct and carry out works until a development contributions plan has been incorporated into the scheme.

The DCPO in the Whitehorse Planning Scheme applies across the municipality and requires that new development pays the prescribed levies.

The overlay requires payment of financial contributions for both residential and non-residential uses.

The overlay also allows for landowners to enter into agreements with Council to provide contributions in kind or via other mechanisms.

This will remain applicable for the subject sites – the amendment does not affect this existing control.

Planning Report



10.6 – ATTACHMENT 1. Planning Report

2.3 Policy Context

2.3.1 Planning Policy Framework (PPF)

The Planning Policy Framework (PPF) seeks to foster the objectives of planning in Victoria (as set out in the Planning and Environment Act, 1987) through appropriate planning policies and practices that encompass relevant environmental, social and economic factors. The PPF includes a number of policies which are relevant to Activity Centres. These are summarised below:

- **Clause 11** – Settlement seeks to facilitate sustainable development that takes full advantage of existing settlement patterns and investment in transport, utility, social, community and commercial infrastructure and services.
- **Clause 11.01-1R** - Settlement – Metropolitan Melbourne seeks to maintain a permanent growth boundary around Melbourne to create a more consolidated network of activity centres, and mixed use neighbourhoods to develop a growth in appropriate scale to address the need of Melbourne’s rapidly growing population. Additionally, develop Activity Centres where an existing network linked by transport may support vibrant centres for the wider community.
- **Clause 11 – Settlement – 11.03-1S**- Activity Centres encourages the concentration of major retail, residential, commercial, administrative, entertainment and cultural developments into activity centres that are highly accessible to the community.
- **11.03-1R Activity Centres** – Metropolitan Melbourne seeks to ensure development and growth of Activity Centres by ensuring that they incorporate a range of land uses, high levels of amenity and are supported by infrastructure.
- **Clause 11.03-1L** - Activity Centres aims to ensure that land use and development in activity centres is consistent with the role and category of the centre. This clause further supports redevelopment of existing buildings, and other works, that improve the visual amenity and streetscape of activity centres.
- **Clause 13** – Environmental Risks and Amenity seeks to strengthen the resilience and safety of communities by adopting a best practice environmental management and risk management approach.
- **Clause 13.05-1S** – Noise Management seeks to assist the management of noise effects on sensitive land uses.
- **Clause 13.07-1S** – Land Use Compatibility seeks to protect community amenity, human health and safety while facilitating appropriate commercial, industrial, infrastructure or other uses with potential adverse off-site impacts.
- **Clause 15** – Built Environment and Heritage seeks to create well designed urban environments that are safe, attractive, easily accessible and provide a sense of cultural identity. Broadly Clause 15 seeks to ensure:
 - Design that enhances liveability, diversity, amenity and safety of the public realm.
 - Require development to respond to its context in terms of urban character, cultural heritage, natural features, surrounding landscape and climate.
 - Ensure transport corridors integrate land use planning, urban design and transport planning and are developed and managed with particular attention to urban design aspects.
 - Design of interfaces between buildings and public spaces, including the arrangement of adjoining activities, entrances, windows, and architectural detailing, should enhance the visual and social experience of the user.
 - Locate commercial facilities in existing or planned activity centres.
 - Provide outlets of trade-related goods or services directly serving or ancillary to industry and which have adequate on-site carparking.
- **Clause 15.01-1S** – Urban Design seeks to create urban environments that are safe, healthy, functional and enjoyable and that contribute to a sense of place and cultural identity.

10.6 – ATTACHMENT 1. Planning Report

- **Clause 15.01-2S** – Building Design seeks to achieve building design and siting outcomes that contribute positively to the local context, enhance the public realm and support environmentally sustainable development.
- **Clause 15.01-4S** – Healthy Neighbourhoods seeks to achieve neighbourhoods that foster healthy and active living and community wellbeing.
- **Clause 15.01-4R** – Healthy neighbourhoods – Metropolitan Melbourne seeks to create a city of 20 minute neighbourhoods, that give people the ability to meet most of their everyday needs within a 20 minute walk, cycle or local public transport trip from their home.
- **Clause 15.01-5S** – Neighbourhood Character seeks to recognise, support and protect neighbourhood character, cultural identity, and sense of place.
- **Clause 15.01-2L – Environmentally Sustainable Development** seeks building design to achieve environmental targets relating to (amongst other things) integrated water management, environmentally sustainable urban form, and improved amenity. .
- **Clause 15.01-5L-01** – Tree Conservation encourages the retention of healthy trees that are appropriately located to allow design and development around them.
- **Clause 16** – Housing outlines that planning should provide for housing diversity, affordability and sustainability.
- **Clause 16.01-1S** – Housing Supply seeks to facilitate well-located, integrated and diverse housing that meets community needs.
- **Clause 16.01-2S** – Housing Affordability seeks to deliver more affordable housing closer to jobs, transport and services.
- **Clause 17** – Economic Development seeks to provide for a strong and robust economy with a focus on innovation and productivity.
- **Clause 17.01.1S** – Diversified Economy seeks to strengthen and diversify the economy.
- **Clause 17.02-1S** – Business seeks to encourage development that meets the community's needs for retail, entertainment, office and other commercial services.
- **Clause 18** – Transport seeks to ensure an integrated and sustainable transport system that provides access to social and economic opportunities, facilitates economic prosperity, contributes to environmental sustainability, coordinates reliable movements of people and goods, and is safe.
- **Clause 18.01-1S** – Land Use and Transport Integration seeks to facilitate access to social, cultural and economic opportunities by effectively integrating land use and transport.
- **Clause 18.02-1S** – Walking seeks to facilitate an efficient and safe walking network and increase the proportion of trips made by walking.
- **Clause 18.02-2S** – Cycling seeks to facilitate an efficient and safe bicycle network and increase the proportion of trips made by cycling.

10.6 – ATTACHMENT 1. Planning Report

2.3.2 Plan for Victoria

The Victorian State Government released the *Plan for Victoria* on 28 February 2025, which sets out a long-term strategy for the development and growth of Victoria. The Plan sets out five pillars underpinning the strategic direction of growth and development within Victoria in seeking to ensure residents of Victoria are provided with high-quality and affordable homes that are proximate to services, transport and amenities, and within well designed neighbourhoods.

The plan envisages that future development will be concentrated within and surrounding Activity Centres. Moreover, the Plan provides specific housing targets to Victoria’s LGAs. With regard to Whitehorse specifically, the Plan sets the target of achieving the delivery of 76,500 new homes by 2051. Overarchingly, the Plan displays a clear intent from the Victoria State Government to address the current shortage of affordable housing, and housing more generally, in driving a significant increase in housing provision within areas well-served by public transport, employment and community infrastructure / services.

A number of key actions outlined within the Plan are relevant to this proposal:

- *Implement housing targets for every local government area.*
- *Implement new planning controls to streamline planning in activity centres.*
- *Increase the number of social and affordable homes.*
- *Encourage walking and cycling.*

Forest Hill Activity Centre is identified in *Plan for Victoria*, the State Government’s planning strategy, as an *Activity Centre – Housing Choice and Station* which are ‘activity centres with sufficient public transport, facilities and services to be locations for large numbers of new homes.’ The activity centre plays a strong regional retail and entertainment role for the eastern suburbs of Melbourne with Forest Hill Chase Shopping Centre at its core and has an existing bus interchange.

Planning Report

Plan for Victoria is structured around five pillars.

They are:



Self-determination and caring for Country:
underpinning and informing the other pillars.



Housing for all Victorians:
delivering sufficient affordable homes for all Victorians



Accessible jobs and services:
making sure you have good access to facilities and good jobs



Great places, suburbs and towns:
creating thriving places that are attractive, safe and welcoming for everyone



Sustainable environments:
preserving the natural values of Victoria and addressing climate change

10.6 – ATTACHMENT 1. Planning Report

Plan for Victoria seeks to provide more homes in well-connected, well-serviced locations around activity centres, train stations and priority precincts. It also includes local government housing targets, with Whitehorse being allocated a target of 76,500 new homes by 2051. The amendment supports the development of activity centres as a focus for high-quality development, activity and living, including forms of higher density living

2.3.3 Municipal Planning Strategy

- **Clause 02.01** – Context identifies Forest Hill Chase as a ‘Large Activity Centre’.
- **Clause 02.02** – Vision identifies a key strategic direction of Council being to promote health and well-being within the community through appropriate land use and development.
- **Clause 02.03-1** – Settlement reinforces the role of the Forest Hill Chase Activity Centre as serving a variety of attributes and functions. This clause further designates the centre as a preferred location for convenience and comparison retail facilities, a cinema complex, and entertainment facilities. More generally, the Clause seeks to direct new development toward activity centres, noting their importance in achieving objectives regarding sustainability, reducing the dominance of car travel, and building a sense of community and place. The strategy seeks to achieve ‘20-minute neighbourhoods’, in alignment with state policy.
- **Clause 02.03-3** – Built environment and heritage seeks to promote best practice Environmentally Sustainable Development (ESD) principles in new developments, and ensure new development is appropriately designed with regard to its surrounding context.
- **Clause 02.03-6** – Housing outlines the objectives and goals for housing growth within the municipality. The clause identifies the need for considerable housing growth within appropriate location, in response to increased housing pressure. Larger activity centres are identified as ‘Substantial change’ areas, where the majority of housing, of varying typology is directed. Importantly the clause seeks to ensure that activity

centres accommodate substantial additional housing and density, predicated on capacity, public transport and infrastructure provision.

- **Clause 02.03-7** – Economic Development provides the context and basis for economic development within Whitehorse. The strategy importantly outlines the strategic direction of focusing increased employment densities within the municipality’s activity centres.
- **Clause 02.03-8** – Transport seeks to encourage sustainable transport use and reduce car dependency, utilising the appropriate integration of land use and transport planning, directing new developments around public transport facilities and within activity centres.

Figure 9 Whitehorse Strategic Framework Plan -



Source: City of Whitehorse Planning Scheme

10.6 – ATTACHMENT 1. Planning Report

3 The Proposed Amendment

The planning scheme amendment seeks to **delete** the existing Development Plan Overlay (DPO1) from applying to the subject sites. The DPO1 is proposed to continue to apply to the other land within the Forest Hill Major Activity Centre.

It is also proposed to **amend** the schedule to the Development Plan by removing the requirement that *'at least 130 car spaces are provided on the east side of Mahoneys Road'*.

The removal of the DPO's applicability to the subject sites, will remove any notations within the Development Plan controlling use and/or development to either subject site. The maximum total floor area of 84,000 sqm outlined within the endorsed Development Plan, pertaining to land subject to Schedule 1 to the DPO, will remain in place, however the floor area of any future development on the subject sites will not be included within this calculation, given it will no longer be affected by the DPO.

Further, the DPO schedule identifies *'car parking in the ratio of at least 5 car spaces to each 100 square metres of leasable floor area for a shop and 4 car spaces to each 100 square metres of leasable floor area for all other uses, or a lesser figure to the satisfaction of the responsible authority'*. Removing the Mahoneys Road carpark does not alter this as an ongoing requirement for the

shopping centre site. This means that future development or redevelopment of the shopping centre must respond to this control and/or seek a reduction from the responsible authority.

We note that it is likely that this carparking control is reviewed in the medium term to respond to changes to the Victoria Planning Provisions introduced in December 2025, however is outside the scope of this proposal.

Altering the DPO schedule will allow the southern carpark parcels to be used for non-parking uses but would not create new built form controls. Therefore, the **introduction** of the Design and Development Overlay will provide clarity to future landowners and neighbours, as to the likely development outcome on the land.

With respect to future development of the office or carpark sites, the removal of the DPO will facilitate the opportunity for future development to undergo public notice, which is currently exempted under the DPO.

An assessment of the amendment against the Strategic Assessment Guidelines is provided in Section 4.4.

Draft amendment documents for the DPO and DDO are enclosed within this submission.

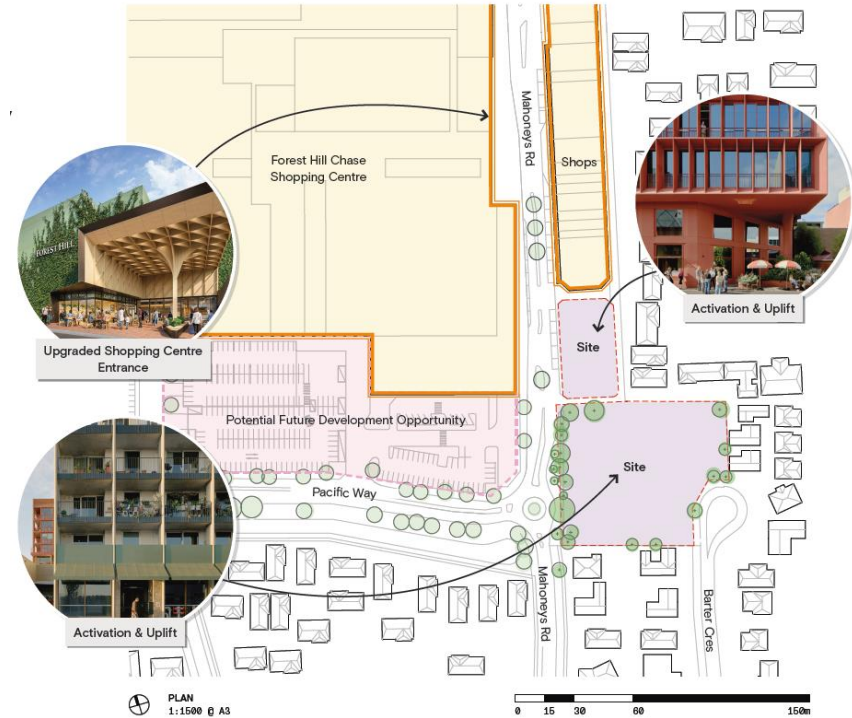
10.6 – ATTACHMENT 1. Planning Report

4 Assessment

The following sections of this report provide an assessment of the proposal against the relevant statutory and strategic provisions of the Planning Scheme. This assessment focuses on the appropriateness of the following key matters:

1. Appropriateness of the proposed planning controls
2. Built Form/Interface Considerations
3. Traffic Considerations
4. Consistency with the Strategic Assessment Guidelines

Each of these matters is dealt with in turn below.



Plans by NH and provided in this report and in their report illustrate Indicative Development Concepts – that is opportunities that may be available based on the proposed planning controls. They do not seek to confirm exactly what will be delivered, but are used to provide examples so that the planning controls proposed can be more readily understood.

We note that as the underlying zoning is Commercial 1, the range of permissible uses under the zone could be undertaken on the land. We have, for the purposes of the imagery prepared, assumed the most likely outcomes are commercial uses on the office land and residential uses on the carpark site.

10.6 – ATTACHMENT 1. Planning Report

4.1 Proposed Planning Control

4.1.1 Change to the DPO Schedule

The existing Development Plan Overlay for the Forest Hill Major Activity Centre provides only limited guidance within which to assess applications for permits on the subject sites.

The DPO schedule is over 25 years old and provides limited information with respect to future development opportunities other than for the main shopping centre. Moreover it also provides limited information to justify the ongoing application of the restrictions to the subject sites.

For the office building the approved Development Plan allows for no change or redevelopment, whilst the carpark is unable to be substantially redeveloped unless it continues to provide a significant carparking supply.

We have undertaken significant research in seeking to determine the history of the restriction of *'not less than 130 car spaces on the eastern side of Mahoneys Road'*. We have not found information that would prohibit removing this restriction.

There is no evidence to suggest the carpark land was ever Council owned (other than a former road link), and certainly not in the time since the land was developed as a carpark.

Whilst we appreciate that some members of the community may consider this land to be a 'public' carpark, it is privately owned land.

We have drawn on the feedback of the City of Whitehorse officers who equally have not found any agreement that the land must be available for 'public' use.

The removal of this restriction will require current users of the carpark site to find alternative locations for parking, other than the Mahoneys Road site. This will require people to use either the onstreet parking in Mahoneys Road north of the subject sites, or carparking associated with the main shopping centre.

The Eukai Report prepared to support the amendment indicates that there are currently 3,118 spaces associated with the shopping centre. This is split between 2,941 spaces on the shopping centre site (i.e. west of Mahoneys Road) and 177 spaces in the subject carpark.

The on-street parking on Mahoneys Road provides 28 publicly accessible spaces (excluding loading and taxi zones).

Given the history of surveys on the shopping centre site, and an assessment of aerial photography, the carparking demands for the shopping and the Mahoneys Road tenancies are more than accounted for by the shopping centre provision.

We note the shopping centre carparking supply is to be reduced as the Level 3 carpark is redeveloped in part to accommodate a basketball stadium. The available parking will reduce to 2,797.

Even accounting for new demand associated with the basketball stadium, the assessment by Eukai is that:

- There are ample vacancies in the existing main centre carpark to accommodate any displaced carparking demands associated with the removal of the Mahoneys Road off-street car park.
- Despite there being a carparking shortfall against the DPO carparking requirement for the main centre carpark following the removal of the Mahoneys Road carpark, there is a surplus of carparking against Clause 52.06 car parking requirements.
- Accordingly, it is considered appropriate to remove the *"at least 130 car spaces to the east of Mahoneys Road"* car parking requirement from the DPO.
- The adoption of the default Clause 52.06 car parking rates for the two sites to the east side of Mahoneys Road is considered appropriate.

Refer to the report by Eukai for further detail.

10.6 – ATTACHMENT 1. Planning Report

4.1.2 Removal of the DPO from the Subject Sites

Whilst a Development Plan Overlay can provide clarity and surety to the owners, the Council and the broader community as to the design outcomes possible for specific land, for the office and carpark sites the endorsed Development Plans provide limited information.

Whilst the approved Development Plan for Forest Hill Chase has expectations for significant additions by way of new retail and non-retail floorspace and new carparking decks, the extent of retail floorspace proposed is unlikely to ever be constructed.

In the years since approval of the Development Plan, the retail landscape in the eastern suburbs has changed greatly. Whilst Forest Hill Chase was earmarked to be a major sub-regional shopping centre, its market share has been eroded by the approval of other retail centres in the catchment. New or expanded centres in the catchment include Burwood One, Burwood Brickworks, Box Hill, Blackburn North, the Mega Mile.

At the higher order, centres such as Doncaster, The Glen and Eastland have also expanded significantly over the past two decades.

As the core retail centre is unlikely to reach the level of development previously planned for, then the need for the carpark sites to be retained to support the centre is questionable.

The office building was constructed in the 1970s – now approaching 50 years of age, and as such the building is not conducive to modern office standards. Consequently, the building has not been fully leased in many years despite the best efforts of the centre managers.

As the detail required by the DPO schedule is primarily directed towards operation of the major shopping centre component, there is no specific direction for development on the subject sites.

We consider the removal of the DPO from the subject sites coupled with the introduction of the new DDO to these sites means that they will have an increased level of planning control rather than a reduction.

4.1.3 Creation of a New DDO

Intention of the DDO

The proposal seeks to include a Design and Development Overlay over the subject sites. The office and carpark sites have bespoke built-form requirements to respond to their specific context.

The purpose of a DDO is as follows:

- *To implement the Municipal Planning Strategy and the Planning Policy Framework.*
- *To identify areas which are affected by specific requirements relating to the design and built form of new development.*

It is important to note that the DDO cannot change the scope of discretion provided in the zone, in this case the Commercial 1 Zone. What this means is that the use of land is primarily controlled via the CIZ. The DDO, however, allows specific, bespoke controls to be developed for the subject sites. And the proposal incorporates different controls to each site, guided by their unique attributes.

Architectural imagery has been prepared by NH Architecture to provide visual representation of the maximum development envelopes that are contemplated under the proposed DDO. It is important to note that these images represent potential development outcomes in an instance where the allowable built form controls are maximised. **They should not be considered to be the only outcome likely under the proposed controls.**

The carpark site for example could be redeveloped for retail or other commercial uses – uses that would have a different architectural expression from the imagery shared. This would be permitted under the Commercial 1 Zone. However, the DDO provides a series of setbacks and heights that any development needs to respond to. Therefore, the parameters are established for any future use on the subject sites.

10.6 – ATTACHMENT 1. Planning Report

In addition to built form requirements, the proposed DDO includes general design and access requirements applicable to future developments on the sites.

We have reviewed existing Whitehorse DDOs in guiding development of this control, with particular focus on the City of Whitehorse’s DDO4, (Neighbourhood Activity Centres), to assist in guiding built form, decision guidelines and design objectives incorporated within the proposed DDO. Notwithstanding, some additional design objectives have been included in the proposed DDO that relate specifically to the land subject to this proposed amendment.

To ensure clarity of the controls, we note that the proposed DDO defines the eastern frontage of the carpark site to Barter Crescent as a rear frontage, with the intention that Mahoneys Road act as the primary frontage of the site and where primary vehicle access is to be implemented. Notwithstanding, the built form controls implemented at this interface have been derived with recognition that this is a residential street interface, and therefore building setbacks should be reflective of the prevailing character of the streetscape. .

With regard to vehicle access, the DDO includes commentary that seeks to minimise vehicular access at via Barter Crescent. Whilst the proposed DDO does not fully restrict vehicular access from Barter Crescent, it is explicitly discouraged. The control allows for appropriate discretion however, and provides opportunity for a future development proposal to incorporate access via this frontage, in the instance where ample justification for this is provided. Justification for vehicular access via this frontage must have regard to the decision guidelines and objectives of the DDO, as well as broader traffic considerations, to the satisfaction of Council.

Ultimately, the creation of the DDO relied upon utilising existing policy both at a state level and Council level, underpinned by four key design principles:

1. Human Scale and Fine Grain
2. Connected and Permeable
3. Future Proof – Complementing the Activity Centre Core
4. Considered Residential Transition

These design principles, in conjunction with the implementation of existing policy, results in a draft DDO that is in our view conducive to future high quality urban and built form that will assist in the growth of the activity centre in a sustainable and design-oriented manner.

Figure 10 Design Principles



Source: NH, Architecture, 2026

Further detail regarding how the DDO has been drafted and design controls is provided below.

Development of Built Form Controls

The proposed built form controls have been carefully considered and derived through a multi-faceted analysis of existing built form controls throughout Whitehorse’s activity centres, Stage Government built form typology guidelines for activity centres outlined within the City of Centres (CoC) strategic document, and relevant Rescode standards (Clauses 55 and 58 of the Whitehorse Planning Scheme).

Moreover, a thorough analysis of the existing conditions on the site and surrounds has been undertaken by NH Architecture and Urbis Urban Design, to create and assess potential built form envelopes that are appropriately designed to limit loss of amenity to surrounding residential areas and

10.6 – ATTACHMENT 1. Planning Report

reflective of each site's unique attributes. These envelopes have been considered through shadow testing of varying built form typologies as well as massing and viewline analyses, to ensure that the proposed controls have appropriately considered the amenity of adjoining residential properties.

At a high level, the collaborative process of deriving built form controls was undertaken as follows:

- Review of built form controls applied to similar activity centres within the City of Whitehorse, specifically DDO4. However, given DDO4 was developed out of specific urban design assessments it was not considered appropriate to replicate this for the subject sites. A bespoke control allows the specifics of the sites to be recognised.
- Review of built form controls applicable within Rescode (Clause 55 and 58) and the City of Centres (CoC) Strategic Document.
- In consultation with Council, adopting the CoC Type 1 AC Fringe Precinct built form controls as the basis for the proposed built form controls.
- Testing built form envelopes allowable under the above controls for massing and shadowing analysis.
- Tweaking setback controls based on amenity testing (shadowing, massing) to ensure no overshadowing posed to residential interfaces.

The above process was undertaken for both sites subject to the proposed amendment, with further detail regarding the derivation of built form controls outlined below.

Ultimately, the controls have been guided to facilitate an outcome on each site that is reflective of the role of the activity centre and relevant state and local objectives, seeking development to be directed toward activity centres, while remaining cognisant of the residential contexts adjacent to the subject sites.

In this respect the controls consider the potential for commercial land uses (office, retail etc) as permitted by the Commercial 1 Zoning and for residential land uses.

The proposed DDO draws heavily from the built form controls outlined within the CoC built form typology guidelines outlined by the State Government. The CoC provides an activity centre hierarchy, denoting differing types of activity centres and directing relevant built form typologies based on the activity centre type and site specific characteristics.

While not specifically identified within the CoC strategic document, our interpretation of the Forest Hill Chase Activity Centre, as it relates to the CoC activity centre hierarchy, would be designated as a "Type 1 AC (lowest density)". This activity centre type refers to localities with limited access to fixed public transport (train or tram) and low employment access. While the Forest Hill Chase Activity Centre does provide for a considerable number of employment opportunities, it is acknowledged this is somewhat minor when compared to other activity centres – given the retail-oriented nature of the Forest Hill Chase Activity Centre, as opposed to a broad-based commercial/office precinct. Moreover, while limited access is provided to fixed public transport, it is noted that an extensive bus network system connects the activity centre to the wider municipality and beyond – ensuring appropriate access for visitors and residents. This is reflected in the designation of Forest Hill Chase as an "Activity Centre – Housing Choice and Station" within the State Government's *Plan for Victoria*.

The CoC document further defines distinct precinct types within each activity centre, guiding built form outcomes at a more fine-grain scale, based on the location of sites within the activity centre.

The sites subject to this proposal are deemed most suitable to be defined within the 'Fringe' precinct, given their location on the edge of the defined core activity centre area. The CoC strategic document describes fringe precincts as comprising fine-grain subdivision patterns with a mix of shopfronts and dwellings. Specific defining attributes of fringe precincts detailed within the document include [emphasis added]:

- *TR22 interface*
- **C1Z, ACZ, or MUZ zoned land**
- *Narrow lots (typically <8m)*

10.6 – ATTACHMENT 1. Planning Report

- Occasionally affected by the Heritage overlay
- **Commonly served by a rear lane**
- **20m wide road reserve**
- **Generally only 1 lot deep**
- **Adjacent to residentially-zoned properties at the rear**

The CoC document provides the following guidelines for Fringe Precincts within Type 1 Activity Centres. The built form controls proposed in the draft DDO have been designed to align with these guidelines.

| Built Form Element | Control |
|---|---|
| Maximum Building Height | 21m (Maximum) |
| Street Wall Height: | 11m |
| Front Setbacks Above Street Wall | <ul style="list-style-type: none"> • Street wall: built to boundary • Above street wall, up to 18m: 3m • Above 18m: 3m + 1m per additional 1m height |
| Side and Rear Setbacks From Land Zoned GRZ | <p>Up to 5m: 0m</p> <p>Above 5m, up to 15m: 1m per additional 1m height above 5m</p> <p>Above 15m, up to 20m: 10m</p> <p>Above 20m: 10m + 1m per additional 1m height above 20m</p> |
| Side Setback From Commercially-Zoned Land | Where average site width > 30m: Minimum 4.5m setback |

The controls detailed above provided the basis for the built form controls applied within the proposed DDO.

However, further amenity testing was conducted by NH Architecture to ensure no shadowing was posed to neighbouring residential properties. Consequently, the minimum setbacks to the eastern interfaces of both sites was increased, compared to the standard City of Centres setback controls – in recognition of the importance of preserving neighbouring residential amenity.

As shown at Page 14, a building envelope adhering to the extent of the discretionary controls, does not result in shadow to neighbouring residential properties on the September equinox. Given the state-level acceptance and direction for such built form controls, the proposed DDO is highly aligned with relevant strategic principles and guidelines.

Moreover, we note that the setback controls to the southern boundary (interfacing with GRZ) are consistent with City of Centres controls, however include some tweaking to reflect the retention of the pedestrian footpath and landscaping strip. For simplicity, the setbacks at this interface according to each storey.

The appropriateness of these built form controls are clearly reflected in the architectural imagery prepared by NH Architecture, as well as Urbis Urban Design input, provided at Section 4.2.1 of this report. The development enveloped result in minimal shadow impacts to neighbouring properties, and a built form outcome that does not inappropriately overlook or overpower the surrounding pedestrian scale. The proposed setback and height controls ensure future built form will provide an appropriate and measured transition from surrounding residential land into the activity centre core – in alignment with the overarching outcomes sought for ‘fringe’ sites within the City of Centres strategy.

Table 4 below displays the final built form controls implemented within the DDO.

10.6 – ATTACHMENT 1. Planning Report

Table 4 – Proposed DDO Built Form Controls

| Property | Preferred maximum height | Preferred setback to Mahoneys Road | Preferred minimum rear* setbacks | Preferred and mandatory minimum side* setbacks, where specified | Built form outcome |
|---|--------------------------|--|--|--|---|
| 69-79 Mahoneys Road, Forest Hill | 17m | 0m | <p>Up to 13m building height:</p> <ul style="list-style-type: none"> 0m (preferred) <p>Above 13m building height:</p> <ul style="list-style-type: none"> 5m | 0m (preferred) | <p>The scale of development provides an appropriate interface with Mahoneys Road and adjacent residential area.</p> <p>Additional overshadowing impacts to neighbouring residential land are mitigated.</p> |
| Carpark (east side of Mahoneys Road, Forest Hill) | 21m | <p>Up to 11m building height:</p> <ul style="list-style-type: none"> 1m from the eastern edge of the footpath fronting Mahoneys Road <p>Above proposed street wall up to 18m in building height:</p> <ul style="list-style-type: none"> 3m from street wall <p>Over 18m building height:</p> <ul style="list-style-type: none"> 3m from the street wall + | <p>Where the title boundary directly interfaces Barter Crescent:</p> <p>Up to 10.5 m building height:</p> <ul style="list-style-type: none"> 5m The setback area should be heavily landscaped <p>Between 10.5 up to 15m height:</p> <ul style="list-style-type: none"> 5m plus 1m per additional 1m height above 10.5m <p>Above 15m, up to 20m height:</p> <ul style="list-style-type: none"> 10m <p>Above 20m height:</p> <ul style="list-style-type: none"> 10m plus 1m per additional 1m height above 20m | <p>Where the title boundary abuts the General Residential Zone (south):</p> <ul style="list-style-type: none"> Up to 7m building height: 4m (mandatory) Between 7m and 10.5m building height : 5m (preferred) Between 10.5m and 14m building height: 9m (preferred) Between 14m and 17.5m building height: 10m (preferred) Above 17.5m building height: 11m (preferred) <p>Where the title boundary abuts the Commercial 1 Zone (North):</p> <p>Up to 11m building height:</p> <ul style="list-style-type: none"> 0m (preferred) <p>Over 11.5m building height:</p> <ul style="list-style-type: none"> 4.5m (preferred) | <p>The scale of development provides an appropriate interface with the activity centre core and residential areas.</p> <p>East-west pedestrian link along southern property boundary and landscaping to the setback from residential property to the south.</p> <p>Development provides a consistent street wall along Mahoneys Road and design detail at south-western corner of site to reduce building bulk adjacent to pedestrian link and residential property to the south.</p> <p>Should not shadow adjoining residential land between 9am and 3pm on the September Equinox.</p> |

10.6 – ATTACHMENT 1. Planning Report

| Property | Preferred maximum height | Preferred setback to Mahoneys Road | Preferred minimum rear* setbacks | Preferred and mandatory minimum side* setbacks, where specified | Built form outcome |
|----------|--------------------------|------------------------------------|--|---|--------------------|
| | | 1m per additional 1m height | Where the title boundary abuts the General Residential Zone to the east: Up to 9m building height: <ul style="list-style-type: none"> ▪ 8m setback. The setback area should be heavily landscaped Above 9m building height: 8m plus 4m per additional 3m height above 9m | Where the title boundary abuts the General Residential Zone to the north=: Up to 7m height: <ul style="list-style-type: none"> ▪ 2m (preferred). The setback area should be heavily landscaped. Between 7m up to 14m height: <ul style="list-style-type: none"> ▪ 2m plus 1m per additional 1m of height above 7.5 m (preferred) Above 14m, up to 20m height: <ul style="list-style-type: none"> ▪ 10m (preferred) Above 20m height: <ul style="list-style-type: none"> ▪ 10m plus 1m per additional 1m height above 20m (preferred) | |

Other Requirements of the DDO

Aside from the built form controls detailed above, the DDO includes several requirements targeting the facilitation of high-quality design outcomes on both the subject sites. These are largely the same as that provided within DDO4 (Neighbourhood Activity Centres) in the City of Whitehorse Planning

Scheme, with some alterations where relevant to the specifics of the subject site.

Design Objectives

10.6 – ATTACHMENT 1. Planning Report

The DDO further provides guidance with regard to the design of future development, incorporating the following design objectives:

- *To ensure new development is designed to facilitate a lively, attractive and safe local activity centre, and assist in improving its economic viability.*
- *To ensure new development is designed to respond to the immediate site environs, reflect the role of the centre and enhance the character of the surrounding residential area.*
- *To ensure new buildings incorporate design detail that provides a high quality and visually interesting interface with the streetscape and the surrounding residential area, addressing issues of amenity, functionality, adaptability and accessibility.*
- *To ensure new buildings create a complementary interface to enhance the public realm.*
- *To ensure an appropriate landscaped response to adjacent residential interfaces and streets.*

The above are derived from DDO4 with the final design objective a new objective.

The above design objectives, in tandem with the Decision Guidelines stipulated within the Schedule will ensure any future development must provide a design response that is appropriate and reflective of the overarching built form character of the site's surrounds.

Buildings and Works

With regard to buildings and works requirements, the proposal seeks to facilitate high quality design and built form outcomes as per the Table to the Schedule. Furthermore, we note that the schedule does not provide any specific exemptions for buildings and works permission requirements – consistent with DDO4.

The DDO provides requirements regarding building heights which are consistent with DDO4, outline that the preferred maximum building heights

should not be exceeded, other than when applications demonstrate how the developments achieves the Design Objectives and Built Form Outcomes of the schedules.

The schedule also includes a building design requirement stipulating that:

“New development should provide for high quality design, finishes and building articulation”.

Setback requirements under the buildings and works provision are largely consistent with DDO4, with only minor alterations made to wording resulting from site-specific qualities (i.e 'properties abutting a residential zone' altered to 'title boundaries abutting a residential zone'). With the addition:

“Buildings should provide a landscaped setback where they abut adjacent residential areas and streets”.

In addition, the schedule includes a provision relating to access and car parking, including that:

- *“New development should provide for safe and appropriate access to Mahoneys Road and abutting laneways.*
- *New development must maintain the pedestrian connection between Mahoneys Road and Barter Crescent, in accordance with [Map 01] and Clause 4.0 of this Schedule.*
- *Vehicle access and movement from Barter Crescent should be discouraged.”*

Given the unique context of the subject sites, as it relates to adjacent laneways and roads, the above provisions were deemed necessary to ensure the facilitation of developments that appropriately consider the context of the subject sites.

With regard to vehicular access, the DDO clearly defines the carpark site's boundary to Barter Crescent as a rear frontage, which is reflected in the built form controls. Moreover, the DDO stipulates the outcome of minimising vehicular access via Barter Crescent, noting that a future development application seeking vehicular access via Barter Crescent will undergo

10.6 – ATTACHMENT 1. Planning Report

appropriate assessment with regard to safe access and traffic impacts. The office site will continue to benefit from vehicle access via the existing public laneway.

The retention of the pedestrian connection is additionally dictated through a mandatory permit condition, detailed further below.

Subdivision

We have not sought to provide any specific requirements above the Commercial 1 Zone in relation to subdivision.

Mandatory Permit Condition

A key consideration of the proposal includes the maintenance of pedestrian access through the site, connecting Barter Crescent in the east to the activity centre core.

The existing pedestrian walkway is located within the carpark site, along the southern title boundaries. In developing the DDO control, we have been cognisant of the importance of this pathway in providing pedestrian permeability and access between eastern residential land and the Forest Hill Chase Activity Centre.

As such, the DDO includes a mechanism to ensure any future development on the carpark site must retain this pedestrian pathway. The following requirement is included within the DDO:

“In deciding to grant a permit for the development of part of the carpark site, the Responsible Authority must include a condition that requires the owner of the land to enter into an agreement with the responsible authority under Section 173 of the Act regarding the retention of the southern pedestrian pathway connecting Barter Crescent to Mahoneys Road, to the satisfaction of the Responsible Authority”

Signs

We have not sought to provide any specific requirements above the standard controls in relation to signage.

Application Requirements

Planning Report

We have not sought to provide any specific requirements above the standard DDO controls in relation to signage.

Decision Guidelines

The Decision Guidelines provided within the proposed DDO match those from DDO4 and are as follows:

- *An application to exceed the preferred maximum building height and/or vary the preferred setbacks must demonstrate how the development will achieve the Design Objectives and Built Form Outcomes of this schedule, and any local planning policy requirements;*
- *Whether the proposed buildings or works achieve high or superior architectural quality;*
- *Whether the proposed buildings or works achieve innovation with respect to environmental sustainability;*
- *Whether the proposed buildings or works involve innovative approaches to heritage fabric;*
- *Whether the proposed buildings or works achieve an equivalent or better design outcome;*
- *Whether the proposed buildings or works minimise impacts on amenity;*
- *Whether the design response to the site context, including heights and setbacks on adjoining properties and existing neighbourhood character; and*
- *Whether the proposed buildings or works achieve specific urban design outcomes such as vista retention, protection of the public realm from excessive shadowing, transition to other land, or achieving specific land use outcomes.*

These guidelines are appropriate in ensuring any future development on the sites incorporates an appropriate consideration of surrounding and internal amenity and implements a high quality design that is coherent with the existing neighbourhood character and implements ESD initiatives and a

Assessment

33

10.6 – ATTACHMENT 1. Planning Report

healthy landscaping provision in keeping with the existing character of the area.

Notice Requirements

The overarching Design and Development Overlay schedule includes the following with regard to applications for subdivision and buildings and works:

A schedule to this overlay may specify that an application is exempt from the notice requirements of section 52(1)(a), (b) and (d), the decision requirements of section 64(1), (2) and (3) and the review rights of section 82(1) of the Act.

No exemptions are proposed within this specific DDO schedule, ensuring that neighbouring properties are provided with ample opportunity to review and make submissions with regard to future applications for buildings and works or subdivision.

4.2 Built Form/Interface Considerations

As detailed throughout this report, the proposed built form controls introduced within the DDO are informed by substantive analysis of amenity preservation to surrounding residential land as well as strategic policy both at state and local level.

Controls seeking to manage massing have been primarily informed by amenity testing and the State Government's City of Centres strategy,

Moreover, the proposed controls are consistent with similarly sized activity centres in Whitehorse, which contemplate building heights and massing outcomes similar to those included in the proposed DDO, such as Schedule 4 to the DDO within the Whitehorse Planning Scheme, which contemplates building heights up to 21.5m (6 storeys).

The use of strategic policy and guidance, coupled with amenity testing undertaken by NH Architecture, will inform highly appropriate and considerate built form outcomes on the sites, that effectively balances the activity centre context within which the sites are located with sensitivity of adjacent residential land.

As shown in the indicative architectural imagery prepared by NH Architecture, a built form envelope built to the full extent allowed under the discretionary controls, does not result in unreasonable overshadowing to neighbouring residential land. Moreover, when viewed from the surrounding residential streetscape, the development presents positively and as a sympathetic transition to the activity centre core.

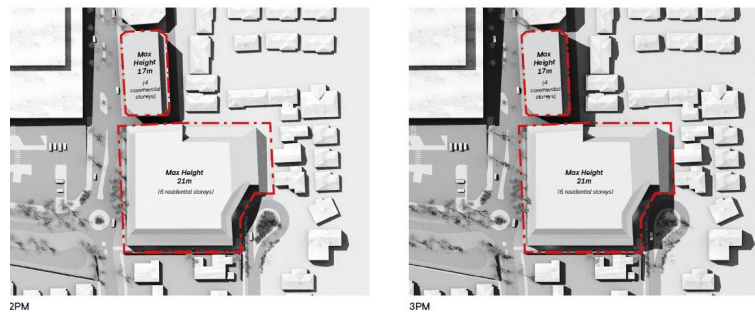
10.6 – ATTACHMENT 1. Planning Report

Figure 11 September Equinox Shadow Outcomes (Max. Allowable Building Envelope)



Source: NH Architecture, 2026

Figure 12 September Equinox Shadow Outcomes (Max. Allowable Building Envelope)



Source: NH Architecture, 2026

With specific regard to the carpark site, the controls have been designed to position the most considerable built form to the northwestern portion of the site, through the incorporation of appropriate setbacks to residentially zoned land, and the integration of a street wall to strengthen a future building's presentation to the activity centre core.

We further note that adjacent residential land is zoned General Residential Zone (GRZ) which contemplates heights of up to three storeys. The office site is provided a maximum height control of 4 storeys, which appropriately acknowledges its commercial zoning and positioning within the activity centre, whilst remaining sympathetic to residential land east adjacent.

With regard to the carpark site, a 21m building height is proposed, recognising the size of the site and its capacity to allow for more intensified built form. Its interfaces with residential land are effectively managed through setback controls, which ensure that no unreasonable amenity impacts are posed to neighbouring residential land. Southern and eastern setback controls to the carpark site have been purposefully tweaked to ensure no shadowing to adjacent residential land, as well as the maintenance of the southern pedestrian link.

Figure 13 Concept Viewline – Mahoneys Road



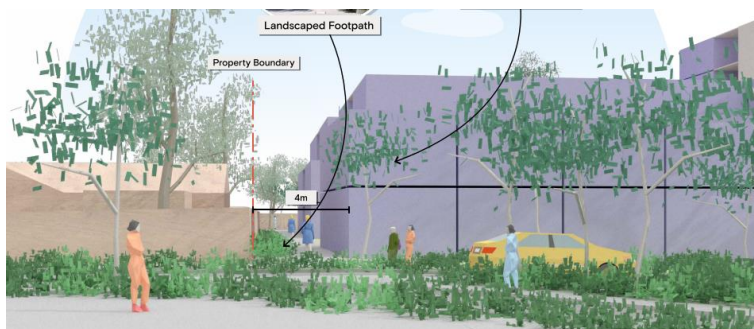
Source: NH Architecture, 2026

The DDO further places emphasis on the provision of landscaping, where appropriate, to ensure any future built form is appropriately softened and presents positively to the surrounding public realm. As dictated by the Decision Guidelines, a future development proposal on site will be assessed for its quality in design and integration of landscaping, to ensure a future

10.6 – ATTACHMENT 1. Planning Report

development does not detract from the existing neighbourhood character, but rather aligns and builds upon it.

Figure 14 Concept Viewline – Barter Crescent



Source: NH Architecture, 2026

4.2.1 Urban Design Assessment

The following section provides a detailed assessment of the following interface responses and the rationale for built form controls that have been proposed:

- Office – rear interface;
- Carpark – northern interface;
- Carpark – eastern interface;
- Carpark – southern interface; and
- Carpark – Barter Crescent interface.

Office – rear interface

As discussed above, 69-79 Mahoneys Road contains a 3 storey office built sheer along the eastern (rear) boundary. Directly adjacent is a 5.3m wide

Council laneway. 5 dwellings are positioned on the opposite side of the laneway that form part of the broader retirement estate, including:

- 23 Lyell Walk – one half of a duplex with a rear backyard opposite the north-east corner of the office site;
- 19 & 21 Lyell Walk – a duplex comprising rear backyards opposite the office site;
- 15 & 17 Lyell Walk – each comprising a double storey dwelling with rear backyard opposite the office site;

The southern edge of the retirement estate comprises a driveway terminus, footpath to the carpark and an established gum tree.

Figure 15 Office site - rear interface



Source: Nearmap, Urbis

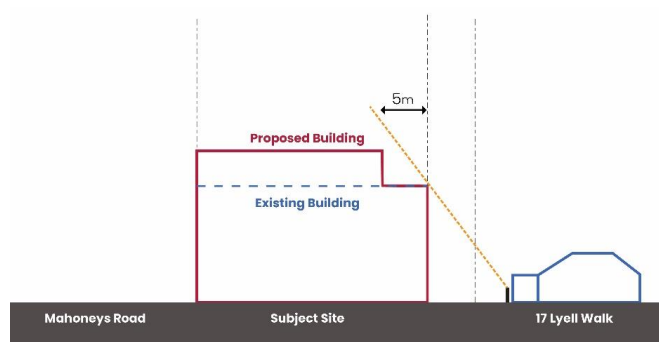
The proposed DDO provides for a 4 storey / 17m development on the office site. However, it is important to ensure this additional height does not result in any further amenity impacts on neighbouring residential properties on the opposite side of the lane. Therefore, proposed controls include the provision for an upper level setback to the top level. Shadow testing has confirmed that an upper level setback of 5m is required to ensure no additional

10.6 – ATTACHMENT 1. Planning Report

shadow is cast to the neighbouring lots. As this is proposed as a discretionary control, a specific built form outcome has been included to this effect (as set out in Table 4).

Proposed DDO Buildings and works requirements for 'Building setbacks' states '*recessed upper levels are preferred to reduce the appearance of building bulk*'. Along this interface, the top level is proposed to set back 5m above the existing building as outlined above. This is considered to be appropriate to ensure any form above the existing office building is hidden in views from the opposite SPOS, illustrated below.

Figure 16 Office site – section diagram through rear interface



Source: Urbis

Carpark – northern interface

The carpark is a large 5,800m² allotment. Its northern interface can be described as follows:

- To the north-west, approximately one-third of the frontage interfaces the office/Council laneway;
- In the centre, approximately one-third (the middle third) of the frontage interfaces the driveway terminus area of the retirement estate; and
- To the north-east, the balance of the frontage (22.4m) interfaces the rear backyard of 13 and 11 Lyell Walk.

Figure 17 Carpark site – northern interface



Source: Nearmap, Urbis

As illustrated above, the most sensitive part of this interface is where the rear backyards of 13 and 11 Lyell Walk interface the carpark.

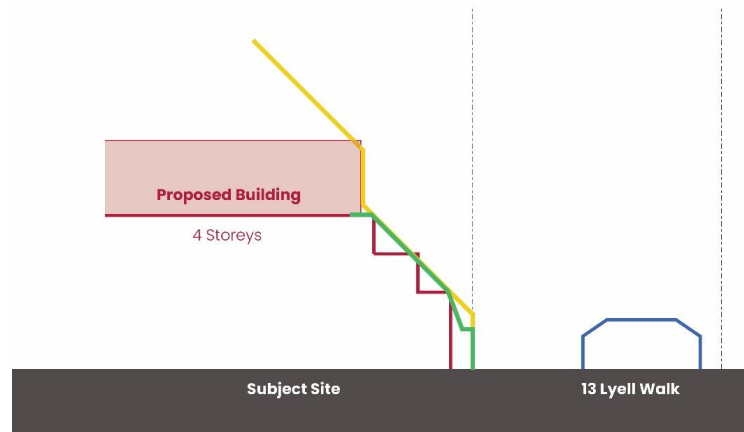
Where directly interfacing 11 Lyell Walk, the proposal will be largely setback due to the 8m landscaped setback proposed to the rear of the site. A 2-storey element will be visible in part in direct views. As a result, the visual bulk impacts from this backyard will be minimal.

The section below demonstrates the visual impact from the backyard of 13 Lyell Walk. This backyard will interface the 2-storey podium element, with a

10.6 – ATTACHMENT 1. Planning Report

3rd and 4th level setback above and recessive in views, in line with Rescode and CoC setback profiles. Importantly, the taller part of the envelope does not directly interface 13 Lyell Walk. It is proposed to be massed away from this interface, avoiding any direct views. This is considered to be an acceptable response to 13 Lyell Walk.

Figure 18 Carpark – section diagram through 13 Lyell Walk (Rescode Standard B2-3.1 in green, and CoC building profile in yellow)

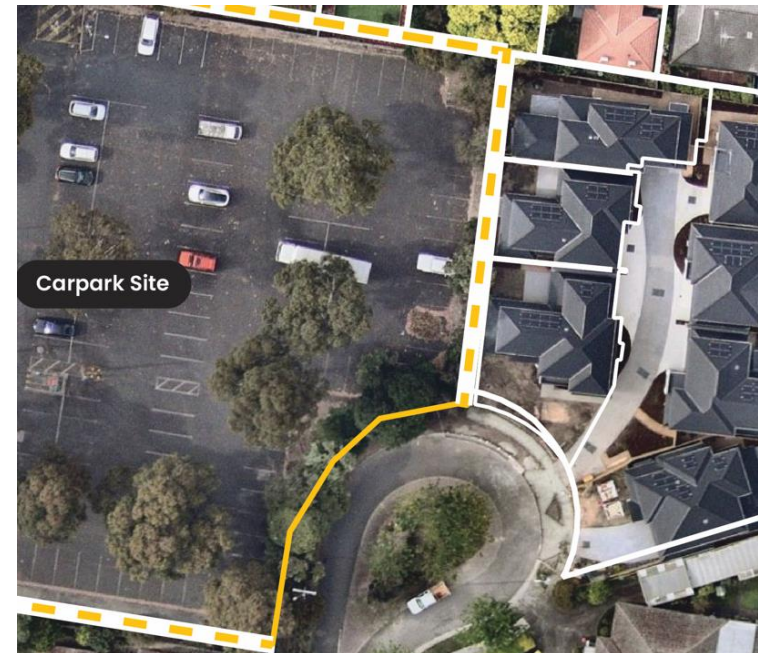


Source: Urbis

Carpark – eastern interface (residential interface)

The northern half of the eastern interface contains 3 double storey dwellings comprising small SPOS areas facing the carpark site. Each dwelling is set back at least 1m from the shared boundary with the site. The southern half of the eastern interface fronts Barter Crescent, discussed below.

Figure 19 Carpark site – eastern interface



Source: Nearmap, Urbis

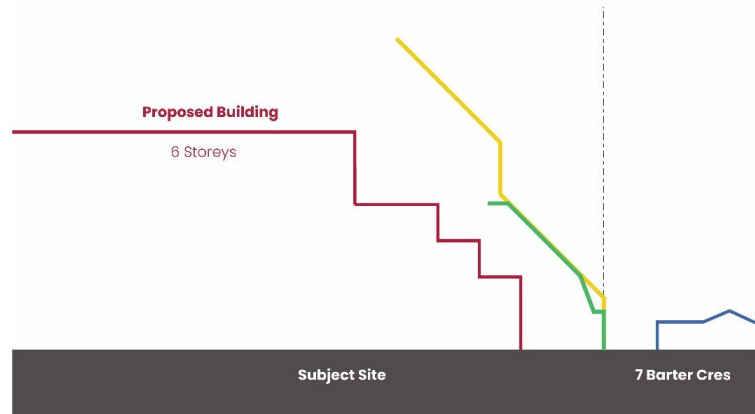
10.6 – ATTACHMENT 1. Planning Report

From the east, the proposal provides a generous 8m rear setback that is sought to be landscaped, as reinforced through the proposed Design Objectives. Above, the proposal rises 2 storeys, with a 3rd and 4th level each setback by an additional 4m. The proposed 6 storey element is setback a total of 24m from the eastern boundary.

As demonstrated in shadow diagrams, this will ensure that the proposal will generally avoid any overshadowing to neighbouring SPOS between 10-3pm at the September equinox.

The section below illustrates how the inclusion of these generous setbacks from the rear will ensure any visual bulk impacts are well within the expectations of Rescode and the CoC setback profile. This is considered to be an acceptable response to properties at 7 Barter Crescent.

Figure 20 Carpark – section diagram through a dwelling at 7 Barter Crescent (Rescode Standard B2-3.1 in green, and CoC building profile in yellow)



Source: Urbis

Carpark – southern interface

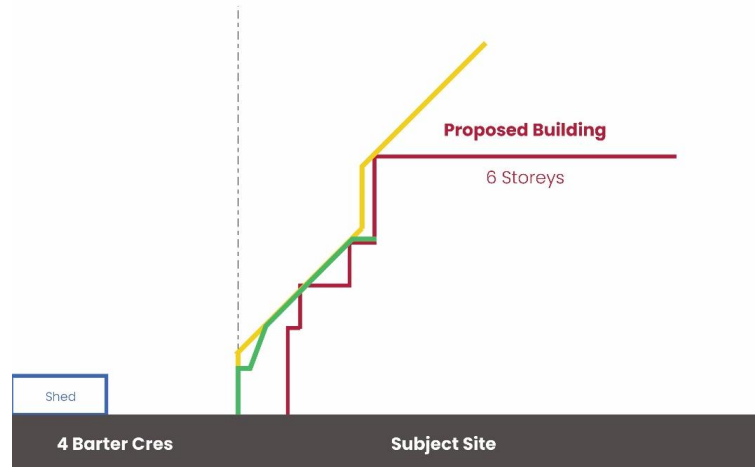
Along its southern interface, the carpark site includes a pedestrian through-link sought to be retained in the proposed DDO to connect Mahoneys Road and Barter Crescent. Interfacing to the south, the site abuts 99 Mahoneys Road containing a commercial use (podiatrist) and a dwelling at 4 Barter Crescent with a rear SPOS.

The proposal provides for a 4m setback at Ground level to maintain the pedestrian link. This is a supported outcome from an urban design perspective that supports neighbourhood permeability. Above, the proposed envelope rises incrementally to match the CoC building profile as shown in the section below. This is considered an appropriate setback profile to follow, which ensures an appropriate balance between delivering housing growth on a large and rare opportunity site such as this, and providing neighbouring dwellings with good amenity. Further, while exceeding Rescode B2-3.1, it is important to note that the primary outlook of this rear backyard is to the west, which will remain open to sky views.

Shadow diagrams produced by NH architecture confirm that the proposed envelope will generally avoid any overshadowing to neighbouring SPOS for a minimum of 5hrs between 9am-3pm at the September equinox. This will be reinforced by proposed Built form outcomes.

10.6 – ATTACHMENT 1. Planning Report

Figure 21 Carpark site – Southern interface (Rescode Standard B2-3.1 in green, and CoC building profile in yellow)



Source: Urbis

Carpark – Barter Crescent

The carpark site has a 45m rounded interface to the turning circle at the terminus of Barter Crescent. It is the only landholding that is zoned C1Z along Barter Crescent. Residential properties along Barter Crescent are zoned GRZ and generally have front setbacks between 5-8m, with some 4.5m setbacks found. The dwelling at 4 Barter Crescent directly south includes a pergola structure setback 4m and a carport within the front setback.

While there is a need to respond to the residential context of the street, it is important to note that the carpark site has a very different context, as it:

- is a substantially large site compared to all other lots along Barter Crescent;
- is zoned C1Z;
- has a 45m frontage to the street;
- sits at the terminus of the street; and
- has a rounded interface which is a much different context to if it were located mid-block.

In this context, a front setback of 5m has been introduced for the Site. This is considered an appropriate response that transitions from neighbouring dwellings while responding to its commercial zone, strategic location and other contextual considerations listed above.

Setbacks of 4m from the south, and 8m from east help to create separation and transition to neighbouring properties. From the south, the pedestrian link earmarks a distinct change in character between zones. From the east, generous landscaping will help soften the transition.

The proposed envelope proposes to rise 3 storeys, then stepping back above. The 3 storey front wall is entirely appropriate in this context, and sits in line with the GRZ zoning of the neighbouring properties. The 3 storey front wall will help to limit views to upper form setback above.

On this basis, it is considered that the proposed envelope responds appropriately to Barter Crescent and its C1Z context.

10.6 – ATTACHMENT 1. Planning Report

4.3 Traffic Considerations

The proposal is supported by a Transport Impact Assessment (TIA) prepared by Eukai.

The TIA provides an assessment with regard to the removal of the at-grade carpark, considering potential impacts on car parking availability for the Centre and surrounding area, the Centre's compliance with the statutory car parking requirements (stipulated within the DPO) against the reduced car parking provision and the appropriateness of car parking outcomes on the sites being subject to Clause 52.06 of the Whitehorse Planning Scheme.

The assessment utilised survey data collected by Eukai to assess relevant car parking for the Centre and surrounding area.

The TIA outlines that removal of the DPO from the subject sites did not result in unreasonable outcomes with relation to traffic, concluding:

- *There are ample vacancies in the existing main centre car park to accommodate any displaced car parking demands associated with the removal of the Mahoneys Road off-street car park.*
- *Despite there being a car parking shortfall against the DPO car parking requirement for the main centre car park following the removal of the Mahoneys Road car park, there is a surplus of car parking against Clause 52.06 car parking requirements.*
- *Accordingly, it is considered appropriate to remove the "at least 130 car spaces to the east of Mahoneys Road" car parking requirement from the DPO.*
- *The adoption of the default Clause 52.06 car parking rates for the two sites to the east side of Mahoneys Road is considered appropriate.*

Moreover, the TIA assesses potential access outcomes for both sites, concluding that while primary vehicle access to any future development is recommended to be via Mahoneys Road, prohibition of a secondary vehicle access to Barter Crescent is not required, as any future access via this interface would require an assessment against the requirements of the Planning Scheme, including impacts to surrounding residential land.

As such, it is deemed overly imposing for this to be disallowed outright within the DDO, as due consideration will be applied in connection with a future planning application, as to a proposed access from Barter Crescent. Notwithstanding, the DDO does outline the requirement that primary access should be facilitated via Mahoneys Road.

4.4 Strategic Considerations

The following subsection responds to Planning Practice Note 46: Strategic Assessment Guidelines, Ministerial Direction No 11 – Strategic Assessment of Amendments (September 2025).

Why is the Amendment Required?

The amendment is needed to facilitate the redevelopment of specific sites within the Forest Hill Activity Centre and remove outdated planning controls which restrict their development.

The current Development Plan Overlay (DPO1) applying to Forest Hill Activity Centre is over 25 years old. While it has enabled a framework for development and growth of a retail centre, it limits the future development of the subject sites and does not reflect the current positioning of activity centres in providing for a broad range of uses including commercial, retail and residential development.

The existing at grade car park represents an underutilisation of well-located land within an existing activity centre, and the existing office building is aged and no longer provides appropriate commercial space.

Forest Hill Activity Centre is identified in Plan for Victoria, the State Government's planning strategy, as an Activity Centre – Housing Choice and

10.6 – ATTACHMENT 1. Planning Report

Station which are 'activity centres with sufficient public transport, facilities and services to be locations for large numbers of new homes.' The activity centre plays a strong regional retail and entertainment role for the eastern suburbs of Melbourne with Forest Hill Chase Shopping Centre at its core and has an existing bus interchange.

Plan for Victoria seeks to provide more homes in well-connected, well-serviced locations around activity centres, train stations and priority precincts. It also includes local government housing targets, with Whitehorse being allocated a target of 76,500 new homes by 2051. The amendment supports the development of activity centres as a focus for high-quality development, activity and living, including forms of higher density living.

The existing Commercial 1 Zone applying to the office and car park sites allows for retail, office and residential development. The amendment replaces the existing DPO with a new Design and Development Overlay (DDO12) that unlocks the potential of the sites to continue growth of the activity centre, whilst ensuring amenity to adjacent residential land. The DDO also retains pedestrian access between Mahoneys Road and Barter Crescent as part of any redevelopment of the car park site.

Does the Amendment Implement the Objectives of Planning in Victoria?

The amendment implements the following objectives of planning in Victoria:

(a) to provide for the fair, orderly, economic, and sustainable use, and development of land

The amendment will result in the fair, orderly, economic and sustainable use and development of the land by facilitating an outcome that improves the Forest Hill Chase Activity Centre and allows for growth and activation of this part of the centre.

(b) Provide for the protection of natural and man-made resources and the maintenance of ecological processes and genetic diversity

The sites do not have significant ecological characteristics required to be maintained however it is noted that any redevelopment of the car park site

will result in the loss of existing mature trees. The proposed DDO provides opportunities for landscaping, including for canopy trees, within setbacks from adjacent residential properties within the car park site.

(c) to secure a pleasant, efficient, and safe working, living and recreational environment for all Victorians and visitors to Victoria

The amendment introduces a new built form framework for the subject sites that actively promotes the ability of the activity centre to provide a high amenity location where people can live and have good access to employment opportunities and services.

(f) Facilitate development in accordance with the objectives

The amendment implements these objectives by removing the DPO that currently restricts development on both sites and introducing a DDO that will facilitate future development in accordance with the site context.

(ft) to facilitate the provision of affordable housing in Victoria

The amendment unlocks the potential for housing to be located on the subject sites, noting that the current DPO requires the car park site to only be used for car parking.

The proposed planning controls do not explicitly require the sites to be used for affordable housing.

(g) to balance the present and future interests of all Victorians

The amendment supports the continued growth and diversification of the Forest Hill Chase Activity Centre, whilst creating the opportunity to redevelop an aged office building and underutilised car park.

10.6 – ATTACHMENT 1. Planning Report

Does the Amendment Address any Environmental, Social and Economic Effects?

The amendment will adequately address any environmental, social and economic effects and result in a net community benefit.

Environmental

The amendment contributes to sustainable development outcomes by facilitating increased development densities on underutilised sites within an existing activity centre with good access to public transport, services and facilities and retains an existing local pedestrian link.

Any redevelopment of the car park site will result in the removal of existing mature trees however the proposed DDO will require landscaped setbacks from boundaries abutting the adjacent residential areas

Social

The amendment will result in positive social impacts through:

- allowing the potential for higher density housing within an existing activity centre providing greater housing choice in the local area;
- providing for improvement to the landscaping outcomes at each site, in fostering a greener environment within the activity centre; and
- requiring the provision of new active frontages, creating for a sense of place, and supporting the potential for commercial, retail and employment offerings.

Economic

The amendment unlocks development opportunities on the sites which will provide for economic benefits to the community including the potential for more housing near employment opportunities and services or new retail and commercial offerings.

The amendment allows for more efficient use of the shopping centre surplus car parks and opens up the subject sites for more productive uses.

Retention of underutilised at grade car parking land in an activity centre is not the best use of land.

Does the Amendment Address Climate Change?

The amendment has regard to climate change and emissions reduction targets in accordance with section 12(2A) of the Planning and Environment Act 1987 and Ministerial Direction No. 22 (Climate Change Consideration). The amendment is supported by the preparation of a *Climate Change Consideration Report*.

The amendment addresses climate change by:

- prioritising growth within an established area and in a designated activity centre to reduce transport-related emissions and take advantage of existing urban infrastructure;
- affecting land not known to be exposed to natural hazards;
- strengthening walkability through the retention of a key pedestrian link, reducing reliance on private vehicles, allowing for residents within the wider area to access the activity centre;
- introducing built form controls for future development to incorporate solar access, passive design, energy efficiency and urban cooling and greening through landscaped setbacks from adjacent residential areas providing for canopy trees and biodiversity outcomes;
- making efficient use of resources and assists in prioritising opportunities for infill housing.

Does the Amendment Address Relevant Bushfire Risk?

The subject site is not located within the Bushfire Management Overlay, nor within a designated 'Bushfire Prone Area'. The amendment will not increase the risk to life, property, infrastructure, or the natural environment from bushfire.

10.6 – ATTACHMENT 1. Planning Report

Does the Amendment Comply with the Relevant Ministerial Directions?

The amendment complies with Ministerial Direction No. 11 (Strategic Assessment of Amendments) under section 12 of the Planning and Environment Act 1987. The amendment is consistent with this direction which ensures a comprehensive strategic evaluation of a planning scheme amendment and the outcomes it produces the potential for redevelopment of underutilised land within an activity centre context.

Does the Amendment Support or Implement the Planning Policy Framework?

The amendment supports the following state provisions within the Planning Policy Framework (PPF):

- Clause 11.01-1S (Settlement) – the amendment will facilitate an outcome that provides for commercial and/or housing growth in a highly accessible area with regard to services, transport and employment, whilst preserving amenity of surrounding residential areas.
- Clause 11.01-1R (Settlement – Metropolitan Melbourne) – the amendment aligns with strategies that maintain a permanent urban growth boundary to create a more consolidated, sustainable city and supports focussing of investment and growth in activity centres.
- Clause 16.-01-1S (Housing Supply) – the amendment unlocks development potential to encourage higher density housing development on sites well located in relation to jobs, services and public transport and contribute towards the achievement of the housing target.
- Clause 17.02-1S (Business) – the amendment encourages development that meets the community’s needs for retail, entertainment, office and other commercial services.
- Clause 18.02-1S (Walking) – the amendment prioritises walking and cycling by including a requirement in the proposed DDO to retain the

existing pedestrian link from Mahoneys Road to Barter Crescent through the south boundary of the car park site.

The amendment supports local policy as follows:

- Clause 11.03-1L (Activity Centres) – the amendment unlocks development opportunity within the Forest Hill Activity Centre while ensuring the preservation of amenity to residential areas bordering the activity centre, through the implementation of appropriate built form controls.
- Clauses 15.01-2L (Environmentally sustainable development) and 16.01-1L (Housing change) – the amendment allows for the repurposing of underutilised land to enable the appropriate provision of residential land within an activity centre with excellent access to transport, services and employment.
- Clause 15.01-5L (Preferred neighbourhood character) – appropriate built form controls ensure the preservation and promotion of amenity to surrounding residential land located within the Garden Suburban Precinct 6.
- Clause 15.01-5L-01 (Tree conservation) – appropriate setbacks allow for landscaping within a future development at interfaces with the public realm and adjacent residential areas.
- Clause 18.02-1L (Sustainable personal transport) – the proposal replaces an existing car park deemed surplus to the car parking requirements of the centre and retains key pedestrian link to ensure permeability and access to the centre utilising sustainable transport modes.
- of commercial uses and/or housing choice within the immediate locality for both current and future residents (Clause 19).

10.6 – ATTACHMENT 1. Planning Report

Is the amendment consistent with the delivery of the relevant housing target set out in the Planning Policy Framework?

The amendment is consistent with the delivery of the housing target for Whitehorse set out in the Planning Policy Framework. Whilst the extent of new housing will be relatively small in the context of the total Whitehorse housing target, the location of the sites within the Forest Hill Chase Activity Centre ensures that if housing is developed on these sites, future residents will be close to jobs, services and public transport.

How does the amendment support or implement the Municipal Planning Strategy (MPS)?

The amendment is consistent with the Municipal Planning Strategy (MPS) and supports its effective implementation by unlocking underutilised land within a designated activity centre (Clause 02.03-1 - Settlement). The MPS seeks to ensure activity centres are safe, attractive and developed in accordance with their role in the network of activity centres. It envisages that any new development will improve the amenity and visual appearance of activity centres, contribute to the City's economy and respond to the needs of the community.

The amendment introduces bespoke built form requirements for the sites to respond to their specific context. The proposed building heights and setbacks consider the amenity and character of adjacent residential areas, while balancing this with their activity centre location and commercial zoning (Clause 02.03-5 - Built environment and heritage). Set back requirements allow for the provision of landscaping at the site's interfaces with the public realm and adjacent residential areas (Clause 02.03-2 - Environment and landscape values). Unlocking underutilised land within an activity centre as an opportunity for additional housing (Clause 02.03-6 - Housing) and employment (Clause 02.03-7 - Economic development) and will activate and regenerate this part of the centre.

The amendment retains the pedestrian link through the car park site supporting sustainable movement options within the centre for local residents and allows for housing near public transport, further promoting the use of sustainable transport (Clause 02.03-8 - Transport).

Does the Amendment make proper use of the Victoria Planning Provisions?

The application of a DDO to replace the existing DPO will provide greater certainty with respect to the design and form of future development on the site, in line with local and state policy. The existing DPO stifles the opportunity for development of the car park site, and does not provide any appropriate parameters for future built form.

The DDO is a well-used mechanism to ensure that the sites subject to the amendment are provided with appropriate, and site responsive planning controls.

Does the Amendment address the views of any relevant agency?

The views of relevant agencies will be sought during exhibition of the amendment.

Does the Amendment address relevant requirements of the Transport Integration Act 2010?

The amendment is consistent with and supports all the stated transport system objectives contained within the *Transport Integration Act 2010*.

10.6 – ATTACHMENT 1. Planning Report

How does the amendment have regard to the principles set out in the Yarra River Protection (Wilip-gin Birrarung murrong) Act 2017 in relation to Yarra River land and other land, the use of development of which may affect Yarra River land?

The *Yarra River Protection (Wilip-gin Birrarung murrong) Act 2017* does not apply to the subject land.

What impact will the new planning provisions have on the administrative costs of the responsible authority?

The amendment supports improved decision making for applications for development within the activity centre and will not present any additional resource or administrative costs for the responsible authority.

10.6 – ATTACHMENT 1. Planning Report

5 Conclusion

This planning scheme amendment request has been progressed to facilitate greater utilisation of land within the Forest Hill Chase Activity Centre, with development of these sites currently restricted under the Development Planning Overlay.

Under the current planning controls, the carpark site is not able to be used for anything other than for parking. This extent of car parking is not required to support the broader activity centre.

The DDO proposed has considered the sensitive interfaces of both sites and has developed requirements to respond to these interfaces within the context of them being Commercial 1 Zoned sites in an existing Major Activity Centre.

The Design and Development Overlay allows for increased density of development relative to the adjoining residential land uses – this is unashamedly an outcome of the proposal. The sites are excellently situated within the Forest Hill Chase Activity Centre. The controls allow for an intensity of development not achievable on the land to the east or south.

However, the potential for development impacts have been explicitly considered – and the controls seek particularly to limit the potential for overshadowing, and to implement height and setback controls informed by existing practice in activity centres and careful urban design analysis, to ensure a positive design outcome on both sites. Moreover, the control allows for pedestrian links to be retained between Barter Crescent and Mahoneys Road to the benefit of surrounding residents.

Planning Report



10.6 – ATTACHMENT 1. Planning Report

Disclaimer

This report is dated 20 April 2026 and incorporates information and events up to that date only and excludes any information arising, or event occurring, after that date which may affect the validity of Urbis Ltd (**Urbis**) opinion in this report. Urbis prepared this report on the instructions, and for the benefit only, of Haben (**Instructing Party**) for the purpose of planning report to support planning scheme amendment request (**Purpose**) and not for any other purpose or use. To the extent permitted by applicable law, Urbis expressly disclaims all liability, whether direct or indirect, to the Instructing Party which relies or purports to rely on this report for any purpose other than the Purpose, and to any other person which relies or purports to rely on this report for any purpose whatsoever (including the Purpose).

In preparing this report, Urbis was required to make judgements which may be affected by unforeseen future events, the likelihood and effects of which are not capable of precise assessment.

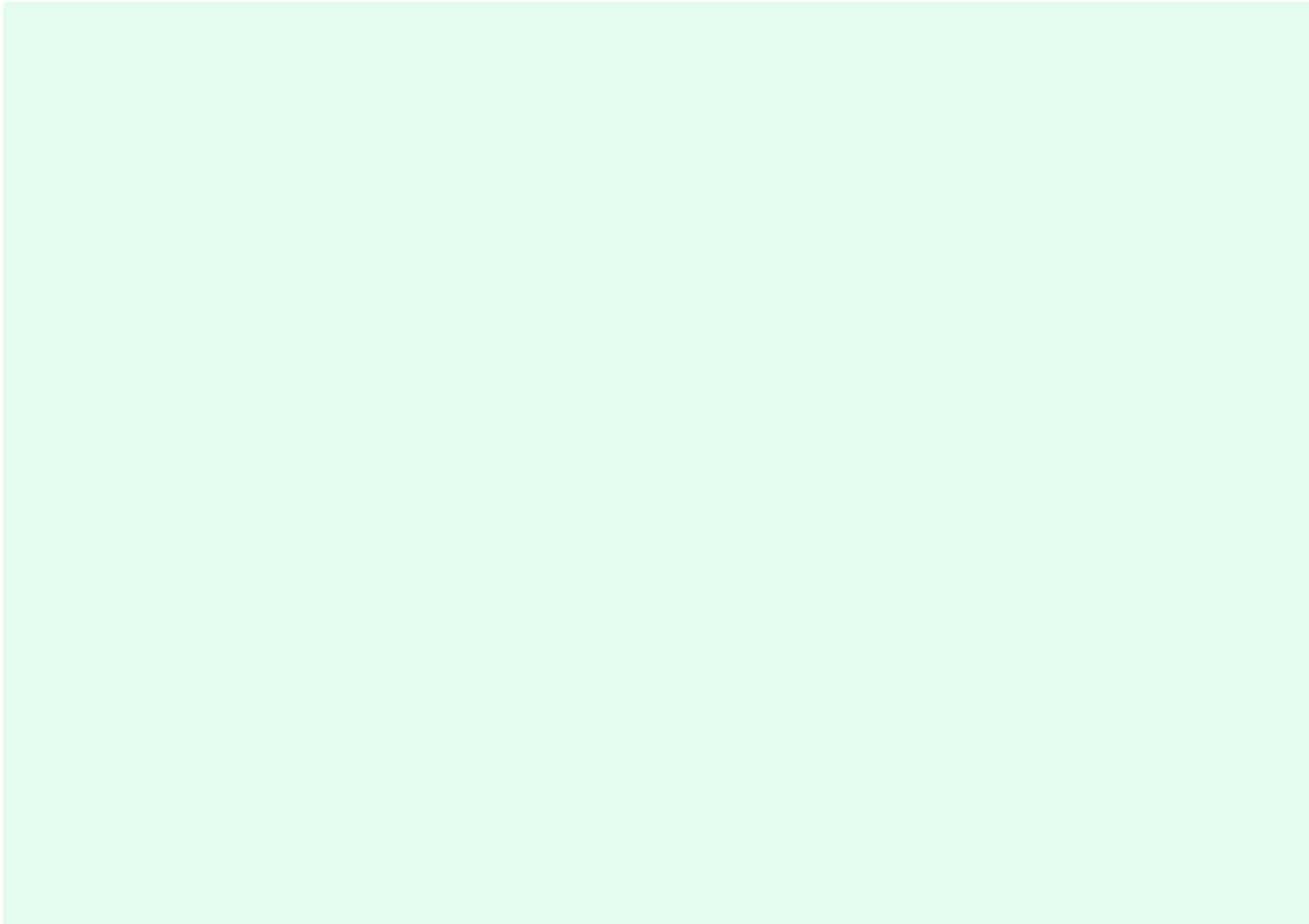
All surveys, forecasts, projections and recommendations contained in or associated with this report are made in good faith and on the basis of information supplied to Urbis at the date of this report, and upon which Urbis relied. Achievement of the projections and budgets set out in this report will depend, among other things, on the actions of others over which Urbis has no control.

In preparing this report, Urbis may rely on or refer to documents in a language other than English, which Urbis may arrange to be translated. Urbis is not responsible for the accuracy or completeness of such translations and disclaims any liability for any statement or opinion made in this report being inaccurate or incomplete arising from such translations.

Whilst Urbis has made all reasonable inquiries it believes necessary in preparing this report, it is not responsible for determining the completeness or accuracy of information provided to it. Urbis (including its officers and personnel) is not liable for any errors or omissions, including in information provided by the Instructing Party or another person or upon which Urbis relies, provided that such errors or omissions are not made by Urbis recklessly or in bad faith.

This report has been prepared with due care and diligence by Urbis and the statements and opinions given by Urbis in this report are given in good faith and in the reasonable belief that they are correct and not misleading, subject to the limitations.

10.6 – ATTACHMENT 1. Planning Report



10.6 – ATTACHMENT 1. Planning Report



Shaping cities
and communities
for a better future.

10.6 – ATTACHMENT 1. Planning Report

Appendix
A

Amendments to Forest Hill
Chase Development Plan

10.6 – ATTACHMENT 1. Planning Report

**A.1 Existing Forest Hill Chase Development
Plan**

10.6 – ATTACHMENT 1. Planning Report

THIS PLAN IS APPROVED PURSUANT TO CLAUSE 43.04 SCHEDULE 1 OF THE WHITEHORSE PLANNING SCHEME IN ACCORDANCE WITH THE COUNCIL RESOLUTION OF 25 AUGUST 2003. THIS PLAN FORMS PART OF THE DEVELOPMENT PLAN FOR THE FOREST HILL ACTIVITY CENTRE

PAGE 1 OF 17 24/11/03
 DATE

 SIGNATURE FOR THE RESPONSIBLE AUTHORITY



FOREST HILL CHASE ACTIVITY CENTRE

Summary of Floor Space

Project No. 399067
 20 August 2003

| Tenancy Type | Existing as Depicted on Development Plan 11.09.00 | Proposed Additional Floor Space As Per Development Plan 11.09.00 | Additional Floor Space Proposed by June 2003 Development Plan | Total FHCAC Proposed Extensions |
|-----------------------------|---|--|---|---------------------------------|
| RETAIL | | | | |
| Major Tenants | 30,822m ² | 9,458m ² | | 40,280m ² |
| Specialty | | | | |
| • Forest Hill Chase | 18,241m ² | 2,879m ² | | 21,120m ² |
| • East Side Mahoneys Road * | 2,500m ² | - | | 2,500m ² |
| Total Retail | 51,563m² | 12,337m² | | 63,900m² |
| NON-RETAIL | | | | |
| Forest Hill Chase | 5,729m ² | 3,953m ² | | 9,682m ² |
| East Side Mahoneys Road * | 4,500m ² | - | 652m ² | 5,152m ² |
| Total Non-Retail | 10,229m² | 3,953m² | 652m² | 14,834m² |
| Total Floor Space | 61,792m² | 16,290m² | 652m² | 78,734m² |

* 'Existing' figures derived from attached 'Table 1 Forest Hill Activity Centre' as exhibited for Amendment L31 to the Nunawading Planning Scheme.

PLANNING SCHEME AMENDMENT

SEPTEMBER 2003
 SUMMARY OF FLOOR SPACE

FOR:
 PACIFIC SHOPPING CENTRES AUSTRALIA PTY LTD

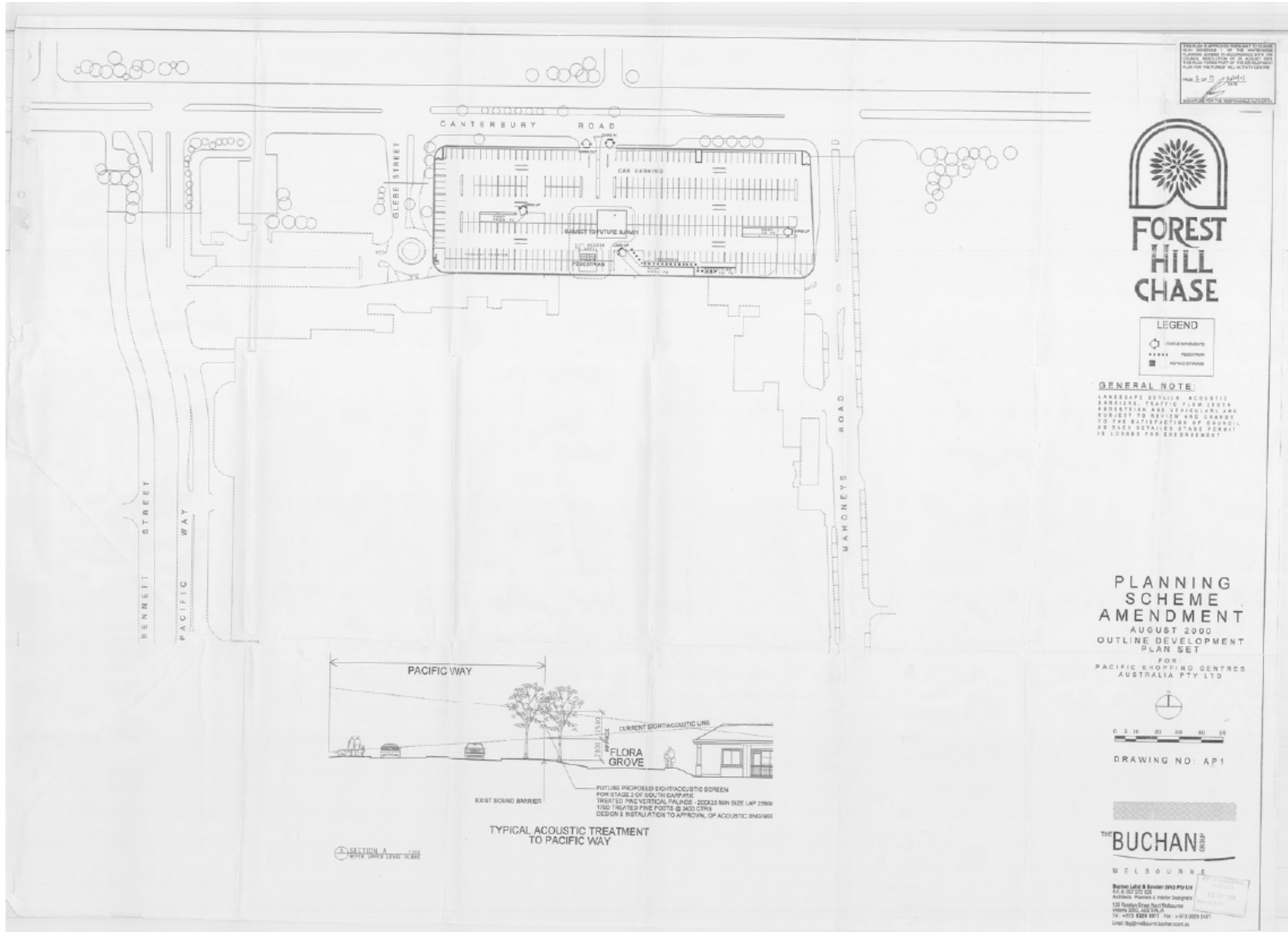
DRAWING NO: AP 16

THE BUCHANAN GROUP

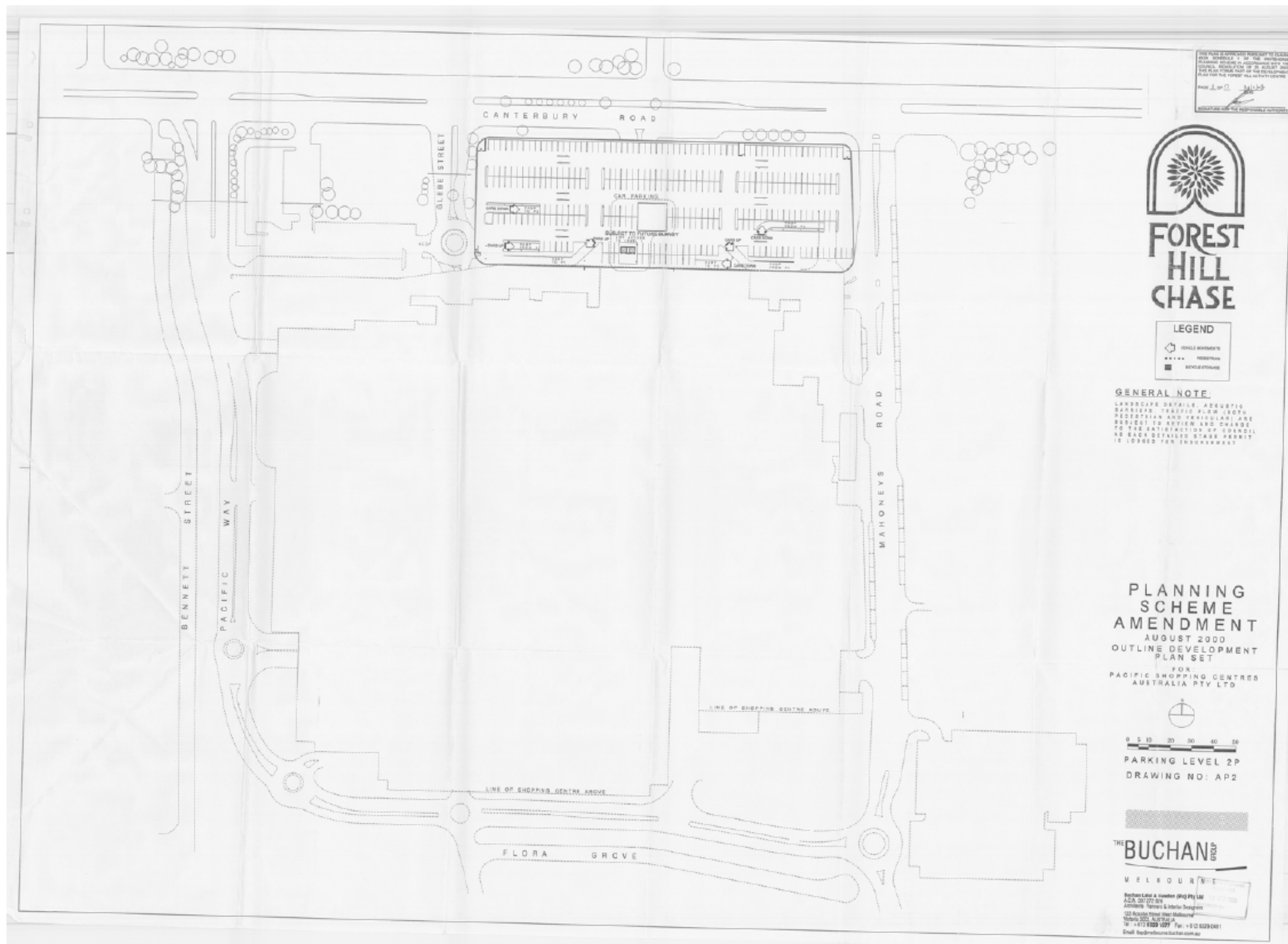
MELBOURNE

Buchanan Label & Review (Vic) Pty Ltd
 A.C.N. 607 877 205
 Architects, Planners, Engineers, Designers
 133 Bourke Street, Melbourne
 VIC 3000, AUSTRALIA
 Tel: +61 3 8228 1077 Fax: +61 3 8228 0441
 Email: bpl@melbourne.buchanan.com.au
 © Buchanan Group 2003

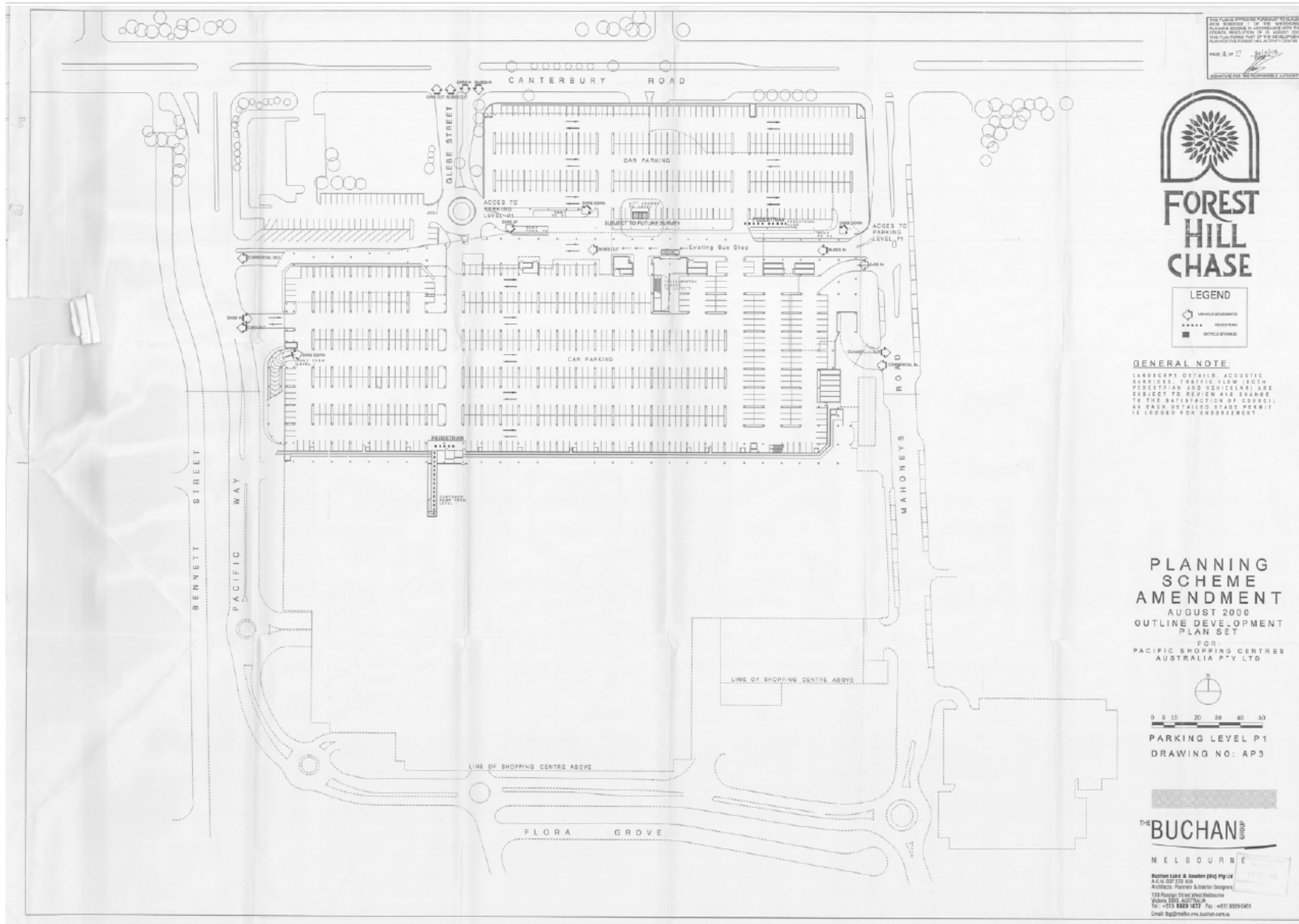
10.6 – ATTACHMENT 1. Planning Report



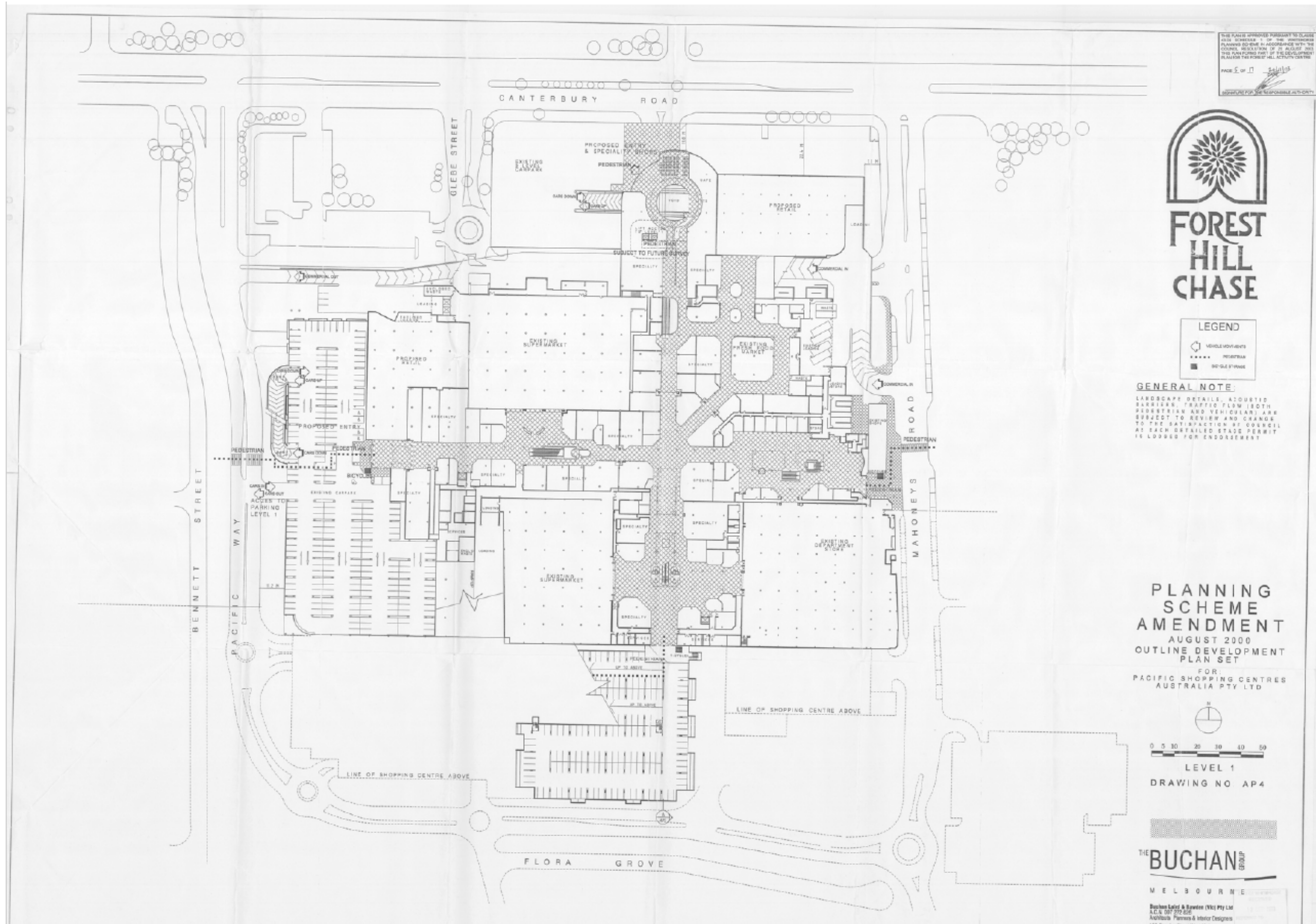
10.6 – ATTACHMENT 1. Planning Report



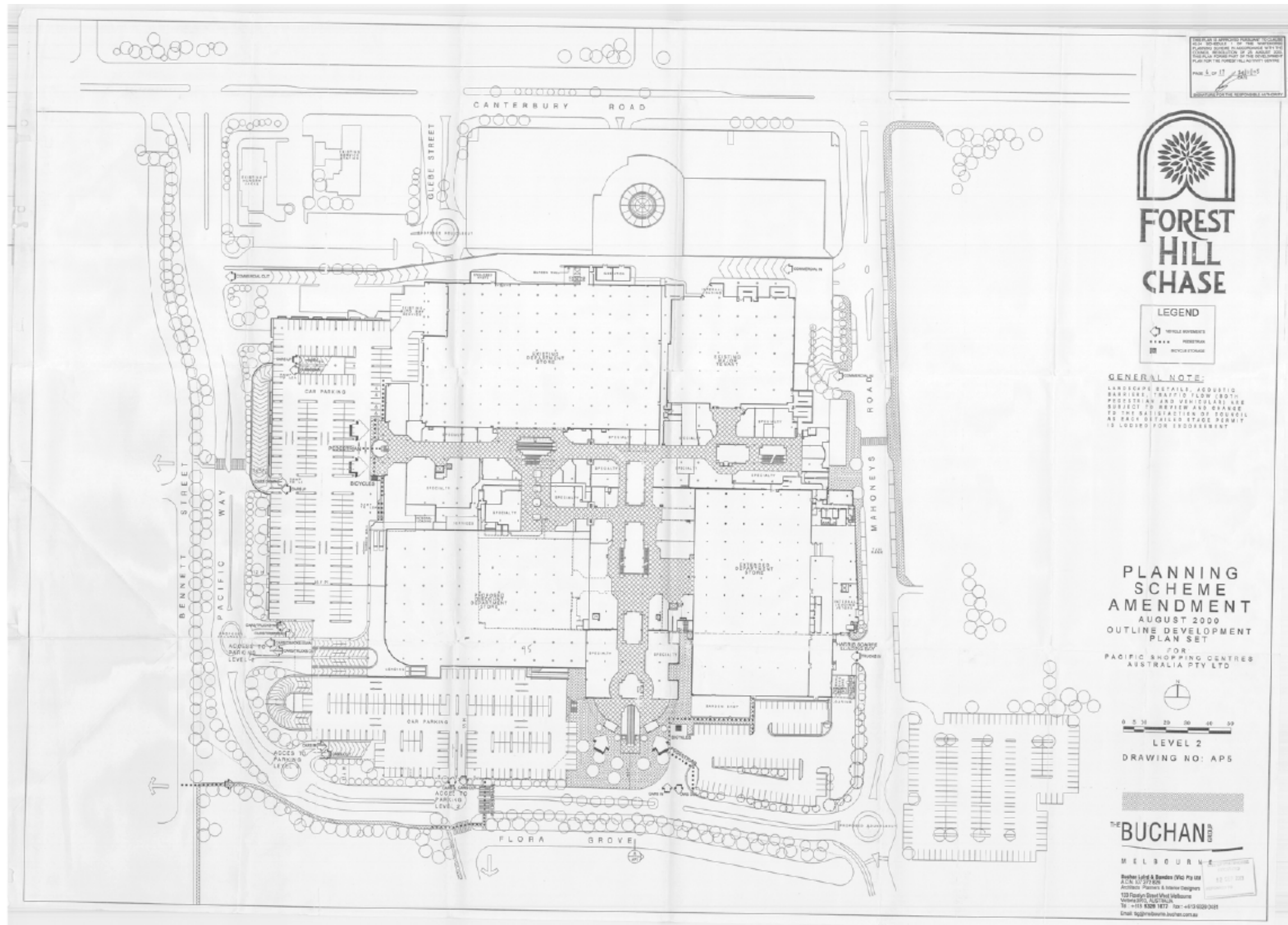
10.6 – ATTACHMENT 1. Planning Report



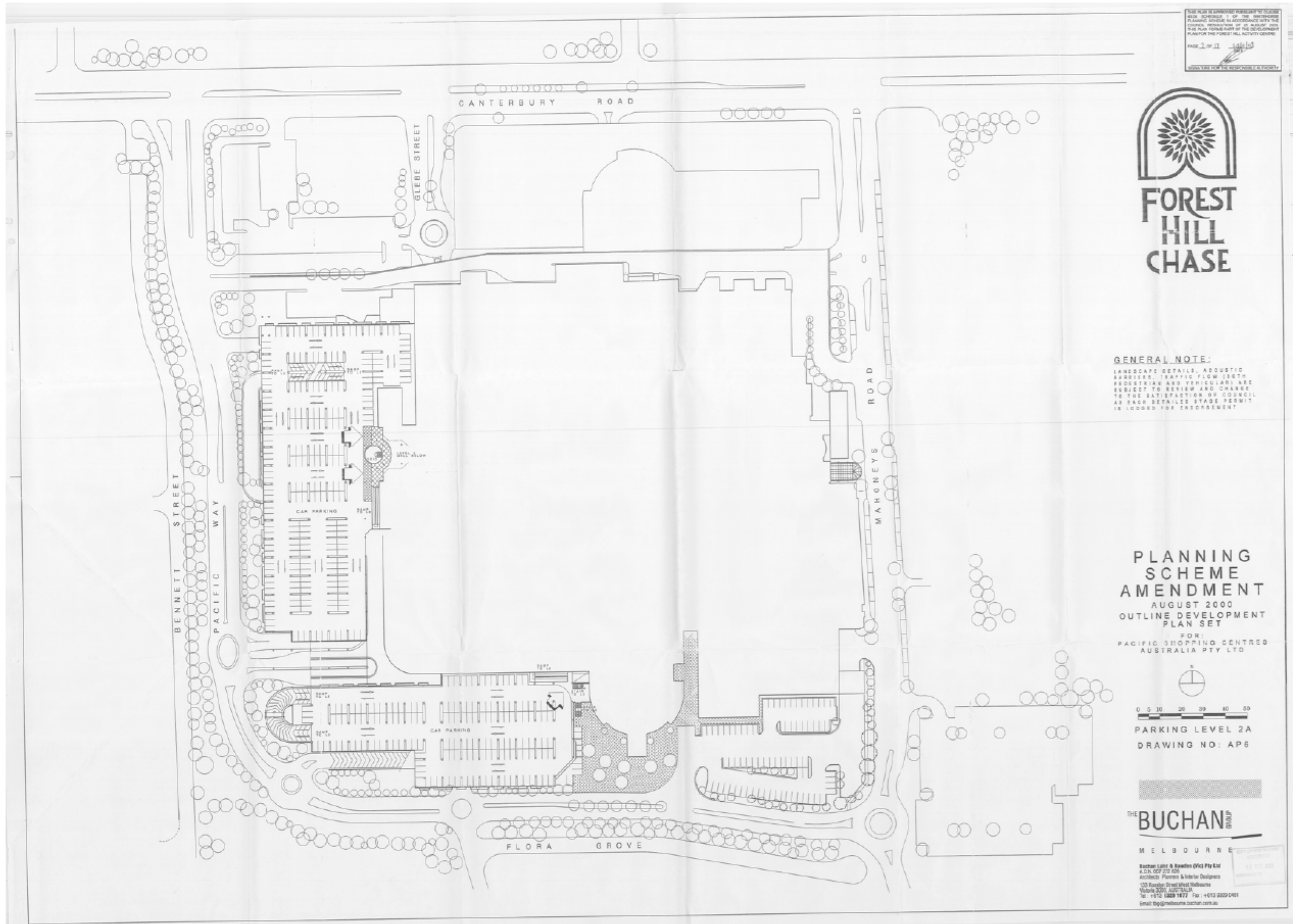
10.6 – ATTACHMENT 1. Planning Report



10.6 – ATTACHMENT 1. Planning Report



10.6 – ATTACHMENT 1. Planning Report



THIS PLAN IS APPROVED FOR SUBMITTAL TO THE COUNCIL AS A PLANNING SCHEME AMENDMENT IN ACCORDANCE WITH THE FOREST HILL CHASE DEVELOPMENT PLAN FOR THE FOREST HILL ACTIVITY CENTRE.

PAGE 1 OF 11

DATE: 14/08/2000

SCALE: 1:1000



GENERAL NOTE:
 LANDSCAPE DETAILS, ACoustic BARRIERS, TRAFFIC FLOW (BOTH PEDESTRIAN AND VEHICULAR) ARE SUBJECT TO REVIEW AND CHANGE TO THE SATISFACTION OF COUNCIL AT EACH DETAILED STAGE PERMIT IS LOGGED FOR ENFORCEMENT

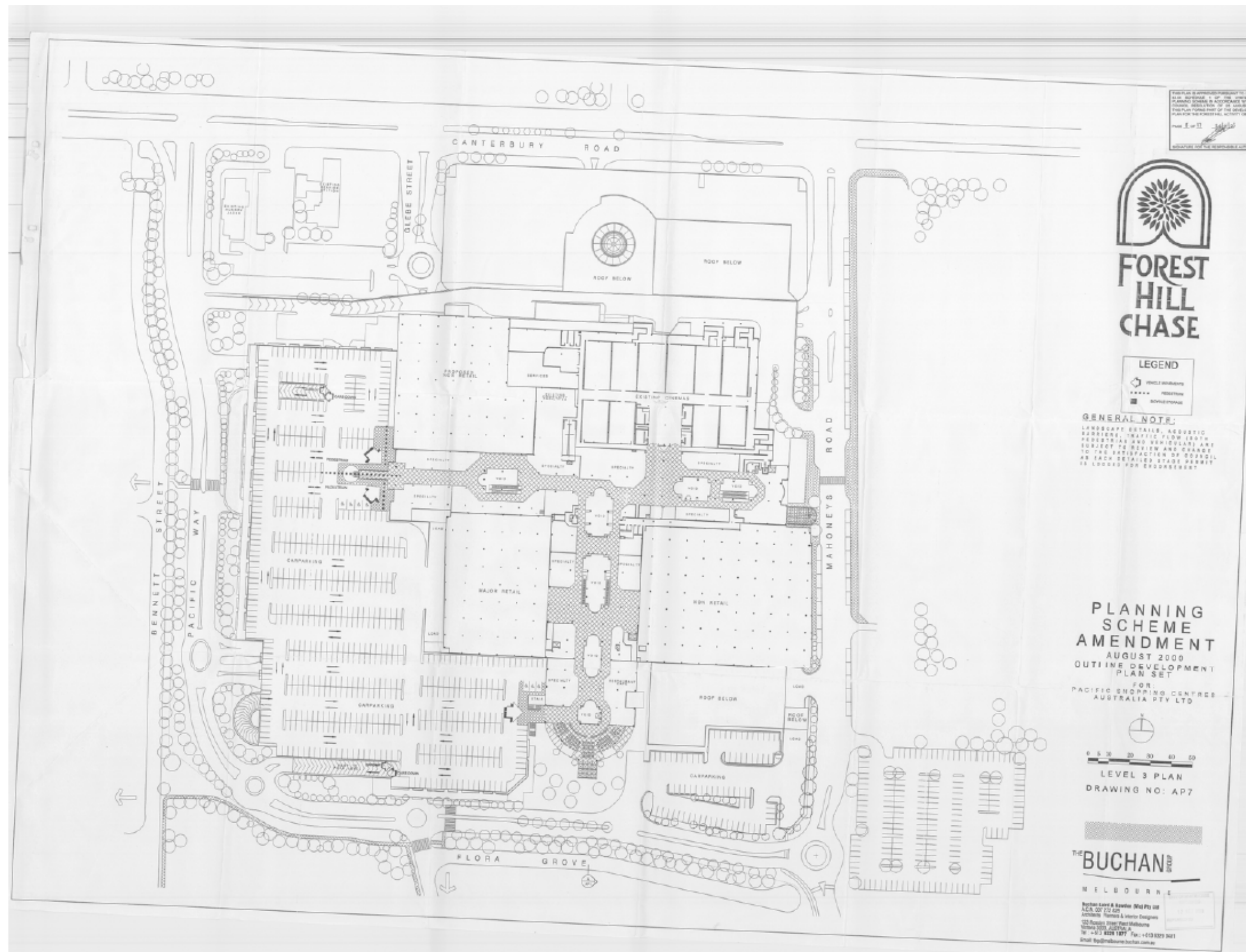
PLANNING SCHEME AMENDMENT
 AUGUST 2000
 OUTLINE DEVELOPMENT PLAN SET
 FOR:
 PACIFIC SHOPPING CENTRES AUSTRALIA PTY LTD

0 5 10 20 30 40 50
 PARKING LEVEL 2A
 DRAWING NO: AP6

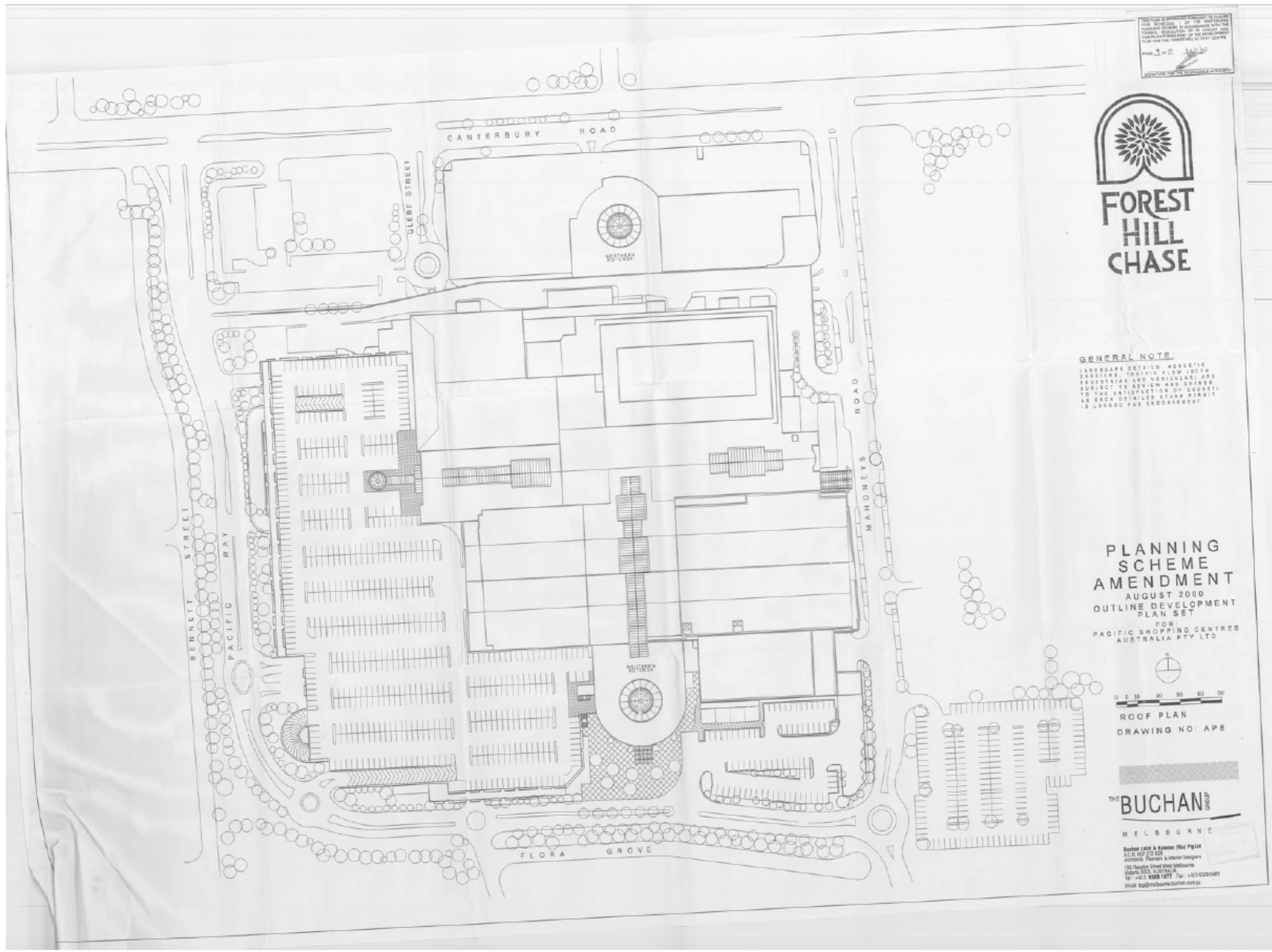
THE BUCHAN GROUP

MELBOURNE
 Buchanan Light & Sound (Vic) Pty Ltd
 A.C.N. 007 272 426
 Architects, Planners & Interior Designers
 123 Bouverie Street West Melbourne
 Victoria 3010 AUSTRALIA
 Tel: +612 8288 1877 Fax: +612 8288 0481
 Email: bop@melbourne.buchan.com.au

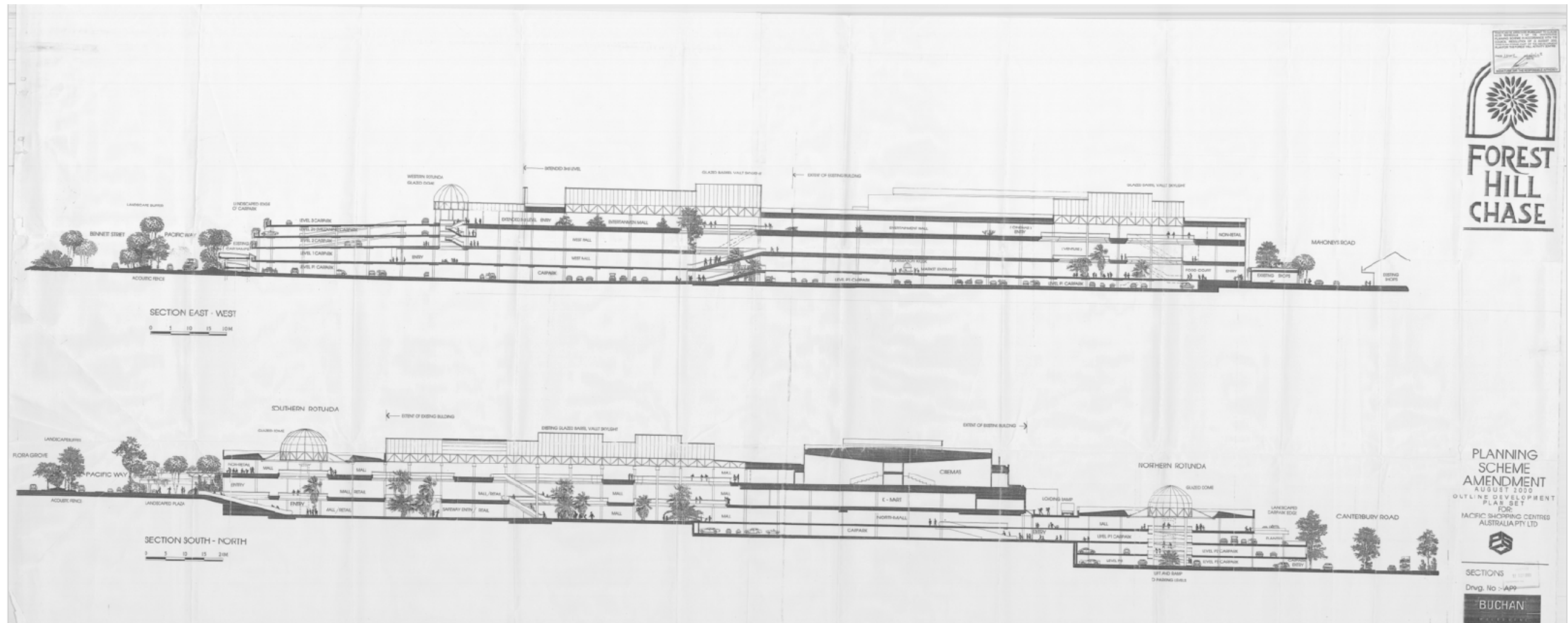
10.6 – ATTACHMENT 1. Planning Report



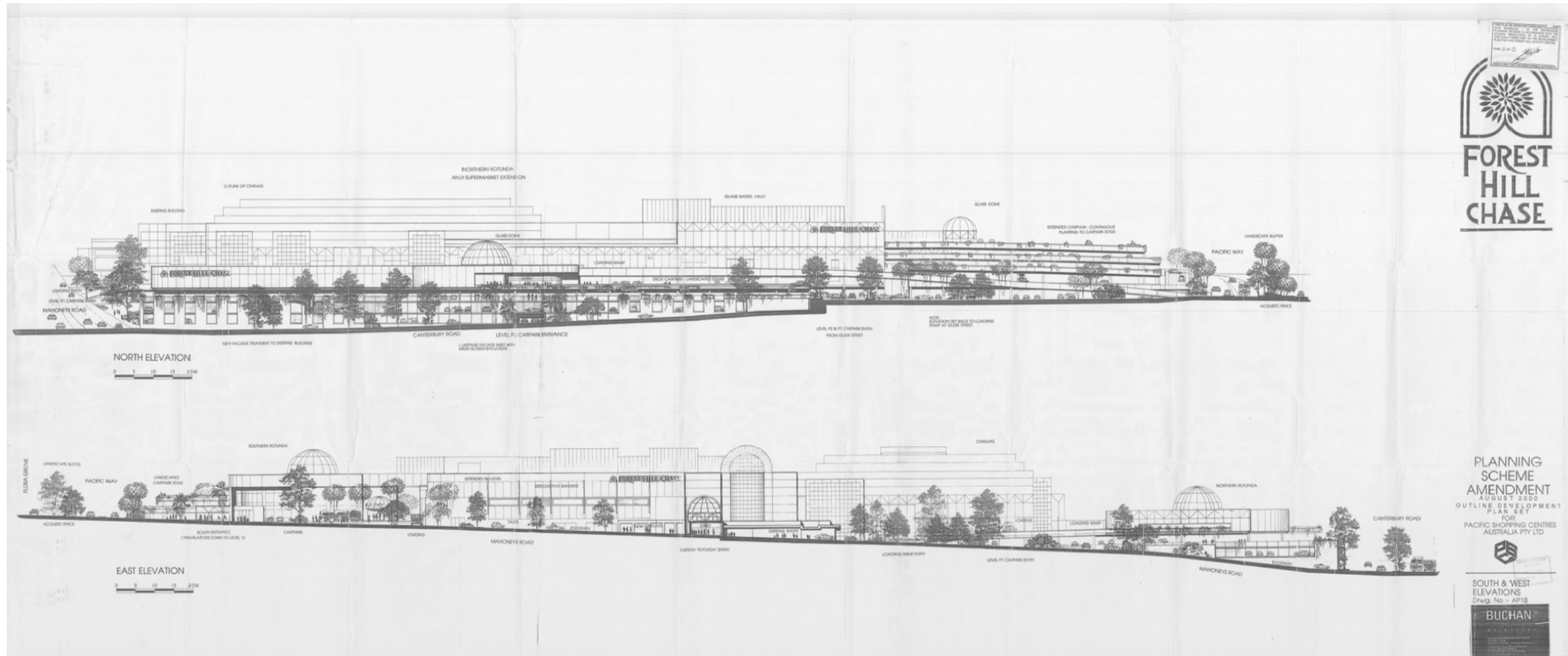
10.6 – ATTACHMENT 1. Planning Report



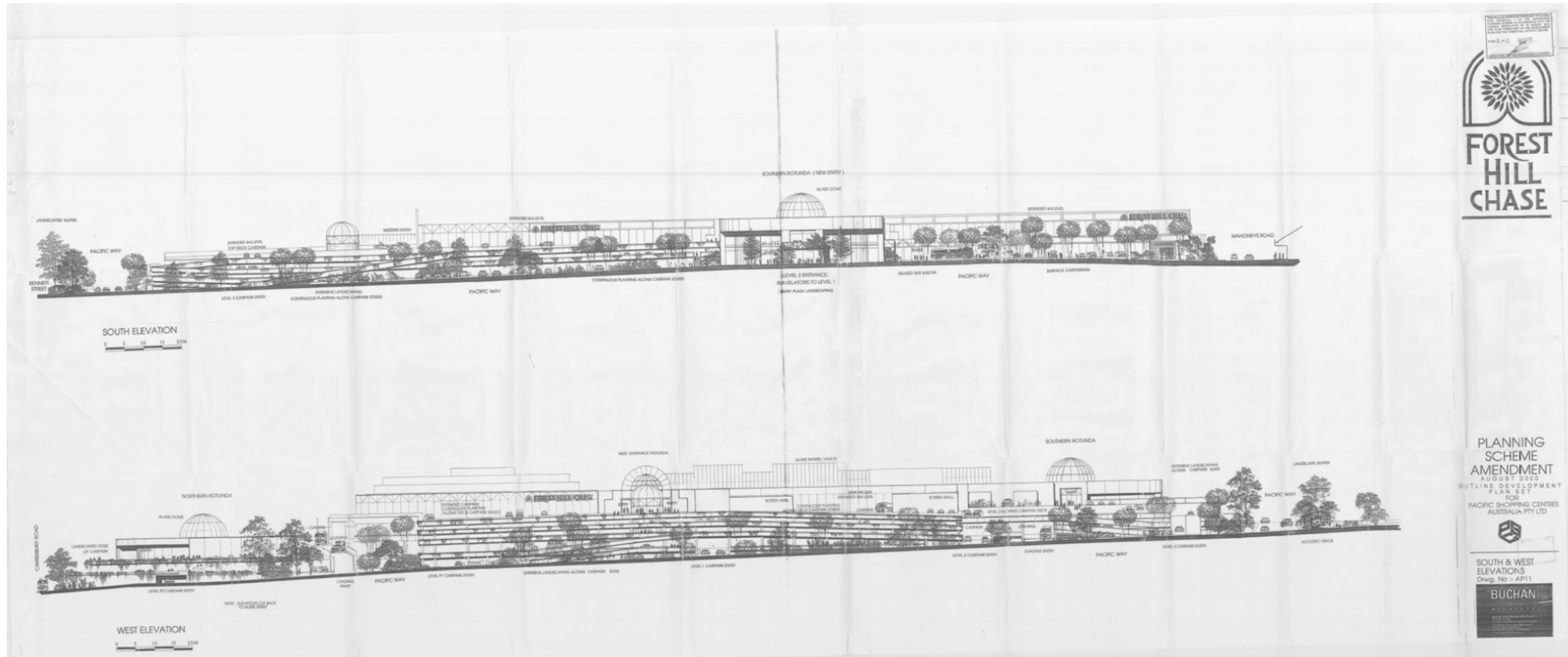
10.6 – ATTACHMENT 1. Planning Report



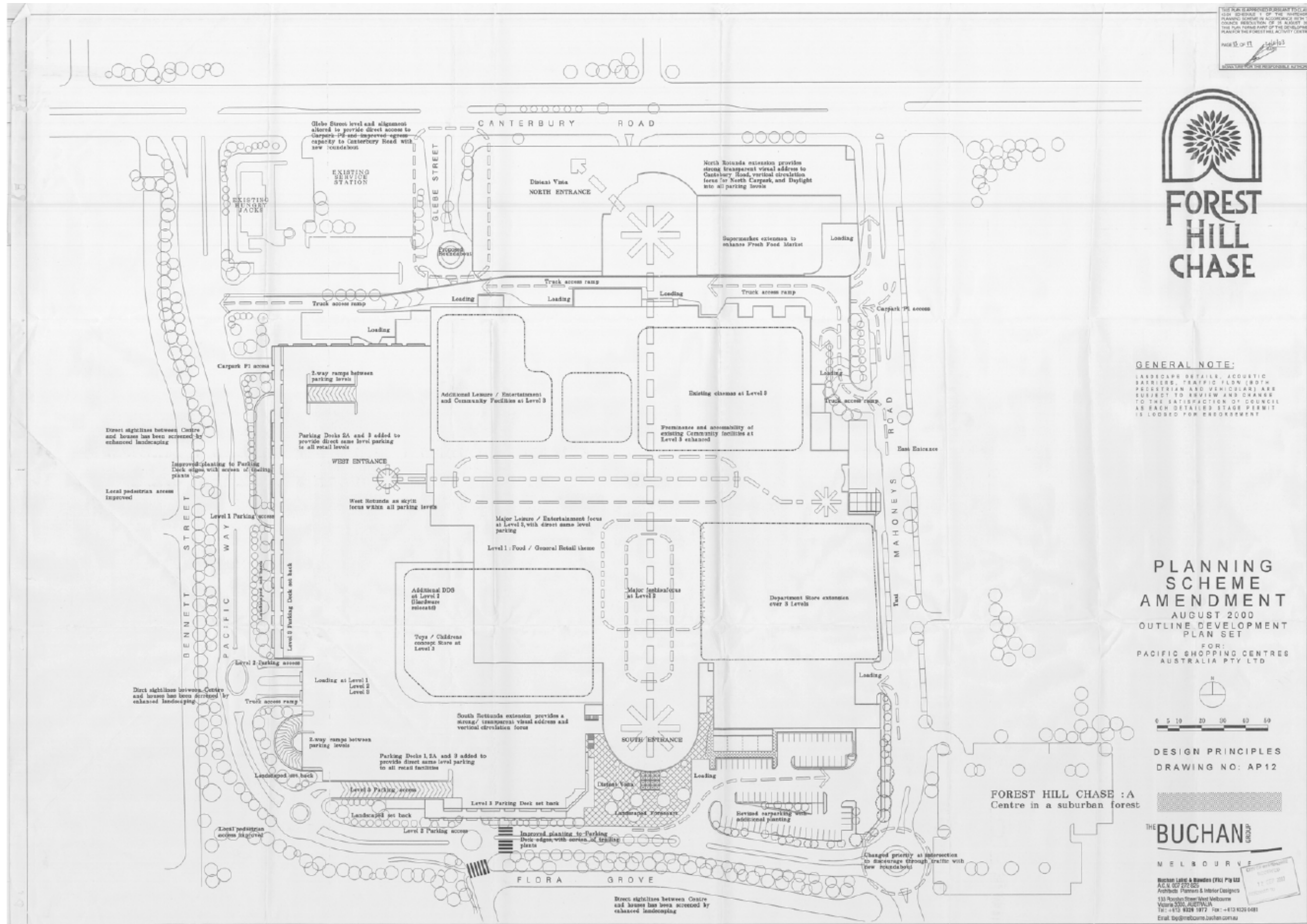
10.6 – ATTACHMENT 1. Planning Report



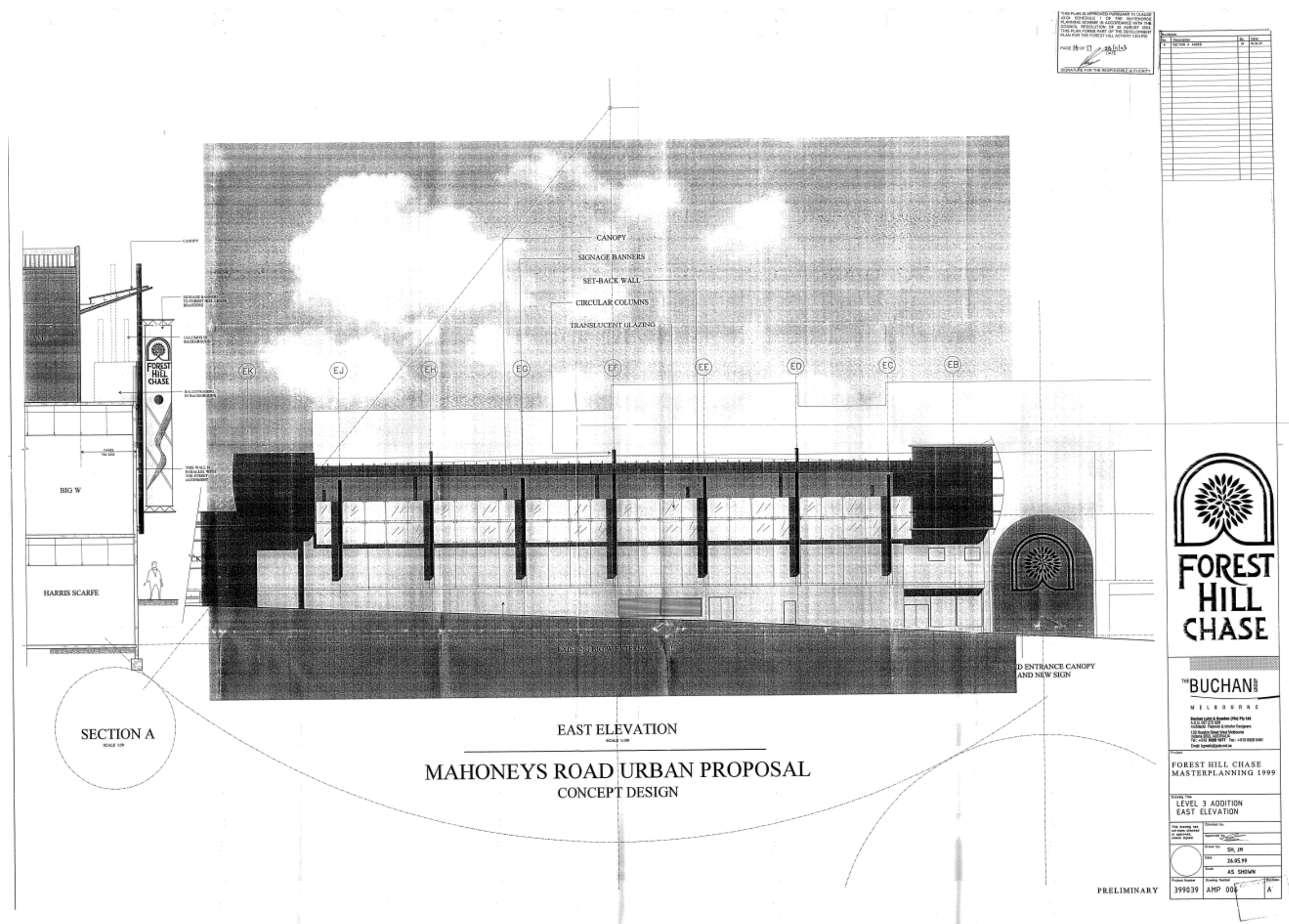
10.6 – ATTACHMENT 1. Planning Report



10.6 – ATTACHMENT 1. Planning Report



10.6 – ATTACHMENT 1. Planning Report



THIS PLAN IS APPROVED FOR SUBMITTAL TO CLASSIFY AS A DEVELOPMENT UNDER THE PROVISIONS OF THE ENVIRONMENTAL MANAGEMENT ACT 1999 AND THE ENVIRONMENTAL MANAGEMENT REGULATIONS 2001. THIS PLAN IS APPROVED FOR THE FOREST HILL ACTIVITY CENTRE.

DATE: 18/10/15

SIGNATURE FOR THE RESPONSIBLE OFFICER:

| NO. | REVISION | DATE |
|-----|------------------|------|
| 1 | ISSUE FOR PERMIT | |
| 2 | | |
| 3 | | |
| 4 | | |
| 5 | | |
| 6 | | |
| 7 | | |
| 8 | | |
| 9 | | |
| 10 | | |



THE BUCHANAN
MELBOURNE

Buchanan Light & Space (Pty) Ltd
A.C.N. 627 952 100
Architects, Planners & Interior Designers
120 Bundoora Street, Melbourne
VIC 3083, Australia Tel: +61 3 9228 9401
Email: info@thebuchanan.com.au

FOREST HILL CHASE
MASTERPLANNING 1999

LEVEL 3 ADDITION
EAST ELEVATION

| | |
|-----------------|----------|
| Drawn by: | SH, JH |
| Check: | 26.05.99 |
| Scale: | AS SHOWN |
| Project Number: | 399039 |
| Drawing Number: | AMP 006 |
| Revision: | A |

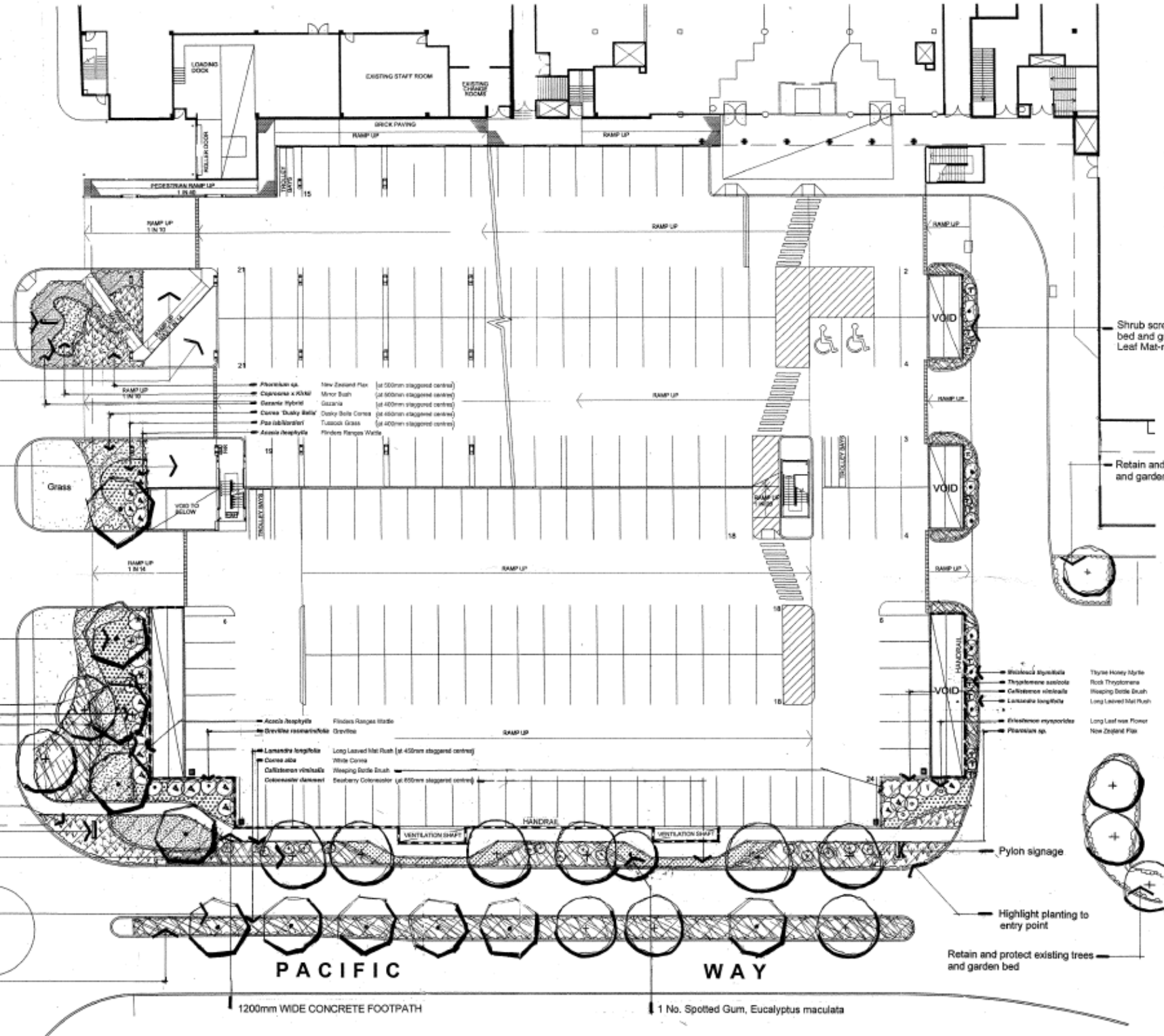
PRELIMINARY

10.6 – ATTACHMENT 1. Planning Report

PLANT SCHEDULE

| BOTANICAL NAME | COMMON NAME | SUPPLY SIZE |
|--------------------------------------|------------------------|------------------------|
| TREES | | |
| <i>Eucalyptus maculata</i> | Spotted Gum | 3.0m Height, 40mm Cal. |
| <i>Eucalyptus mannifera maculosa</i> | Red Spotted Gum | 2.4m Height, 40mm Cal. |
| SHRUBS | | |
| <i>Acacia longyfolia</i> | Flinders Ranges Wattle | 200mm pot size |
| <i>Callistemon citrinus</i> | Red Bottle Brush | 200mm pot size |
| <i>Callistemon viminalis</i> | Weeping Bottle Brush | 200mm pot size |
| <i>Conocarpus alatus</i> | Waxie Cornia | 200mm pot size |
| <i>Eriobotrya japonica</i> | Long Leaf rose Flower | 300mm pot size |
| <i>Grevillea rostrata</i> | Grevillea | 200mm pot size |
| <i>Malakoa macrophylla</i> | Western Tea Myrtle | 200mm pot size |
| <i>Malakoa thyrsiflora</i> | Thyme Honey Myrtle | 200mm pot size |
| <i>Myoporum laetifolium</i> | Rock Thymianum | 200mm pot size |
| GROUND COVERS | | |
| <i>Cyperus x 1000</i> | Minor Rush | 150mm pot size |
| <i>Carex "Dusky Bell"</i> | Dusky Bell Carex | 150mm pot size |
| <i>Carex reflexa</i> | Common Carex | 150mm pot size |
| <i>Cotoneaster divaricatus</i> | Beachberry Cotoneaster | 150mm pot size |
| <i>Gazania Hybrid</i> | Gazania | 150mm pot size |
| <i>Lomandra longifolia</i> | Long Leaved Mat Rush | 150mm pot size |
| <i>Phormium sp.</i> | New Zealand Flax | 150mm pot size |
| <i>Poa labillardieri</i> | Tussock Grass | tube |
| CLIMBERS AND TRAILERS | | |
| <i>Clematis arvensis</i> | Austral Clematis | 150mm pot size |
| <i>Arenaria coccinea</i> | Coast Vine | 180mm pot size |

- Pylon signage
- Massed ground cover planting
- Paved surface
- Paved Surface
- Grass
- New advanced tree planting of Red Spotted Gum, *Eucalyptus mannifera maculosa*.
- Shrub screen planting to rear of garden bed
- Highlight planting to entry point
- Pylon signage
- Retain and protect existing Eucalypts along Pacific Way frontage
- Advanced tree planting of Spotted Gum, *Eucalyptus maculata* to compliment existing tree planting
- Massed ground cover planting of Long Leaf Mat Rush, *Lomandra longifolia* to central median with ganitic sand mulch (planted at 450mm centres)



KEY

- Existing Tree to be Retained and Protected
- Proposed Tree Planting
- Garden Bed: 75mm Cultivated Subgrade, 200mm Imported Topsoil, 75mm Mulch
- Grass: 75mm Cultivated Site Soil, 75mm Imported Topsoil, Seeded Grass as specified



MARK McWHIA
LANDSCAPE ARCHITECT
2/398 SMITH ST
COLLINGWOOD 3066
PH: 03 9315 2215 FAX: 03 9315 0508

THE BUCHANAN
MELBOURNE
Landscape Architecture
120 Collins Street Melbourne
VIC 3000
Tel: +61 3 9247 1111 Fax: +61 3 9247 0444
Email: hugh@buchanan.com.au

FOR:
PACIFIC SHOPPING CENTRES
AUSTRALIA PTY LTD

Project: Forest Hill Chase
Date: 12 SEP 2003
Title: Landscape Concept Plan

Scale: 1:200
Date: November 2000
Project Number: 907
Drawing Number: LC 2
Revision: B

THIS PLAN IS APPROVED PURSUANT TO CLAUSE 43.04 SCHEDULE 1 OF THE WHITEHORSE PLANNING SCHEME IN ACCORDANCE WITH THE COUNCIL RESOLUTION OF 25 AUGUST 2003. THIS PLAN FORMS PART OF THE DEVELOPMENT PLAN FOR THE FOREST HILL ACTIVITY CENTRE.

PAGE 16 OF 17
DATE: 11/11/03
SIGNATURE FOR THE RESPONSIBLE AUTHORITY

FOREST HILL CHASE PROPOSED CARPARK LANDSCAPE CONCEPT PLAN

10.6 – ATTACHMENT 1. Planning Report

**A.2 Amended Forest Hill Chase
Development Plan**

10.6 – ATTACHMENT 1. Planning Report

THIS PLAN IS APPROVED PURSUANT TO CLAUSE 43.04 SCHEDULE 1 OF THE WHITEHORSE PLANNING SCHEME IN ACCORDANCE WITH THE COUNCIL RESOLUTION OF 25 AUGUST 2003. THIS PLAN FORMS PART OF THE DEVELOPMENT PLAN FOR THE FOREST HILL ACTIVITY CENTRE

PAGE 1 OF 17 DATE 24/11/03

SIGNATURE FOR THE RESPONSIBLE AUTHORITY



FOREST HILL CHASE ACTIVITY CENTRE

Summary of Floor Space

Project No. 399067
20 August 2003

| Tenancy Type | Existing as Depicted on Development Plan 11.09.00 | Proposed Additional Floor Space As Per Development Plan 11.09.00 | Additional Floor Space Proposed by June 2003 Development Plan | Total FHCAC Proposed Extensions |
|-----------------------------|---|--|---|---------------------------------|
| RETAIL | | | | |
| Major Tenants | 30,822m ² | 9,458m ² | | 40,280m ² |
| Specialty | | | | |
| • Forest Hill Chase | 18,241m ² | 2,879m ² | | 21,120m ² |
| • East Side Mahoneys Road * | 2,500m ² | - | | 2,500m ² |
| Total Retail | 51,563m² | 12,337m² | | 63,900m² |
| NON-RETAIL | | | | |
| Forest Hill Chase | 5,729m ² | 3,953m ² | | 9,682m ² |
| East Side Mahoneys Road * | 4,500m ² | - | 652m ² | 5,152m ² |
| Total Non-Retail | 10,229m² | 3,953m² | 652m² | 14,834m² |
| Total Floor Space | 61,792m² | 16,290m² | 652m² | 78,734m² |

* 'Existing' figures derived from attached 'Table 1 Forest Hill Activity Centre' as exhibited for Amendment L31 to the Nunawading Planning Scheme.

PLANNING SCHEME AMENDMENT

SEPTEMBER 2003
SUMMARY OF FLOOR SPACE

FOR:
PACIFIC SHOPPING CENTRES AUSTRALIA PTY LTD

DRAWING NO: AP 16

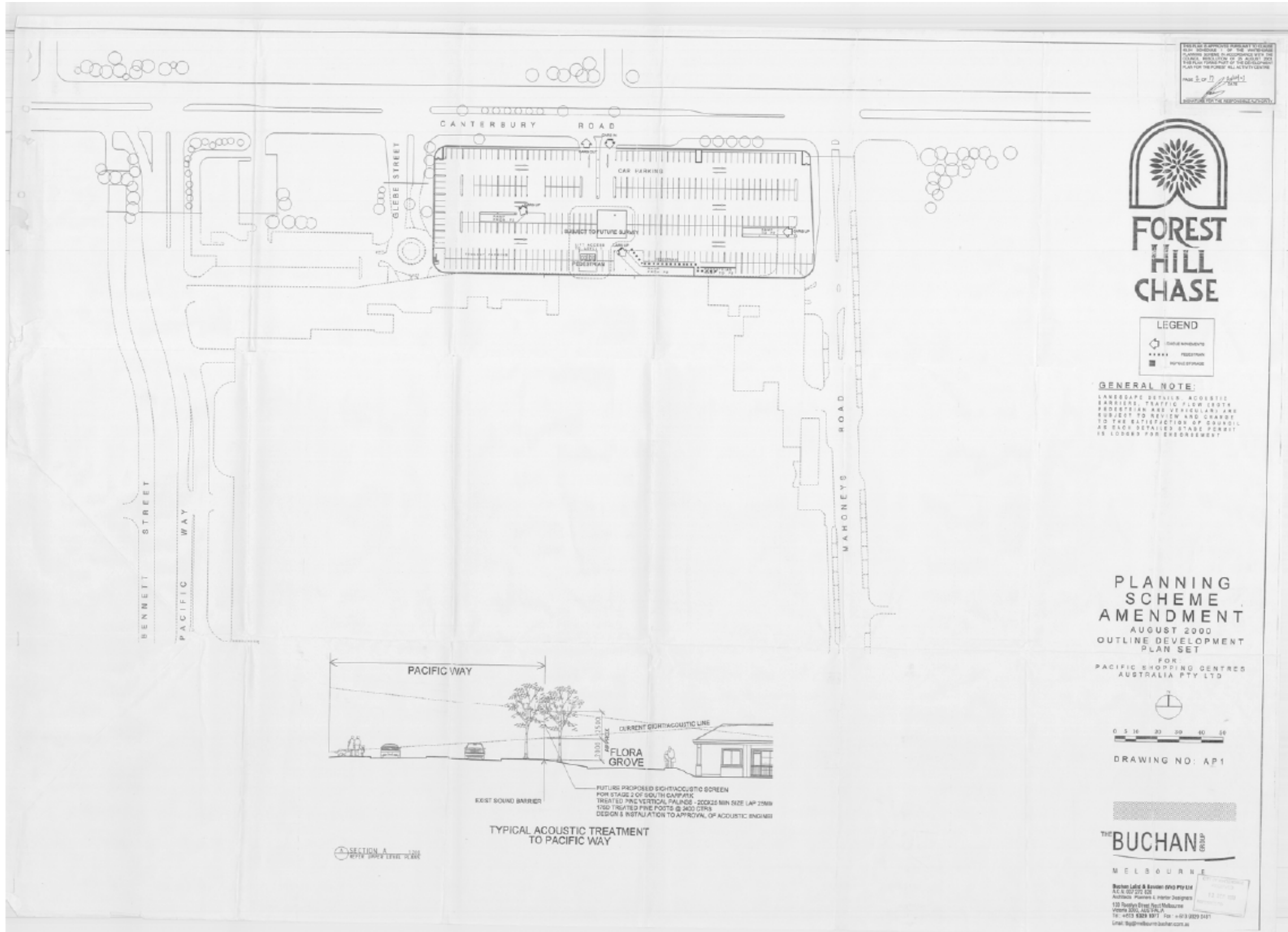
THE BUCHAN GROUP

MELBOURNE

Buchan Lobb & Rowley (194) Pty Ltd
A.C.N. 007 877 205
Architects, Planners, Engineers, Designers
133 Bouverie Street, Melbourne
VIC 3000, AUSTRALIA
Tel: +61 3 8228 1077 Fax: +61 3 8228 0441
Email: bll@melbourne.buchan.com.au
© The Buchanan Group 2003

March 2026
Amendments proposed shown clouded

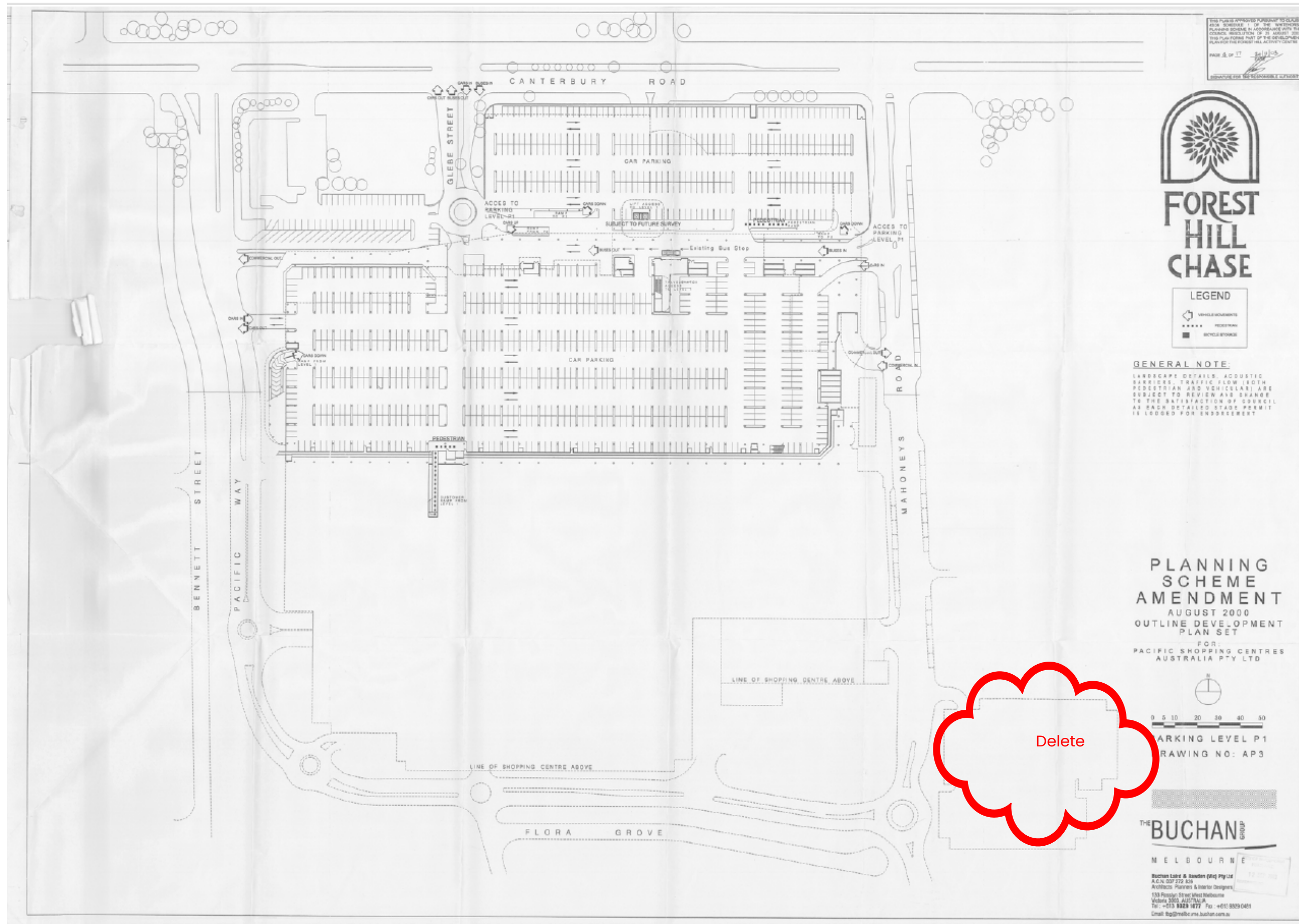
10.6 – ATTACHMENT 1. Planning Report



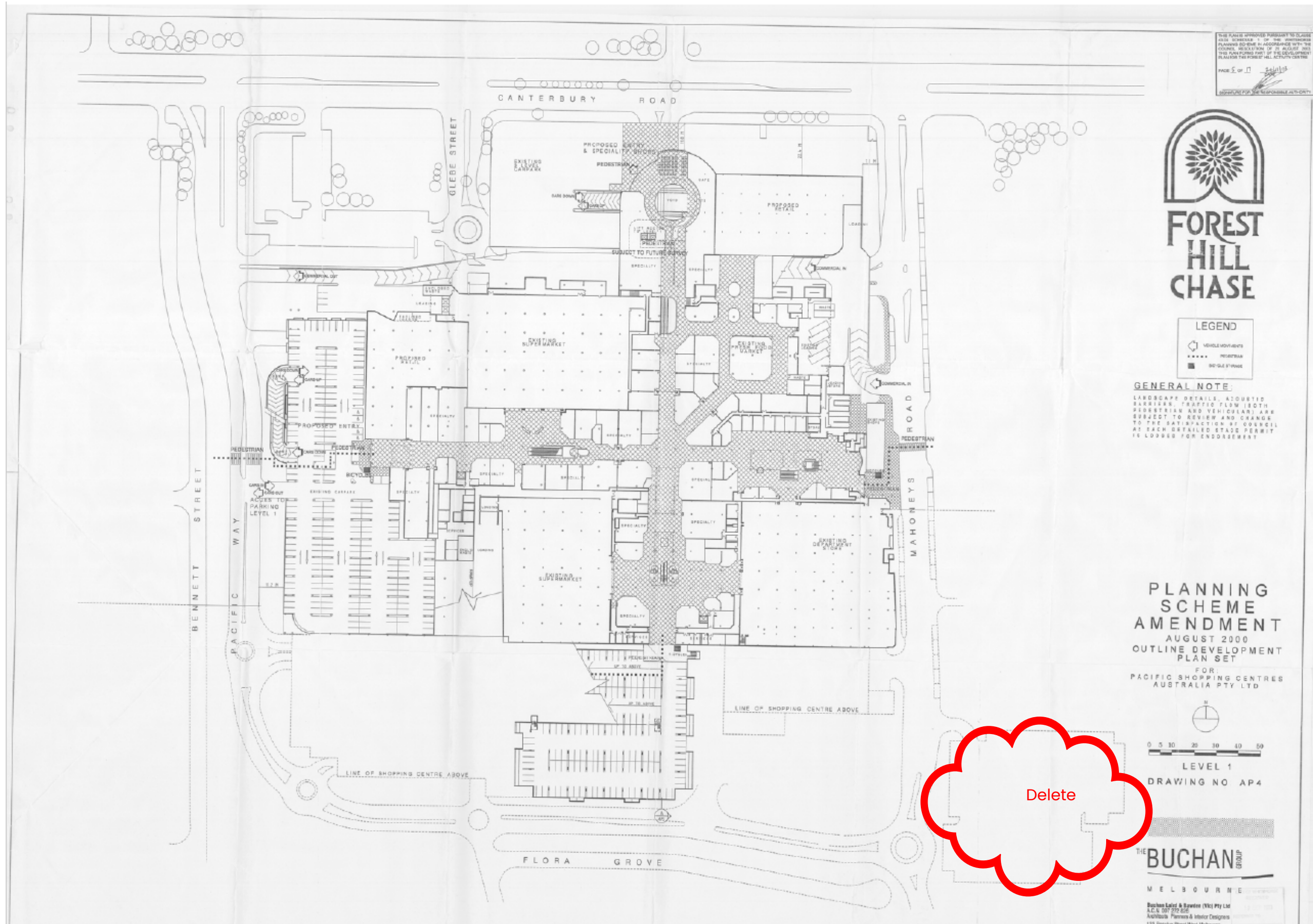
10.6 – ATTACHMENT 1. Planning Report



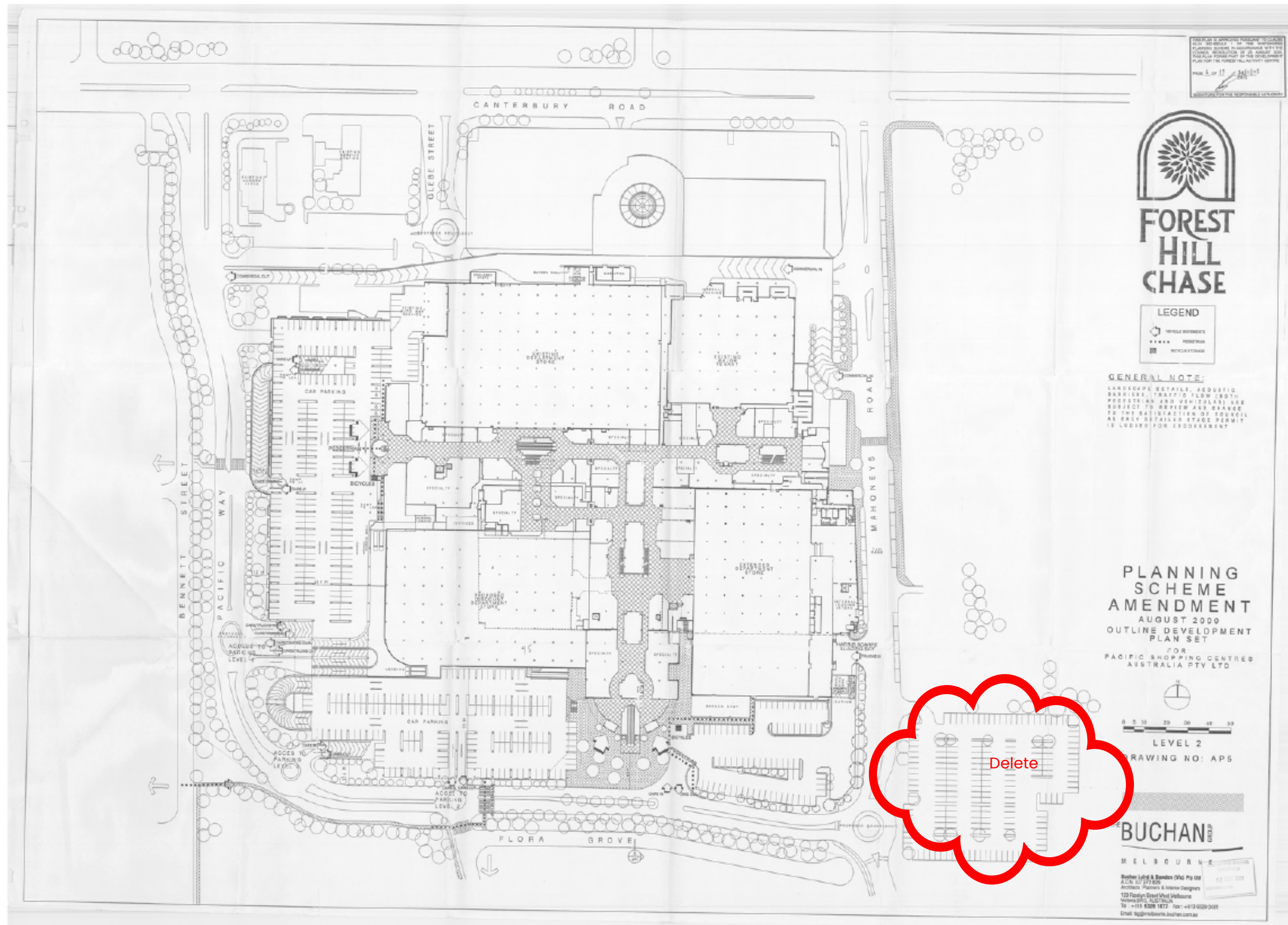
10.6 – ATTACHMENT 1. Planning Report



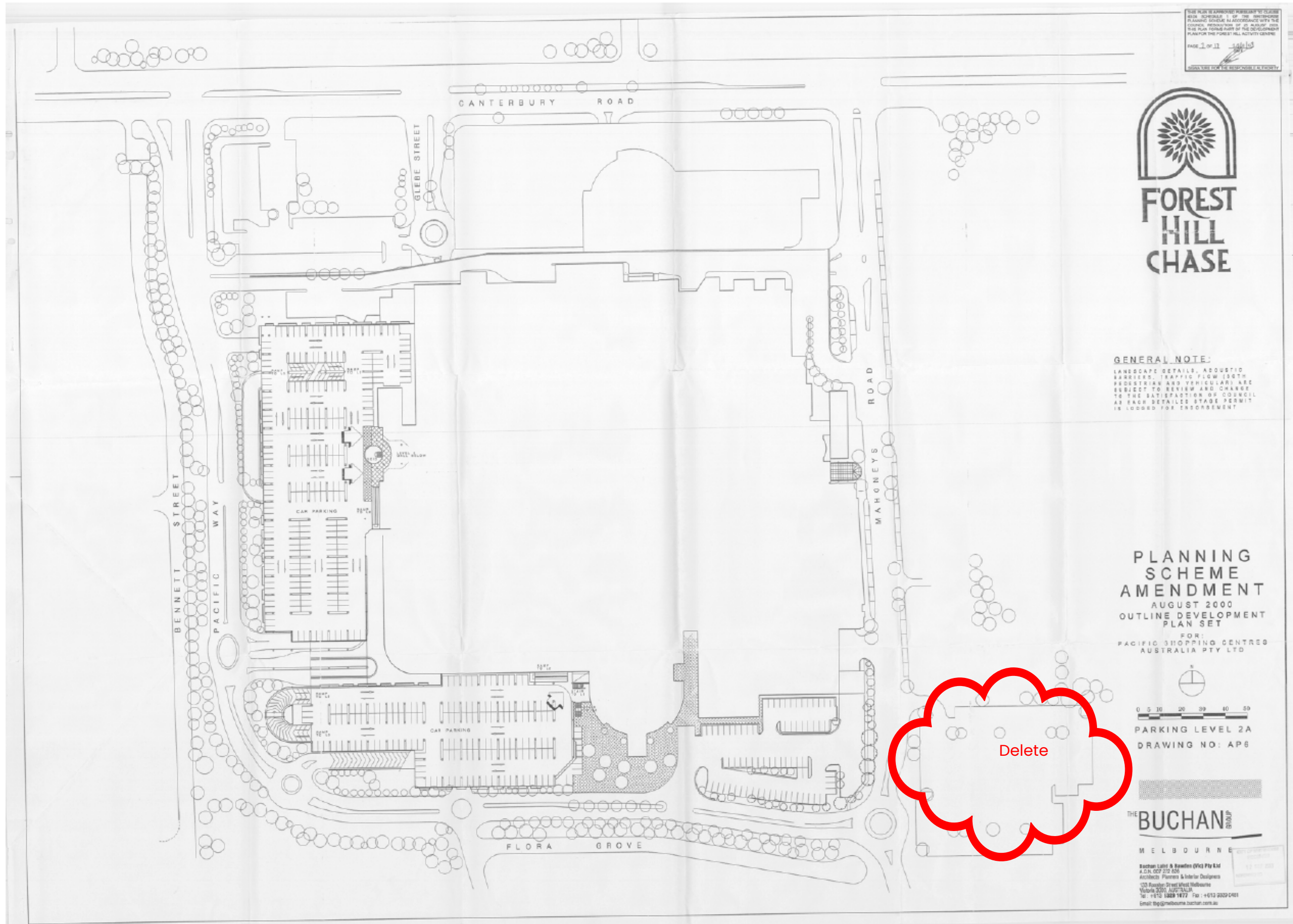
10.6 – ATTACHMENT 1. Planning Report



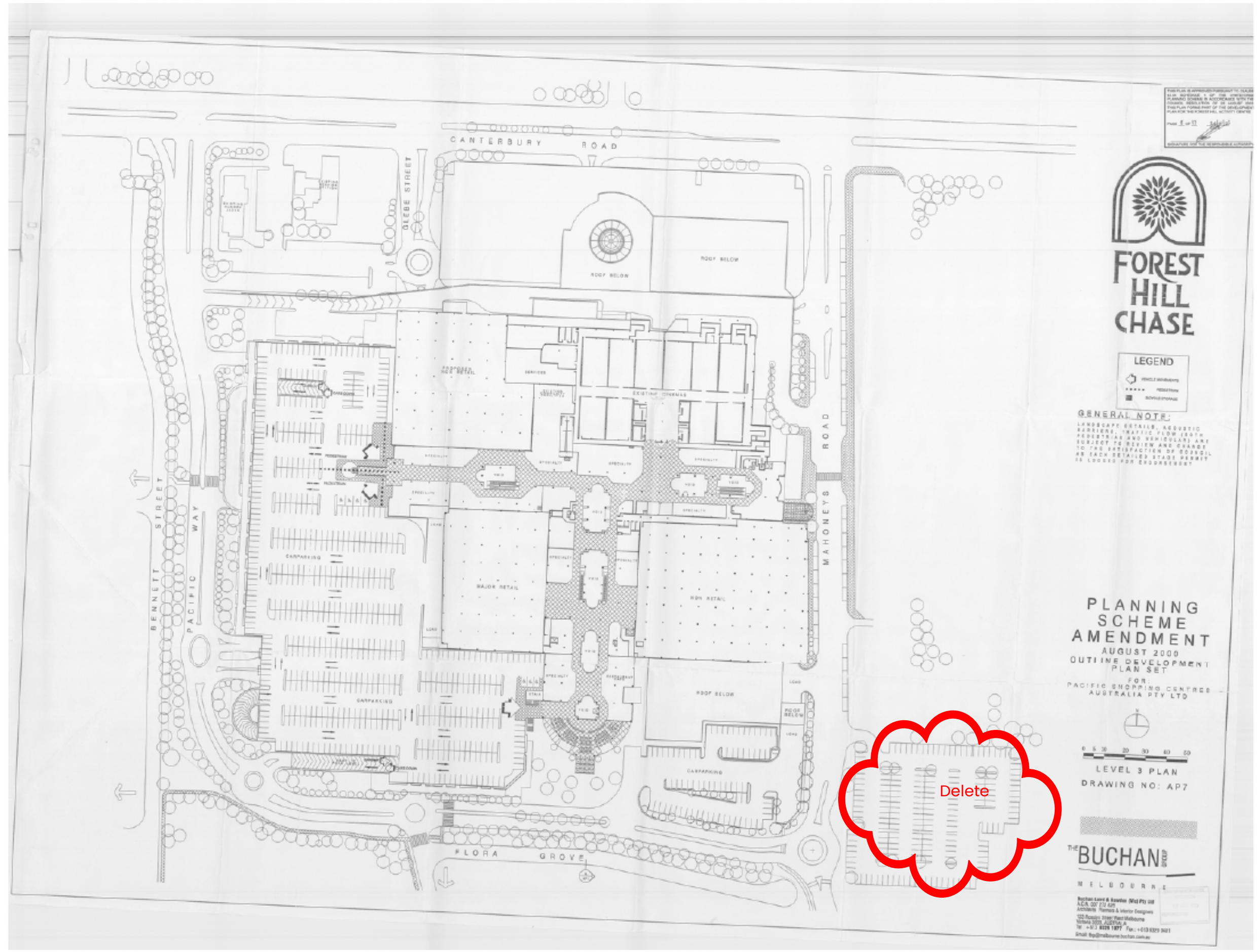
10.6 – ATTACHMENT 1. Planning Report



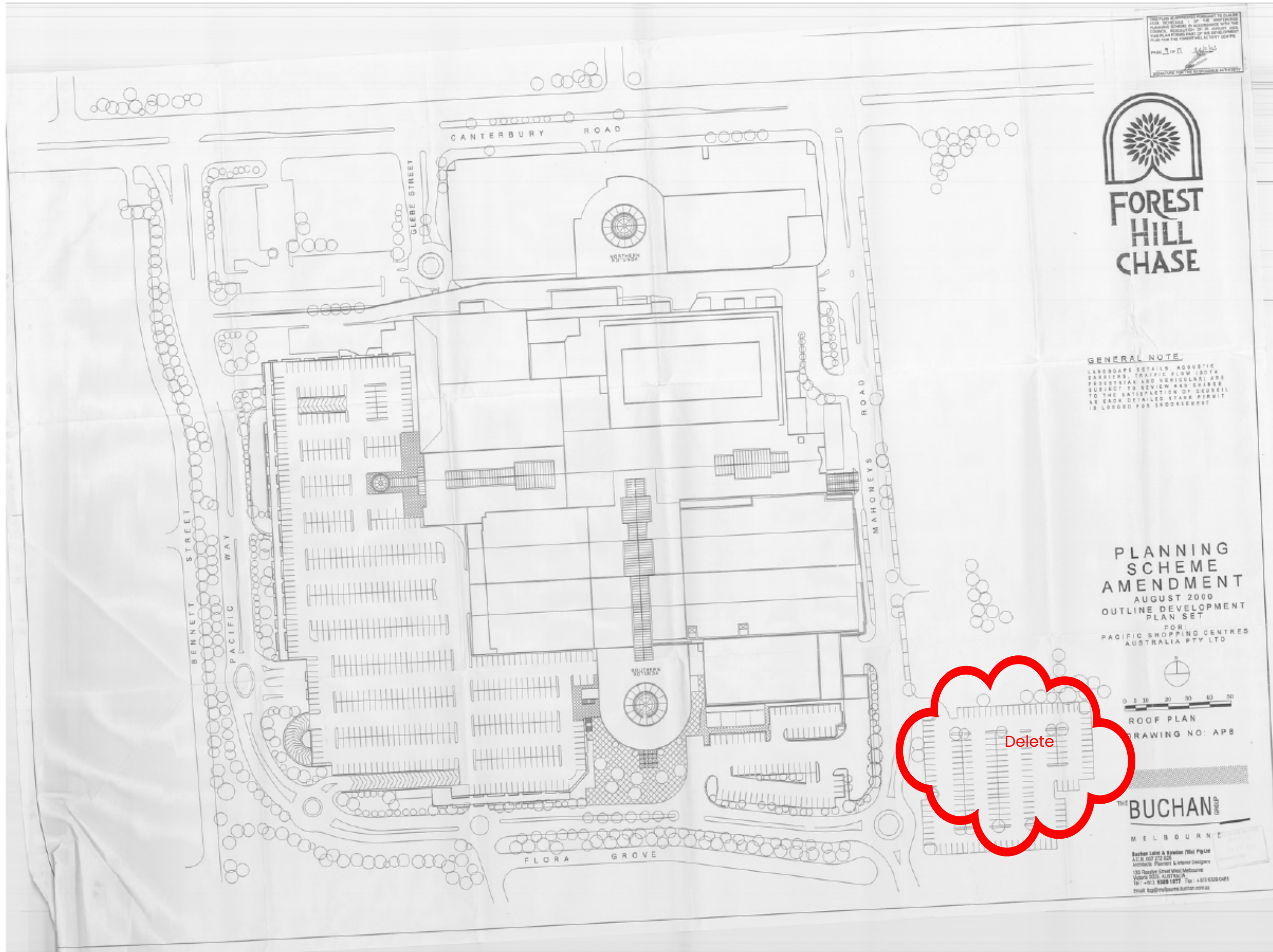
10.6 – ATTACHMENT 1. Planning Report



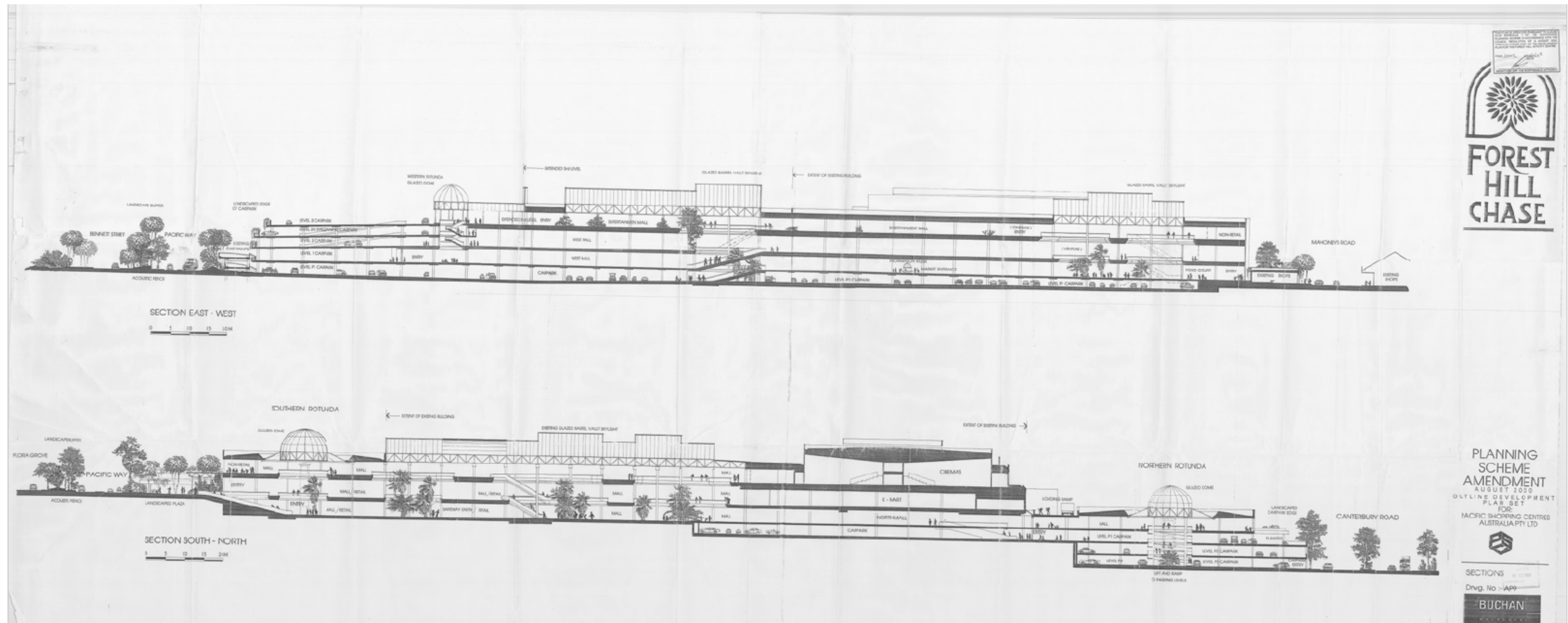
10.6 – ATTACHMENT 1. Planning Report



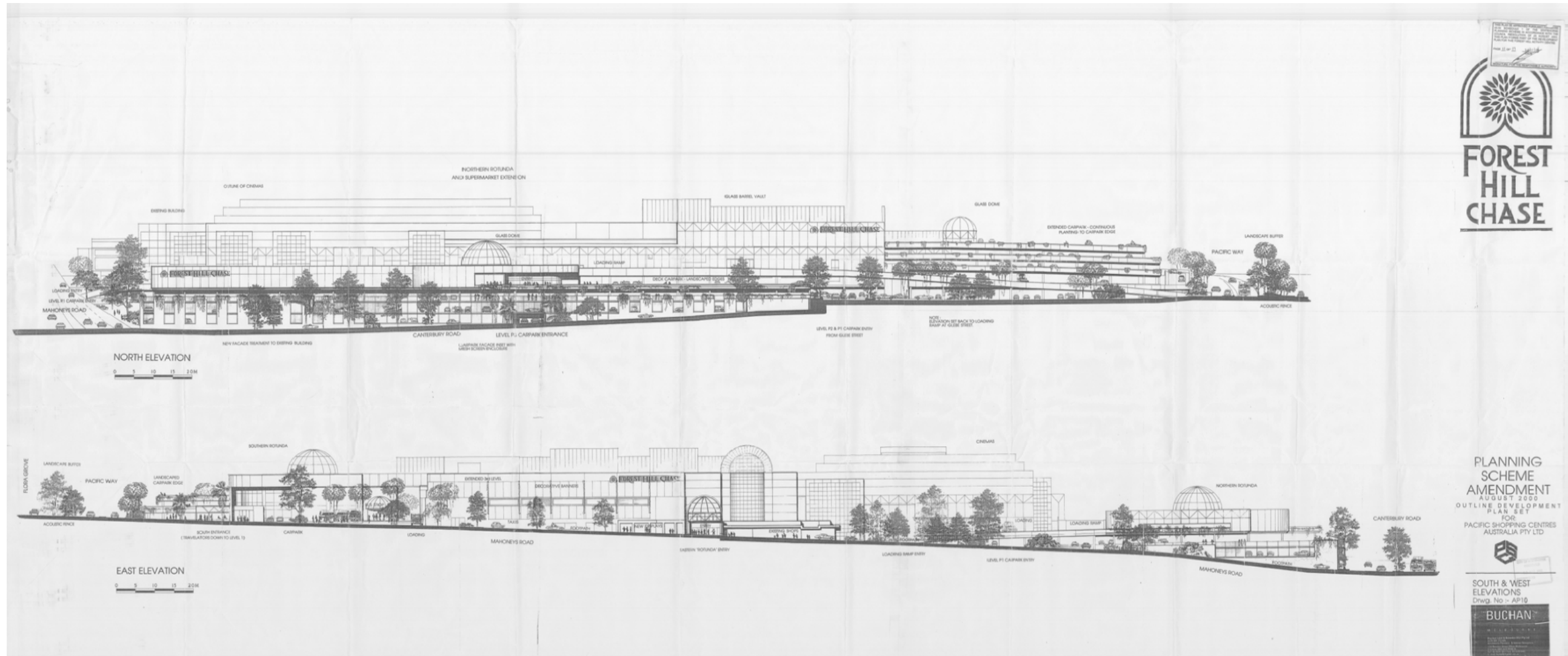
10.6 – ATTACHMENT 1. Planning Report



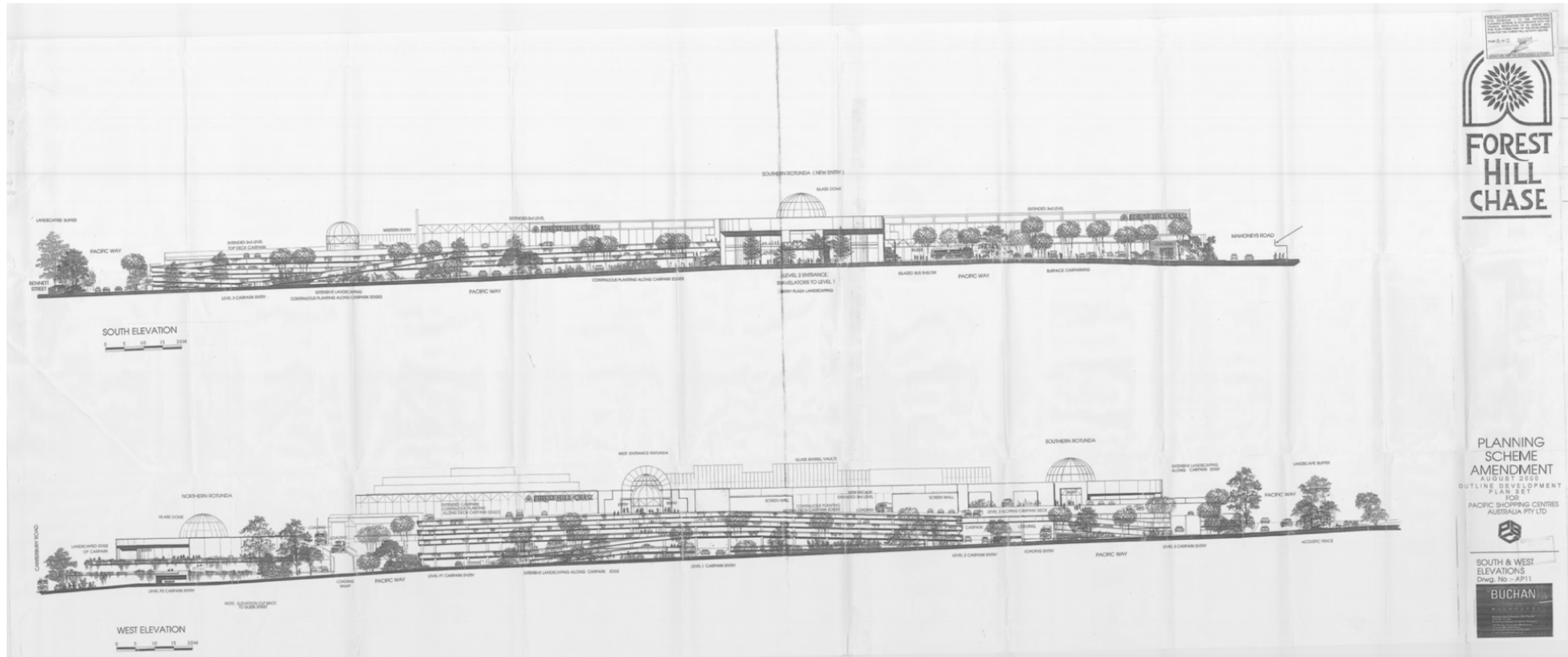
10.6 – ATTACHMENT 1. Planning Report



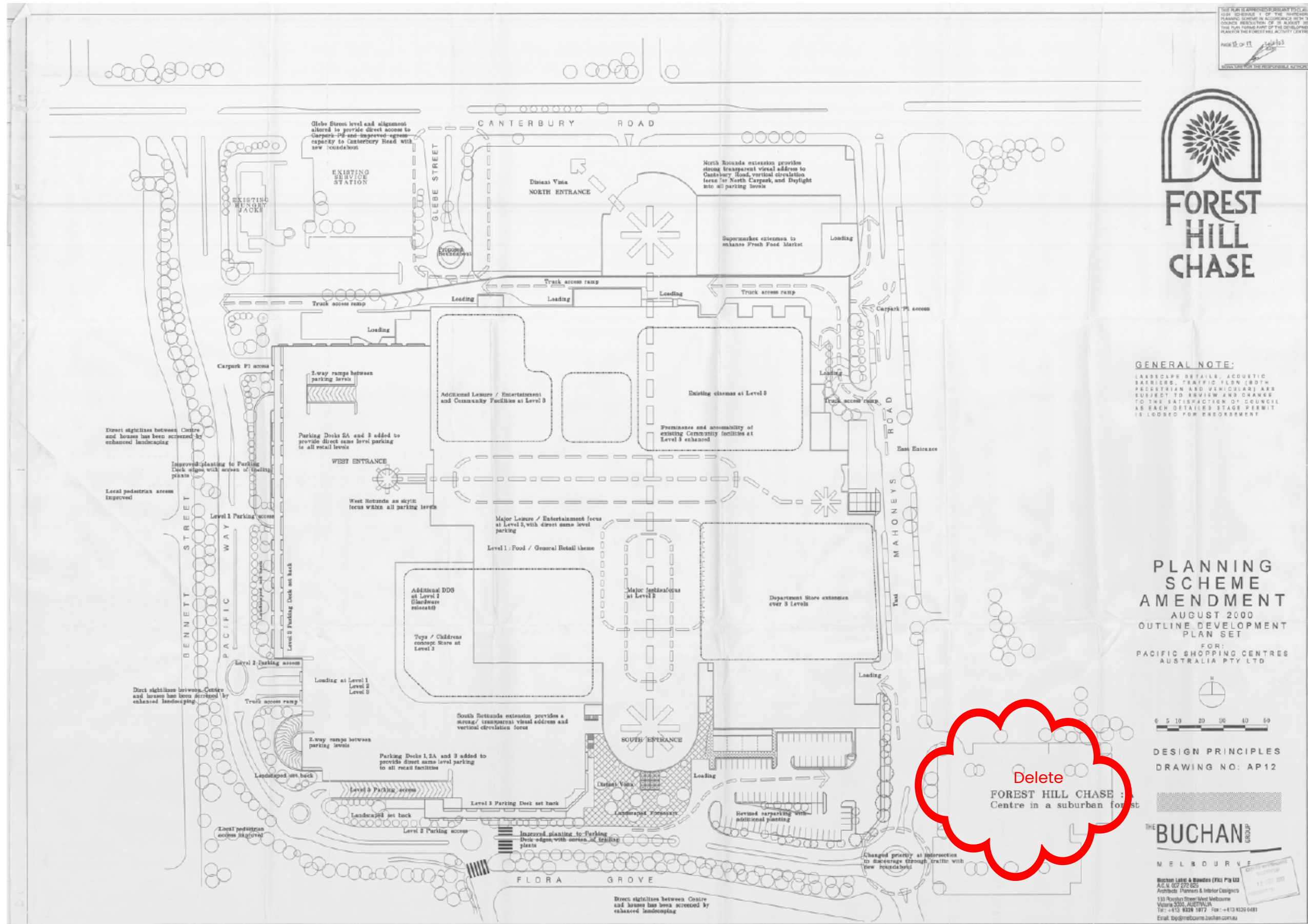
10.6 – ATTACHMENT 1. Planning Report



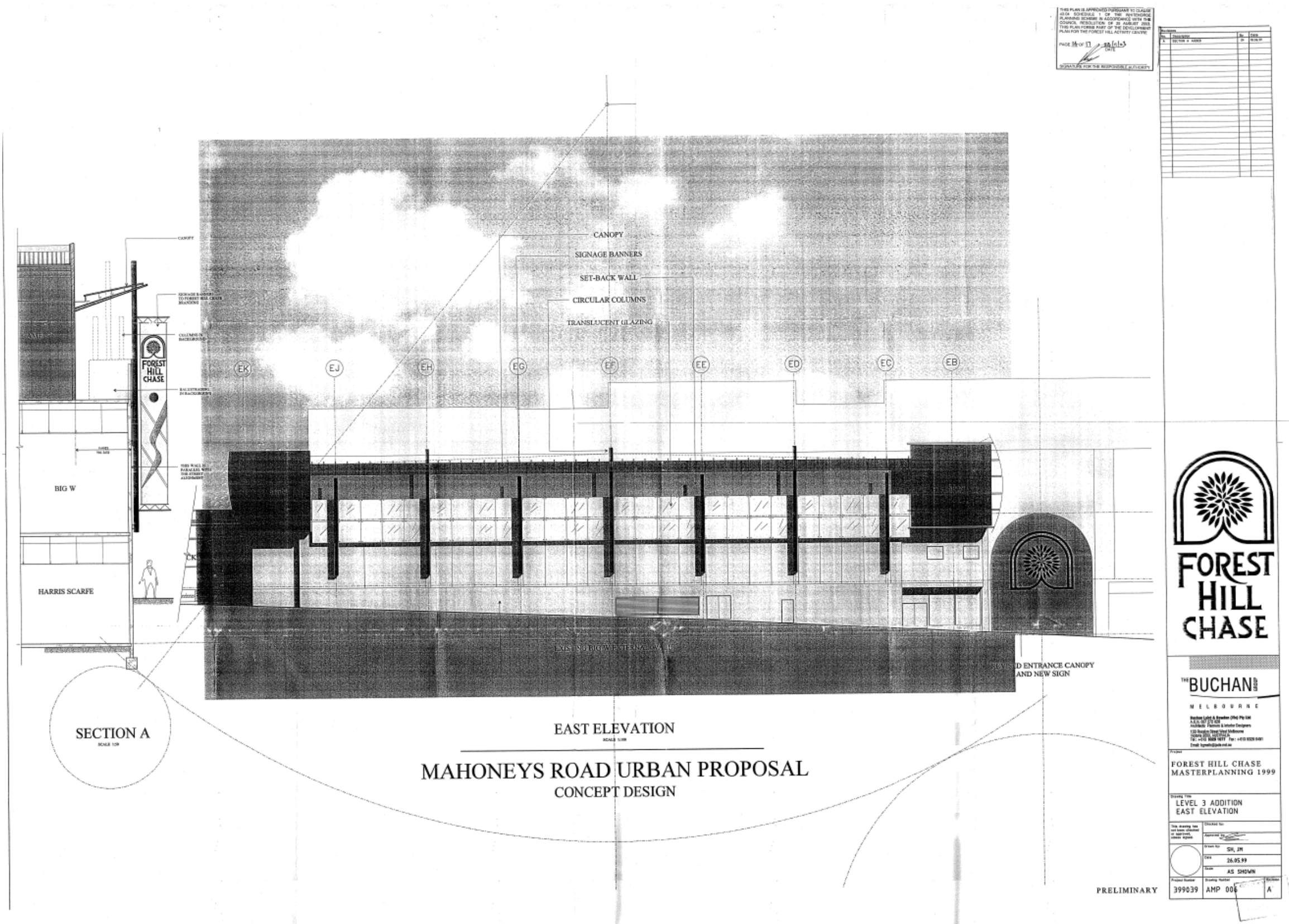
10.6 – ATTACHMENT 1. Planning Report



10.6 – ATTACHMENT 1. Planning Report



10.6 – ATTACHMENT 1. Planning Report



THIS PLAN IS APPROVED FOR SUBMITTAL TO CLASSIFY AS A DEVELOPMENT PLAN UNDER THE ENVIRONMENTAL PLANNING AND DEVELOPMENT ACT 1993. THE APPROVAL IS GIVEN IN ACCORDANCE WITH THE LOCAL GOVERNMENT ACT 1989 AND THE LOCAL GOVERNMENT (MISCELLANEOUS PROVISIONS) ACT 1989. THIS PLAN FORMS PART OF THE DEVELOPMENT PLAN FOR THE FOREST HILL ACTIVITY CENTRE.

DATE: 18/10/15

SIGNATURE FOR THE RESPONSIBLE OFFICER:

| NO. | DATE | BY | REVISION |
|-----|------|----|----------|
| 1 | | | |
| 2 | | | |
| 3 | | | |
| 4 | | | |
| 5 | | | |
| 6 | | | |
| 7 | | | |
| 8 | | | |
| 9 | | | |
| 10 | | | |



THE BUCHANAN
MELBOURNE

Buchanan Light & Space (Pty) Ltd
ALL RIGHTS RESERVED
Architects, Planners & Interior Designers
120 Bundoora Street, Melbourne
VIC 3083, Australia Tel: +61 3 9228 9401
Email: info@thebuchanan.com.au

FOREST HILL CHASE
MASTERPLANNING 1999

LEVEL 3 ADDITION
EAST ELEVATION

| | |
|-----------------|----------|
| Drawn by: | SH, JH |
| Check: | 26.05.99 |
| Scale: | AS SHOWN |
| Project Number: | 399039 |
| Drawing Number: | AMP 006 |
| Revision: | A |

PRELIMINARY

10.6 – ATTACHMENT 1. Planning Report

Appendix
B

Amendments to
Mahoneys Road
Development Plan

10.6 – ATTACHMENT 1. Planning Report

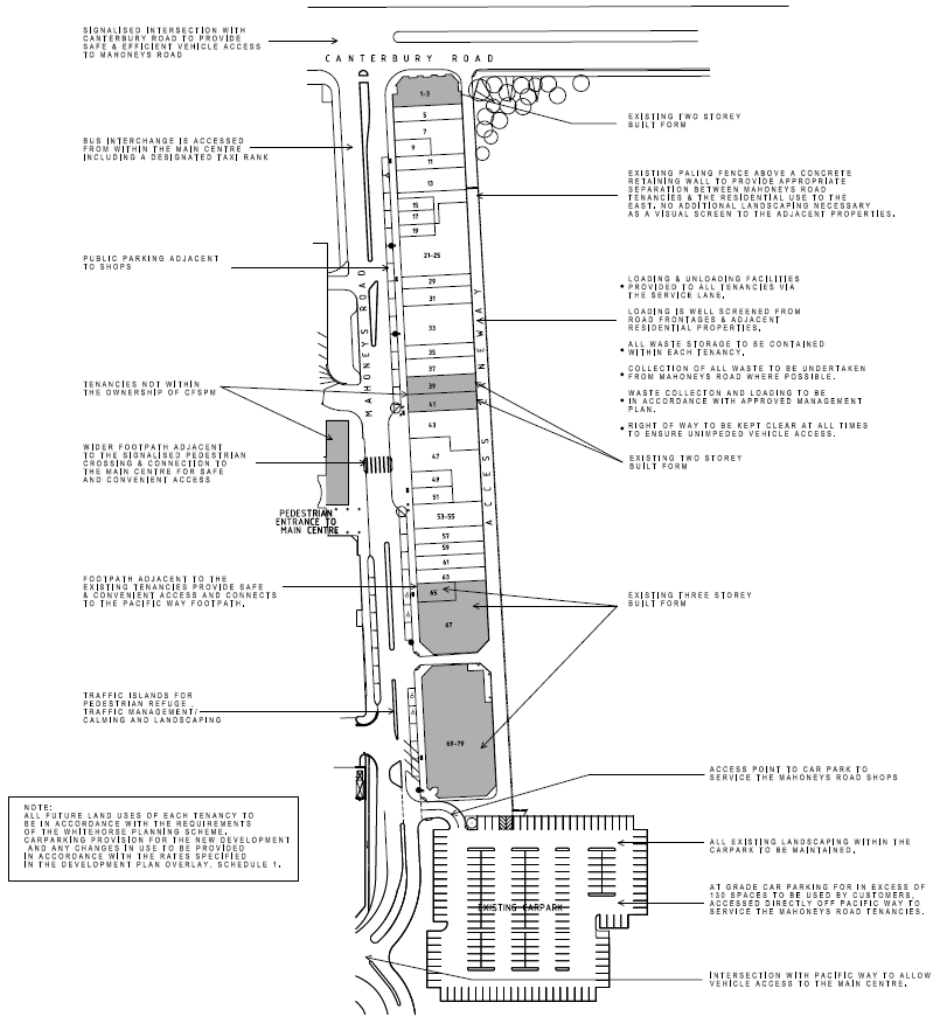
**B.1 Existing Mahoneys Road Development
Plan**

10.6 – ATTACHMENT 1. Planning Report

GENERAL NOTES:

- THIS DEVELOPMENT PLAN APPLIES ONLY TO THE MAHONEYS ROAD SHOPS AND THE FOOTPATH/PAVEMENT DIRECTLY IN FRONT OF THESE SHOPS. THE ROAD RESERVE AND OPEN AIR CARPARK TO THE SOUTH OF THE SHOPS ARE COVERED BY THE DEVELOPMENT PLAN FOR FOREST HILL CHASE.
- THE FOREST HILL DEVELOPMENT PLAN SHOULD BE REFERENCED FOR DETAILS IN RELATION TO:
 - LOCATION AND LAYOUT OF CARPARKING AREAS,
 - TRAFFIC CONTROL AND TRAFFIC MANAGEMENT DEVICES,
 - EXISTING AND PROPOSED LANDSCAPING.
- WASTE HANDLING AND COLLECTION PROCEDURES TO BE KEPT AWAY FROM ADJACENT RESIDENTIAL AREAS.
- DRAINAGE OF SITE TO THE SATISFACTION OF THE RESPONSIBLE AUTHORITY.
- ANY FUTURE LANDSCAPE WORKS TO MAHONEYS ROAD AND PACIFIC WAY TO BE UNDERTAKEN IN ACCORDANCE WITH THE FUTURE DEVELOPMENT OF THE MAIN CENTRE AND THE CORRESPONDING DEVELOPMENT PLAN.
- ALL EXISTING LANDSCAPING ALONG THE MAHONEYS ROAD FOOTPATH AREA AND THE MEDIAN STRIP TO BE MAINTAINED.
- MAXIMUM TOTAL FLOOR AREA FOR THE LAND BOUNDED BY THE DEVELOPMENT PLAN OVERLAY SCHEDULE 1 IS 84,000 SQ.M.
- STREET LIGHTING AND LIGHTING TO TENANCIES SHALL BE LOCATED TO PREVENT LIGHT SPILLAGE INTO ADJACENT RESIDENTIAL AREAS.
- BUILDINGS ARE SINGLE STOREY UNLESS OTHERWISE NOMINATED.

| FLOOR AREA TABLE | |
|-----------------------------|--------------------------------|
| MAHONEYS ROAD STREET NUMBER | ESTIMATED GLA AS AT MARCH 2006 |
| 01 | 720 |
| 05 | 150 |
| 07 | 230 |
| 09 | 80 |
| 11 | 130 |
| 13 | 160 |
| 15 | 70 |
| 17 | 70 |
| 19 | 60 |
| 21-25 | 540 |
| 27 | 270 |
| 31 | 130 |
| 33 | 330 |
| 35 | 180 |
| 37 | 150 |
| 39 | 380 |
| 41 | 400 |
| 43 | 280 |
| 47 | 350 |
| 49 | 100 |
| 51 | 80 |
| 53-55 | 350 |
| 57 | 130 |
| 59 | 130 |
| 61 | 130 |
| 63 | 320 |
| 65-67 | 500 |
| 69-73 | 3,000 |
| Total Mahoneys Road | 9,500 |
| Total CFSPM | 8,740 |
| Non-CFSPM (no. x 38x41) | 760 |



MAHONEYS ROAD SHOPS

DEVELOPMENT PLAN

THE BUCHANAN
MELBOURNE

PROJECT NO: 005118
SCALE: 1:500 @ A0
DATE: 11 APRIL 2006
DRAWING NO: ATP 001 REG 6

10.6 – ATTACHMENT 1. Planning Report

**B.2 Amended Mahoneys Road
Development Plan**

10.6 – ATTACHMENT 1. Planning Report

GENERAL NOTES:

- THIS DEVELOPMENT PLAN APPLIES ONLY TO THE MAHONEYS ROAD SHOPS AND THE FOOTPATH/PAVEMENT DIRECTLY IN FRONT OF THESE SHOPS.
- THE ROAD RESERVE AND OPEN AIR CARPARK TO THE SOUTH OF THE SHOPS ARE COVERED BY THE DEVELOPMENT PLAN FOR FOREST HILL DRIVE.
- THE CORRECT TITLE DEVELOPMENT PLAN SHOULD BE REFERENCED FOR DETAILS IN SECTION 10.
- LOCATION AND LAYOUT OF CARPARKING AREAS.
- TRAFFIC CONTROL AND TRAFFIC MANAGEMENT DEVICES.
- EXISTING AND PROPOSED LANDSCAPING.
- WASTE HANDLING AND COLLECTION PROCEDURES TO BE KEPT AWAY FROM ADJACENT RESIDENTIAL AREAS.
- DRAINAGE OF SITE TO THE SATISFACTION OF THE RESPONSIBLE AUTHORITY.
- ANY FUTURE LANDSCAPE WORKS TO MAHONEYS ROAD AND PACIFIC WAY TO BE UNDERTAKEN IN ACCORDANCE WITH THE FUTURE DEVELOPMENT OF THE MAIN CENTRE AND THE CORRESPONDING DEVELOPMENT PLAN.
- ALL EXISTING LANDSCAPING ALONG THE MAHONEYS ROAD FOOTPATH AREA AND THE MEDIAN STRIP TO BE MAINTAINED.
- MAXIMUM TOTAL FLOOR AREA FOR THE LAND BOUNDED BY THE DEVELOPMENT PLAN OVERLAY SCHEDULE 1 IS 8,500 SQ.M.
- STREET LIGHTING AND LIGHTING TO TENANCIES SHALL BE LOCATED TO PREVENT LIGHT SPILLAGE INTO ADJACENT RESIDENTIAL AREAS.
- BUILDINGS ARE SINGLE STOREY UNLESS OTHERWISE NOMINATED.

SIGNALISED INTERSECTION WITH CANTERBURY ROAD TO PROVIDE SAFE & EFFICIENT VEHICLE ACCESS TO MAHONEYS ROAD

BUS INTERCHANGE IS ACCESSED FROM WITHIN THE MAIN CENTRE INCLUDING A DESIGNATED TAXI RANK

PUBLIC PARKING ADJACENT TO SHOPS

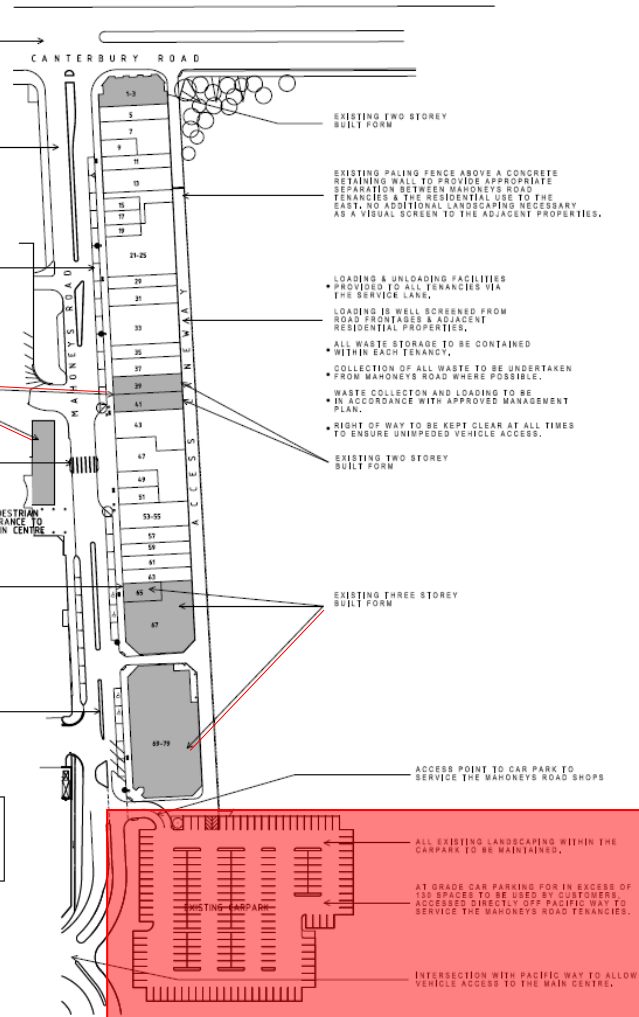
TENANCIES NOT WITHIN THE OVERLAP OF CSFPM

WIDER FOOTPATH ADJACENT TO THE SIGNALISED PEDESTRIAN CROSSING & CONNECTION TO THE MAIN CENTRE FOR SAFE AND CONVENIENT ACCESS

FOOTPATH ADJACENT TO THE EXISTING TENANCIES PROVIDE SAFE & CONVENIENT ACCESS AND CONNECTS TO THE PACIFIC WAY FOOTPATH.

TRAFFIC ISLANDS FOR PEDESTRIAN REFUGE / TRAFFIC MANAGEMENT / CALMING AND LANDSCAPING

NOTE: ALL FUTURE LAND USES OF EACH TENANCY TO BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE WHITEHORSE PLANNING SCHEME. CARPARKING PROVISION FOR THE NEW DEVELOPMENT AND ANY CHANGES IN USE TO BE PROVIDED IN ACCORDANCE WITH THE RATES SPECIFIED IN THE DEVELOPMENT PLAN OVERLAY, SCHEDULE 1.



| FLOOR AREA TABLE | |
|------------------------------|--------------------------------|
| MAHONEYS ROAD STREET NUMBER | ESTIMATED GFA AS AT MARCH 2008 |
| 01 | 720 |
| 05 | 130 |
| 07 | 230 |
| 09 | 80 |
| 11 | 130 |
| 13 | 180 |
| 15 | 70 |
| 17 | 70 |
| 19 | 80 |
| 21-25 | 540 |
| 27 | 270 |
| 31 | 130 |
| 33 | 330 |
| 35 | 180 |
| 37 | 180 |
| 39 | 380 |
| 41 | 400 |
| 43 | 280 |
| 45 | 330 |
| 47 | 100 |
| 49 | 80 |
| 51 | 130 |
| 53-55 | 350 |
| 57 | 130 |
| 59 | 130 |
| 61 | 130 |
| 63 | 320 |
| 65-67 | 300 |
| 69-73 | 3,080 |
| Total Mahoneys Road | 8,500 |
| Total CFSPM | 8,740 |
| Non-CFSPM (nos 39-41) | 780 |

MAHONEYS ROAD SHOPS DEVELOPMENT PLAN

BUCHANAN
WELLSURVEY

PROJECT NO: 308118
SCALE: 1:200 @ A4
DATE: 11 APRIL 2024
DRAWING NO: ATP 001 REG 1

10.6 – ATTACHMENT 2. Explanatory Report, instruction sheet, draft DPO1 and DDO12

Planning and Environment Act 1987

Whitehorse Planning Scheme

Amendment C262whse

Explanatory Report

Overview

This amendment seeks to facilitate the redevelopment of land at 69 - 79 Mahoneys Road, Forest Hill (existing office building) and an existing at grade car park immediately south of the office building, both of which are located within the Forest Hill Chase Activity Centre.

The amendment removes the existing Development Plan Overlay – Schedule 1 (DPO1) from these sites and introduces a new Design and Development Overlay (DDO12) to guide their future development. The existing Commercial 1 Zone is not proposed to change.

The amendment also removes from DPO1 the need for the provision of '*at least 130 car spaces are provided on the east side of Mahoneys Road*'. This requirement is currently met by the existing at grade car park.

DPO1 currently applies to Forest Hill Chase Shopping Centre, commercial uses on the southern side of Canterbury Road and properties on the eastern side of Mahoneys Road. The office building and car park are privately owned and currently under the same ownership as the Forest Hill Chase Shopping Centre.

DDO12 establishes height, setback, design and access requirements for these sites for the Responsible Authority to consider for any planning permit application. It also provides for the retention of the pedestrian link and the associated landscape strip from Mahoneys Road to Barter Crescent along the south boundary of the car park as part of any redevelopment of the site.

Where you may inspect this amendment

The amendment can be inspected free of charge at the Whitehorse City Council website at www.whitehorse.vic.gov and in hard copy during office hours at Whitehorse City Council, 379-399 Whitehorse Road, Nunawading, 3131.

The amendment can also be inspected free of charge at the Department of Transport and Planning website at planning.vic.gov.au/public-inspection or by contacting the office on 1800 789 386 to arrange a time to view the amendment

10.6 – ATTACHMENT 2. Explanatory Report, instruction sheet, draft DPO1 and DDO12

documentation.

Submissions

Any person may make a submission to the planning authority about the amendment. Submissions about the amendment must be received by [insert submissions due date].

A submission must be sent to: Attn: Strategic Planning Unit, Whitehorse City Council, 379-399 Whitehorse Road, Nunawading, 3131.

Panel hearing dates

In accordance with clause 4(2) of Ministerial Direction No.15 the following panel hearing dates have been set for this amendment:

- Directions hearing: [insert directions hearing date]
- Panel hearing: [insert panel hearing date]

Details of the amendment**Who is the planning authority?**

This amendment has been prepared by the City of Whitehorse, who is the planning authority for this amendment.

The amendment has been made at the request of Haben, owners of Forest Hill Chase Shopping Centre and the office and car park sites in Mahoneys Road, Forest Hill.

Land affected by the amendment

The amendment applies to the land at:

- 69-79 Mahoneys Road, Forest Hill (office site)
- 270 Canterbury Road, Forest Hill, (car park site) formally known as:
 - Lots 125, 126, 127, 128, 129, 139, and 140 on LP50918
 - Lot 1 on TP805768

A mapping reference table is at Attachment 1 to this Explanatory Report.

10.6 – ATTACHMENT 2. Explanatory Report, instruction sheet, draft DPO1 and DDO12

Figure 1: Subject Land

What the amendment does

The amendment facilitates the development and renewal of the subject sites which are located in the southeast corner of the Forest Hill Activity Centre. The amendment will remove the Development Plan Overlay – Schedule 1 (DPO1) from the sites and introduce a new Design and Development Overlay (DDO12) to facilitate redevelopment at an appropriate scale, given their location within a designated activity centre and adjacent to existing residential areas.

It is anticipated that the office building and car park could be redeveloped for commercial or residential land uses. The DDO establishes height and setback requirements as well as design guidelines for the Responsible Authority to consider for any planning permit application.

The amendment also amends DPO1 by removing specific reference to the need to provide 'at least 130 car spaces to the east of Mahoneys Road'. This is a longstanding requirement for the provision of car parking for the Forest Hill Activity Centre which can be accommodated within the existing centre car parking.

10.6 – ATTACHMENT 2. Explanatory Report, instruction sheet, draft DPO1 and DDO12**Strategic assessment of the amendment****Why is the amendment required?**

The amendment is needed to facilitate the redevelopment of specific sites within the Forest Hill Activity Centre and remove outdated planning controls which restrict their development.

The current Development Plan Overlay (DPO1) applying to Forest Hill Activity Centre is over 25 years old. While it has enabled a framework for development and growth of a retail centre, it limits the future development of the subject sites and does not reflect the current positioning of activity centres in providing for a broad range of uses including commercial, retail and residential development.

The existing at grade car park represents an underutilisation of well-located land within an existing activity centre, and the existing office building is aged and no longer provides appropriate commercial space.

Forest Hill Activity Centre is identified in *Plan for Victoria*, the State Government's planning strategy, as an *Activity Centre – Housing Choice and Station* which are 'activity centres with sufficient public transport, facilities and services to be locations for large numbers of new homes.' The activity centre plays a strong regional retail and entertainment role for the eastern suburbs of Melbourne with Forest Hill Chase Shopping Centre at its core and has an existing bus interchange.

Plan for Victoria seeks to provide more homes in well-connected, well-serviced locations around activity centres, train stations and priority precincts. It also includes local government housing targets, with Whitehorse being allocated a target of 76,500 new homes by 2051. The amendment supports the development of activity centres as a focus for high-quality development, activity and living, including forms of higher density living.

The existing Commercial 1 Zone applying to the office and car park sites allows for retail, office and residential development. The amendment replaces the existing DPO with a new Design and Development Overlay (DDO12) that unlocks the potential of the sites to continue growth of the activity centre, whilst ensuring amenity to adjacent residential land. The DDO also retains pedestrian access between Mahoneys Road and Barter Crescent as part of any redevelopment of the car park site.

How does the amendment implement the objectives of planning in Victoria?

The amendment implements the following objectives of planning in Victoria:

(a) to provide for the fair, orderly, economic, and sustainable use, and development of land

10.6 – ATTACHMENT 2. Explanatory Report, instruction sheet, draft DPO1 and DDO12

The amendment will result in the fair, orderly, economic and sustainable use and development of the land by facilitating an outcome that improves the Forest Hill Chase Activity Centre and allows for growth and activation of this part of the centre.

(b) Provide for the protection of natural and man-made resources and the maintenance of ecological processes and genetic diversity

The sites do not have significant ecological characteristics required to be maintained however it is noted that any redevelopment of the car park site will result in the loss of existing mature trees. The proposed DDO provides opportunities for landscaping, including for canopy trees, within setbacks from adjacent residential properties within the car park site.

(c) to secure a pleasant, efficient, and safe working, living and recreational environment for all Victorians and visitors to Victoria

The amendment introduces a new built form framework for the subject sites that actively promotes the ability of the activity centre to provide a high amenity location where people can live and have good access to employment opportunities and services.

(f) Facilitate development in accordance with the objectives

The amendment implements these objectives by removing the DPO that currently restricts development on both sites and introducing a DDO that will facilitate future development in accordance with the site context.

(f1) to facilitate the provision of affordable housing in Victoria

The amendment unlocks the potential for housing to be located on the subject sites, noting that the current DPO requires the car park site to only be used for car parking.

The proposed planning controls do not explicitly require the sites to be used for affordable housing.

(g) to balance the present and future interests of all Victorians

The amendment supports the continued growth and diversification of the Forest Hill Chase Activity Centre, whilst creating the opportunity to redevelop an aged office building and underutilised car park.

How does the amendment address any environmental, social and economic effects?

The amendment will adequately address any environmental, social and economic effects and result in a net community benefit.

Environmental

The amendment contributes to sustainable development outcomes by facilitating increased development densities on underutilised sites within an existing activity centre with good access to public transport, services and facilities and retains an

10.6 – ATTACHMENT 2. Explanatory Report, instruction sheet, draft DPO1 and DDO12

existing local pedestrian link.

Any redevelopment of the car park site will result in the removal of existing mature trees however the proposed DDO will require landscaped setbacks from boundaries abutting the adjacent residential areas

Social

The amendment will result in positive social impacts through:

- allowing the potential for higher density housing within an existing activity centre providing greater housing choice in the local area.
- providing for improvement to the landscaping outcomes at each site, in fostering a greener environment within the activity centre; and
- requiring the provision of new active frontages, creating for a sense of place, and supporting the potential for commercial, retail and employment offerings.

Economic

The amendment unlocks development opportunities on the sites which will provide for economic benefits to the community including the potential for more housing near employment opportunities and services or new retail and commercial offerings.

The amendment allows for more efficient use of the shopping centre surplus car parks and opens up the subject sites for more productive uses.

Retention of underutilised at grade car parking land in an activity centre is not the best use of land.

Does the amendment address climate change?

The amendment has regard to climate change and emissions reduction targets in accordance with section 12(2A) of the *Planning and Environment Act 1987* and Ministerial Direction No. 22 (Climate Change Consideration). The amendment is supported by the preparation of a *Climate Change Consideration Report*.

The amendment addresses climate change by:

- prioritising growth within an established area and in a designated activity centre to reduce transport-related emissions and take advantage of existing urban infrastructure.
- affecting land not known to be exposed to natural hazards.
- strengthening walkability through the retention of a key pedestrian link, reducing reliance on private vehicles, allowing for residents within the wider area to access the activity centre.
- introducing built form controls for future development to incorporate solar access, passive design, energy efficiency and urban cooling and greening through landscaped setbacks from adjacent residential areas providing for canopy trees and biodiversity outcomes.

10.6 – ATTACHMENT 2. Explanatory Report, instruction sheet, draft DPO1 and DDO12

- making efficient use of resources and assists in prioritising opportunities for infill housing.

Does the amendment address relevant bushfire risk?

The subject site is not located within the Bushfire Management Overlay, nor within a designated 'Bushfire Prone Area'. The amendment will not increase the risk to life, property, infrastructure, or the natural environment from bushfire.

Does the amendment comply with the requirements of any other Minister's Direction applicable to the amendment?

The amendment complies with Ministerial Direction No. 11 (Strategic Assessment of Amendments) under section 12 of the *Planning and Environment Act 1987*. The amendment is consistent with this direction which ensures a comprehensive strategic evaluation of a planning scheme amendment and the outcomes it produces the potential for redevelopment of underutilised land within an activity centre context.

How does the amendment support or implement the Planning Policy Framework and any adopted State policy?

The amendment supports the following state provisions within the Planning Policy Framework (PPF):

- Clause 11.01-1S (Settlement) – the amendment will facilitate an outcome that provides for commercial and/or housing growth in a highly accessible area with regard to services, transport and employment, whilst preserving amenity of surrounding residential areas.
- Clause 11.01-1R (Settlement – Metropolitan Melbourne) – the amendment aligns with strategies that maintain a permanent urban growth boundary to create a more consolidated, sustainable city and supports focussing of investment and growth in activity centres.
- Clause 16.-01-1S (Housing Supply) – the amendment unlocks development potential to encourage higher density housing development on sites well located in relation to jobs, services and public transport and contribute towards the achievement of the housing target.
- Clause 17.02-1S (Business) – the amendment encourages development that meets the community's needs for retail, entertainment, office and other commercial services.
- Clause 18.02-1S (Walking) - the amendment prioritises walking and cycling by including a requirement in the proposed DDO to retain the existing pedestrian link from Mahoneys Road to Barter Crescent through the south boundary of the car park site.

The amendment supports local policy as follows:

- Clause 11.03-1L (Activity Centres) - the amendment unlocks development

10.6 – ATTACHMENT 2. Explanatory Report, instruction sheet, draft DPO1 and DDO12

opportunity within the Forest Hill Activity Centre while ensuring the preservation of amenity to residential areas bordering the activity centre, through the implementation of appropriate built form controls.

- Clauses 15.01-2L (Environmentally sustainable development) and 16.01-1L (Housing change) - the amendment allows for the repurposing of underutilised land to enable the appropriate provision of residential land within an activity centre with excellent access to transport, services and employment.
- Clause 15.01-5L (Preferred neighbourhood character) - appropriate built form controls ensure the preservation and promotion of amenity to surrounding residential land located within the Garden Suburban Precinct 6.
- Clause 15.01-5L-01 (Tree conservation) - appropriate setbacks allow for landscaping within a future development at interfaces with the public realm and adjacent residential areas.
- Clause 18.02-1L (Sustainable personal transport) - the proposal replaces an existing car park deemed surplus to the car parking requirements of the centre and retains key pedestrian link to ensure permeability and access to the centre utilising sustainable transport modes.

Is the amendment consistent with the delivery of the relevant housing target set out in the Planning Policy Framework?

The amendment is consistent with the delivery of the housing target for Whitehorse set out in the Planning Policy Framework. Whilst the extent of new housing will be relatively small in the context of the total Whitehorse housing target, the location of the sites within the Forest Hill Chase Activity Centre ensures that if housing is developed on these sites, future residents will be close to jobs, services and public transport.

How does the amendment support or implement the Municipal Planning Strategy?

The amendment is consistent with the Municipal Planning Strategy (MPS) and supports its effective implementation by unlocking underutilised land within a designated activity centre (Clause 02.03-1 - Settlement). The MPS seeks to ensure activity centres are safe, attractive and developed in accordance with their role in the network of activity centres. It envisages that any new development will improve the amenity and visual appearance of activity centres, contribute to the City's economy and respond to the needs of the community.

The amendment introduces bespoke built form requirements for the sites to respond to their specific context. The proposed building heights and setbacks consider the amenity and character of adjacent residential areas, while balancing this with their activity centre location and commercial zoning (Clause 02.03-5 - Built environment and heritage). Set back requirements allow for the provision of landscaping at the site's interfaces with the public realm and adjacent residential areas (Clause 02.03-2

10.6 – ATTACHMENT 2. Explanatory Report, instruction sheet, draft DPO1 and DDO12

- Environment and landscape values). Unlocking underutilised land within an activity centre as an opportunity for additional housing (Clause 02.03-6 - Housing) and employment (Clause 02.03-7 -Economic development) and will activate and regenerate this part of the centre.

The amendment retains the pedestrian link through the car park site supporting sustainable movement options within the centre for local residents and allows for housing near public transport, further promoting the use of sustainable transport (Clause 02.03-8 - Transport).

Does the amendment make proper use of the Victoria Planning Provisions?

The application of a DDO to replace the existing DPO will provide greater certainty with respect to the design and form of future development on the site, in line with local and state policy. The existing DPO stifles the opportunity for development of the car park site and does not provide any appropriate parameters for future built form.

The DDO is a well-used mechanism to ensure that the sites subject to the amendment are provided with appropriate, and site responsive planning controls.

How does the amendment address the views of any relevant agency?

The views of relevant agencies will be sought during exhibition of the amendment.

Does the amendment address relevant requirements of the *Transport Integration Act 2010*?

The amendment is consistent with and supports all the stated transport system objectives contained within the *Transport Integration Act 2010*.

How does the amendment have regard to the principles set out in the *Yarra River Protection (Wilip-gin Birrarung murrn) Act 2017* in relation to Yarra River land and other land, the use or development of which may affect Yarra River land?

The *Yarra River Protection (Wilip-gin Birrarung murrn) Act 2017* does not apply to the subject land.

Resource and administrative costs**What impact will the new planning provisions have on the resource and administrative costs of the responsible authority?**

10.6 – ATTACHMENT 2. Explanatory Report, instruction sheet, draft DPO1 and DDO12

The amendment supports improved decision making for applications for development within the activity centre and will not present any additional resource or administrative costs for the responsible authority.

Attachment 1 – Mapping reference table

| Location | Land /Area Affected | Mapping Reference | Changes to Planning Scheme |
|-------------|---|--|-------------------------------|
| Forest Hill | 69-79 Mahoneys Road, Forest Hill | Whitehorse C262whse Map05 DDO Exhibition | Amend Map 5DDO to apply DDO12 |
| | | Whitehorse C262whse Map05 d-DPO Exhibition | Amend Map 5DPO to delete DPO1 |
| Forest Hill | Car park site immediately south of 69-79 Mahoneys Road, Forest Hill (270 Canterbury Road, Forest Hill [formally known as: Lots 125, 126, 127, 128, 129, 139, and 140 on LP50918 and Lot 1 on TP805768]) | Whitehorse C262whse Map05 DDO Exhibition | Amend Map5DDO to apply DDO12 |
| | | Whitehorse C262whse Map05 d-DPO Exhibition | Amend Map 5DPO to delete DPO1 |

10.6 – ATTACHMENT 2. Explanatory Report, instruction sheet, draft DPO1 and DDO12

Planning and Environment Act 1987

Whitehorse Planning Scheme**Amendment C262whse****Instruction sheet**

The planning authority for this amendment is Whitehorse City Council.

The Whitehorse Planning Scheme is amended as follows:

Planning Scheme Maps

The Planning Scheme Maps are amended by a total of 2 attached map sheets.

Overlay Maps

1. Amend Planning Scheme Map No 5DDO. in the manner shown on the 1 attached map marked "Whitehorse Planning Scheme, Amendment C262whse".
2. Amend Planning Scheme Map No 05DPO in the manner shown on the 1 attached map marked "Whitehorse Planning Scheme, Amendment C262whse".

Planning Scheme Ordinance

The Planning Scheme Ordinance is amended as follows:

3. In **Overlays**- Clause 43.02, insert a new Schedule in the form of the attached document.
4. In **Overlays** – Clause 43.04, replace Schedule 1 with a new Schedule 1 in the form of the attached document.

End of document

10.6 – ATTACHMENT 2. Explanatory Report, instruction sheet, draft DPO1 and DDO12**WHITEHORSE PLANNING SCHEME**

xx/xx/xxxx
Cxxxwhse

SCHEDULE 1 TO CLAUSE 43.04 DEVELOPMENT PLAN OVERLAY

Shown on the planning scheme map as **DPO1**.

FOREST HILL ACTIVITY CENTRE

1.0
21/07/2022
C222whse

Objectives

None specified.

2.0
21/07/2022
C222whse

Requirement before a permit is granted

None specified.

3.0
21/07/2022
C222whse

Conditions and requirements for permits

A permit granted must include the following conditions:

- All buildings and works must be maintained to the satisfaction of the responsible authority.
- All landscaping and associated works must be maintained to the satisfaction of the responsible authority.

4.0
xx/xx/xxxx
Cxxxwhse

Requirements for development plan

A development plan must include the following requirements:

Buildings and works

- Details of the location, height, dimensions, design, floor area, elevations and external materials, finish and colours of all buildings and works.
- The proposed uses of buildings.
- The location and layout of all car parking areas and access to and from them.
- The location of public transport facilities, access to them and passenger facilities, including taxi stands, bus interchange area, bicycle racks and pedestrian accessways.
- Details of drainage.
- The location and layout of loading and unloading areas, including details of waste storage areas and collection and removal facilities.
- Measures to enhance the urban design character of the Centre.

Management

- General amenity provisions including noise control and external lighting effects.
- Operational matters relating to such things as control of nuisance, waste collection, vehicle loading and unloading, management and maintenance of car parking areas and trolley control.
- Adequate loading facilities.
- Effective screening of loading and storage areas from roads and nearby residential areas.
- Measures to minimise the impacts of waste handling methods and operating procedures on nearby residential areas.
- Measures to minimise the impact of the development with respect to visual presentation, noise and privacy issues on the amenity and character of the surrounding area.

Traffic and transport

- Traffic management and traffic control works considered necessary on the land and impacted roads.

10.6 – ATTACHMENT 2. Explanatory Report, instruction sheet, draft DPO1 and DDO12**WHITEHORSE PLANNING SCHEME**

- Measures to help minimise after-hours use of Mahoneys Road by Centre patrons.
- Measures to appropriately provide for pedestrian movement along and across Pacific Way and Mahoneys Road north of Pacific Way.
- Adequate car parking which is easily accessible from the surrounding street network.
- Car parking in the ratio of at least 5 car spaces to each 100 square metres of leasable floor area for a shop and 4 car spaces to each 100 square metres of leasable floor area for all other uses, or a lesser figure to the satisfaction of the responsible authority.
- ~~At least 130 car spaces to the east of Mahoneys Road.~~
- Adequate public transport and associated facilities for public transport users.
- Safe and efficient pedestrian and cyclist routes to, from and on the land and adjoining roads.

Landscaping

- Effective landscaping to act as a visual screen to adjoining residential areas and to soften the appearance of buildings, roads and car parking areas.
- Landscape improvements along the Canterbury Road frontage to the land.
- Landscaping proposed in the road reserve of Mahoneys Road and on land to the north of its intersection with Pacific Way.
- Appropriate acoustic fencing and associated landscaping along the south and west side of Pacific Way.
- All other proposed landscaping.

Architectural features of the buildings and building services may exceed the building envelope levels shown on the development plan by up to 5 metres.

The following may be located in the building setback areas shown on the development plan:

- Minor utility installations.
- Verandahs to a maximum height of 3.5 metres above ground level.
- Light poles and bollards.
- Flagpoles.
- Fences.
- Signs.
- Litter bins.
- Planter boxes.
- Seats.
- Public telephones.

10.6 – ATTACHMENT 2. Explanatory Report, instruction sheet, draft DPO1 and DDO12

WHITEHORSE PLANNING SCHEME

xx/xx/20xx
C262whse**SCHEDULE 12 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY**Shown on the planning scheme map as **DDO12**.**69-79 MAHONEYS ROAD, FOREST HILL AND CAR PARK ON EAST SIDE OF MAHONEYS ROAD, FOREST HILL****1.0 Design objectives**xx/xx/20xx
C262whse

To encourage opportunities to improve street activation and a sense of place on key sites within the Forest Hill Activity Centre.

To ensure new development is designed to respond to the immediate site environs and minimises potential off-site impacts such as noise (including from services), overlooking, access to sunlight, and light spillage on adjoining residential properties.

To ensure new buildings incorporate design detail that provides a high quality and visually interesting interface with the streetscape and the surrounding residential area, addressing issues of amenity, functionality, adaptability and accessibility.

To ensure an appropriate landscape response to adjacent residential interfaces and streets.

2.0 Buildings and worksxx/xx/20xx
C262whse

The following buildings and works requirements apply to an application to construct a building or construct or carry out works.

In calculating the building height based on storeys, the following floor to floor dimensions should apply:

- 3.5m for residential use,
- 4.0m for non-residential use, 5.0m at ground level.

Building height is the vertical distance between the footpath or natural surface level at the centre of the site frontage and the highest point of the building, with the exception of architectural features and building services.

Building design

- New development should provide for high quality design, finishes and articulation.

Building heights

- Buildings and works should not exceed the preferred maximum building height specified in the table and map to this schedule.
- An application to exceed the preferred maximum building height must demonstrate how the development will continue to achieve the Design Objectives and Built Form Outcomes of this schedule, and any local planning policy requirements.
- The preferred maximum building height excludes rooftop services which should be hidden from view from any adjoining public space or designed as architectural roof top features. Rooftop services includes but is not limited to plant rooms, air conditioning, lift overruns and roof mounted equipment.
- Where the property abuts a residentially zoned site, all measurements of height should be taken from the abutting residential property boundary.

Building setbacks

- Buildings and works should be in accordance with the mandatory and preferred setbacks specified in the table to this schedule.
- Properties abutting a residential zone should provide transitional upper level setbacks at the residential interface to maintain the amenity of adjoining residential properties.

10.6 – ATTACHMENT 2. Explanatory Report, instruction sheet, draft DPO1 and DDO12

WHITEHORSE PLANNING SCHEME

- Built form for non-residential uses at ground level should provide active frontages to streetscapes.
- Recessed upper levels are preferred to reduce the appearance of building bulk.
- Buildings should provide a landscaped setback where they abut adjacent residential areas and streets.
- An application to vary the preferred setbacks must demonstrate how the development will continue to achieve the Design Objectives and Built Form Outcomes of this schedule and any local planning policy requirements.

Access, carparking and pedestrian link

- Development should provide for safe and appropriate access to Mahoneys Road and abutting laneways.
- Vehicle access and movement from Barter Crescent should be discouraged.
- Vehicle ingress and egress to the sites are to be provided via Mahoneys Road and abutting laneways, to ensure that proposed development does not have an unreasonable impact on safety and amenity of the surrounding local residential road network.
- Development must maintain the pedestrian connection between Mahoneys Road and Barter Crescent, in accordance with Map 01.

Permit condition requirement

- In deciding to grant a permit for the development of the Carpark site, the Responsible Authority must include a condition that requires the owner of the land to enter into an agreement with the Responsible Authority under section 173 of the Act regarding the retention of the southern pedestrian pathway (landscaped strip adjacent to southern boundary and pedestrian path) connecting Mahoneys Road to Barter Crescent, to the satisfaction of the Responsible Authority.

Table to Schedule 12

| Property | Preferred maximum height | Preferred setback to Mahoneys Road | Preferred minimum rear* setbacks | Preferred and mandatory minimum side* setbacks, where specified | Built form outcome |
|----------------------------------|--------------------------|------------------------------------|---|---|--|
| 69-79 Mahoneys Road, Forest Hill | 17m | 0m | Up to 13m building height: <ul style="list-style-type: none"> ▪ 0m (preferred) Above 13m building height: <ul style="list-style-type: none"> ▪ 5m | 0m (preferred) | The scale of development provides an appropriate interface with Mahoneys Road and adjacent residential area. Additional overshadowing impacts to neighbouring residential land are mitigated. |

10.6 – ATTACHMENT 2. Explanatory Report, instruction sheet, draft DPO1 and DDO12

WHITEHORSE PLANNING SCHEME

| Property | Preferred maximum height | Preferred setback to Mahoneys Road | Preferred minimum rear* setbacks | Preferred and mandatory minimum side* setbacks, where specified | Built form outcome |
|---|--------------------------|--|---|--|---|
| Carpark (east side of Mahoneys Road, Forest Hill) | 21m | <p>Up to 11m building height:</p> <ul style="list-style-type: none"> 1m from the eastern edge of the footpath fronting Mahoneys Road <p>Above proposed street wall up to 18m in building height:</p> <ul style="list-style-type: none"> 3m from street wall <p>Over 18m building height:</p> <ul style="list-style-type: none"> 3m from the street wall + 1m per additional 1m height | <p>Where the title boundary directly interfaces Barter Crescent:</p> <p>Up to 10.5 m building height:</p> <ul style="list-style-type: none"> 5m The setback area should be heavily landscaped <p>Between 10.5 up to 15m height:</p> <ul style="list-style-type: none"> 5m plus 1m per additional 1m height above 10.5m <p>Above 15m, up to 20m height:</p> <ul style="list-style-type: none"> 10m <p>Above 20m height:</p> <ul style="list-style-type: none"> 10m plus 1m per additional 1m height above 20m <p>Where the title boundary abuts the General Residential Zone to the east:</p> <p>Up to 9m building height:</p> <ul style="list-style-type: none"> 8m setback. The setback area should | <p>Where the title boundary abuts the General Residential Zone (south):</p> <ul style="list-style-type: none"> Up to 7m building height: 4m (mandatory) Between 7m and 10.5m building height : 5m (preferred) Between 10.5m and 14m building height: 9m (preferred) Between 14m and 17.5m building height: 10m (preferred) Above 17.5m building height: 11m (preferred) <p>Where the title boundary abuts the Commercial 1 Zone (North):</p> <p>Up to 11m building height:</p> <ul style="list-style-type: none"> 0m (preferred) <p>Over 11.5m building height:</p> <ul style="list-style-type: none"> 4.5m (preferred) <p>Where the title boundary abuts the General Residential Zone to the north=:</p> | <p>The scale of development provides an appropriate interface with the activity centre core and residential areas.</p> <p>East-west pedestrian link along southern property boundary and landscaping to the setback from residential property to the south.</p> <p>Development provides a consistent street wall along Mahoneys Road and design detail at south-western corner of site to reduce building bulk adjacent to pedestrian link and residential property to the south.</p> <p>Should not shadow adjoining residential land between 9am and 3pm on the September Equinox.</p> |

10.6 – ATTACHMENT 2. Explanatory Report, instruction sheet, draft DPO1 and DDO12

WHITEHORSE PLANNING SCHEME

| Property | Preferred maximum height | Preferred setback to Mahoneys Road | Preferred minimum rear* setbacks | Preferred and mandatory minimum side* setbacks, where specified | Built form outcome |
|------------------------|--------------------------|------------------------------------|--|--|--------------------|
| 21/07/2022 C222whse | | | be heavily landscaped Above 9m building height: 8m plus 4m per additional 3m height above 9m | Up to 7m height: ▪ 2m (preferred). The setback area should be heavily landscaped. Between 7m up to 14m height: ▪ 2m plus 1m per additional 1m of height above 7.5 m (preferred) Above 14m, up to 20m height: ▪ 10m (preferred) Above 20m height: ▪ 10m plus 1m per additional 1m height above 20m (preferred) | |

**Refer to Map 01 to this Schedule for identification of side and rear setback locations.*

3.0 Subdivision

xx/xx/20xx
C262whse None specified.

4.0 Signs

xx/xx/20xx
C262whse None specified.

5.0 Application requirements

xx/xx/20xx
C262whse None specified.

10.6 – ATTACHMENT 2. Explanatory Report, instruction sheet, draft DPO1 and DDO12

WHITEHORSE PLANNING SCHEME

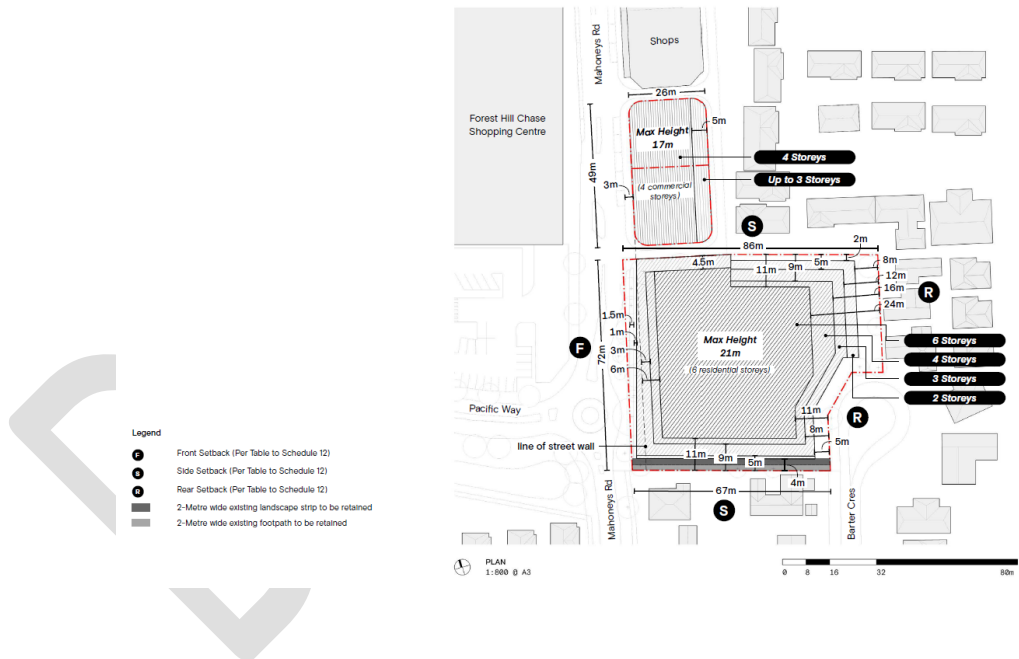
6.0 Decision guidelines

xx/xx/20xx
C262whse

The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specified in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:

- Whether an application to exceed the preferred maximum building height and/or vary the preferred setbacks demonstrates how the development will achieve the Design Objectives and Built Form Outcomes of this schedule, and any local planning policy requirements;
- Whether the proposed buildings or works achieve high or superior architectural quality;
- Whether the proposed buildings or works achieve an equivalent or better design outcome;
- Whether the proposed buildings or works minimise impacts on amenity;
- Whether the design responds to the site context, including heights and setbacks on adjoining properties and vehicular and pedestrian access to the local road network.

Map 01



10.6 – ATTACHMENT 3. Traffic Report

eukai

23 February 2026

| | |
|------------------|--|
| Client: | Haben |
| Eukai Reference: | 24033 |
| Primary Contact: | Andrew Farran, Director, Eukai Pty Ltd |

69-79 Mahoneys Road, Forest Hill
Transport Impact Assessment

10.6 – ATTACHMENT 3. Traffic Report



Acknowledgement of Country

In the spirit of reconciliation, Eukai acknowledges the Traditional Custodians of country throughout Australia and their connections to land, sea, and community. We pay our respect to their Elders past and present, and we extend that respect to all Aboriginal and Torres Strait Islander peoples.

Issue Record

| Issue | Date | Status | Prepared By | Reviewed By | Signature |
|-------|----------|--------|-------------|---------------|-----------|
| A | 23/02/26 | Final | - | Andrew Farran | |

© Eukai Pty Ltd 2026 [ABN 45 676 072 165] The information and intellectual property contained in this document is the property of Eukai Pty Ltd. Use or copying of this document in whole or in part without the written permission of Eukai Pty Ltd constitutes an infringement of copyright. This document has been prepared on behalf of and for the exclusive use of Enkai's client and is subject to the agreement between Eukai Pty Ltd and its client. Eukai Pty Ltd accepts no liability or responsibility whatsoever for any use or reliance upon this document by any third party.

10.6 – ATTACHMENT 3. Traffic Report



Table of Contents

1 Introduction..... 4

 1.1 Background..... 4

 1.2 Structure of this Report..... 6

 1.3 References 6

2 Existing Context 7

 2.1 Site Location 7

 2.2 Transport Network 8

 2.3 Car Parking..... 9

 2.3.1 Supply.....9

 2.3.2 Surveys.....10

 2.3.3 Car Parking Demands.....10

3 Future Parking Situation..... 14

 3.1 Preamble 14

 3.2 Car Parking Availability 14

 3.3 Shopping Centre Car Parking Requirement..... 16

 3.3.1 Preamble16

 3.3.2 DPO Rates Assessment.....16

 3.3.3 Clause 52.06 Assessment.....17

 3.3.4 Summary18

 3.4 Mahoneys Road Site Car Parking Controls 18

4 Council Request for Further Information..... 20

 4.1 Preamble 20

 4.2 Car Parking Supply..... 20

 4.3 Traffic Survey Methodology 20

 4.4 Future vehicular and pedestrian access 22

 4.5 Shopping Centre Car Parking Requirement..... 22

5 Summary 23

Appendix A..... 24

 Existing Car Parking Plans 24

Appendix B 25

 Car Parking Survey Data 25

Appendix C 26

 Shopping Centre Door Count Data 26

10.6 – ATTACHMENT 3. Traffic Report

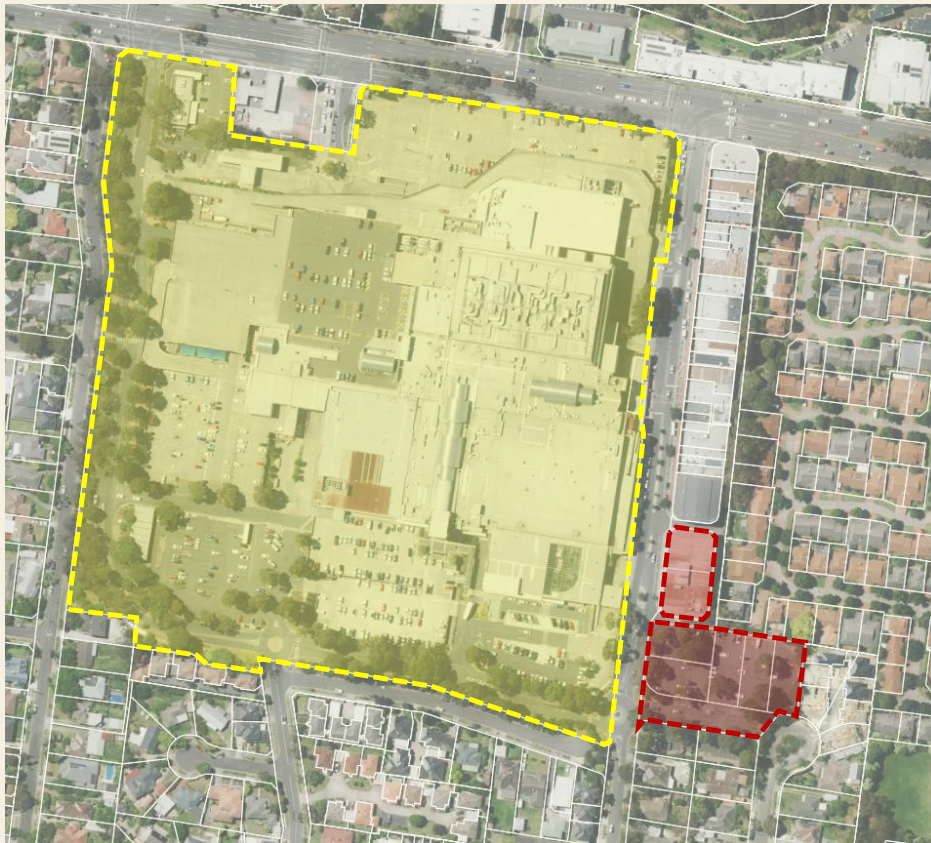
1 Introduction

1.1 Background

The Forest Hill Chase Shopping Centre (Shopping Centre) currently comprises of the main centre and two smaller sites (an office building located at 69-79 Mahoneys Road and an adjacent at-grade car park) located on the east side of Mahoneys Road. The Applicant (Haben) is seeking to divest the two sites on the east side of Mahoneys Road.

The location of the main centre (highlighted yellow) and the two sites on the east side of Mahoneys Road (highlighted red) is shown in **Figure 1.1**.

Figure 1.1: Aerial Photo of Forest Hill Chase Shopping Centre

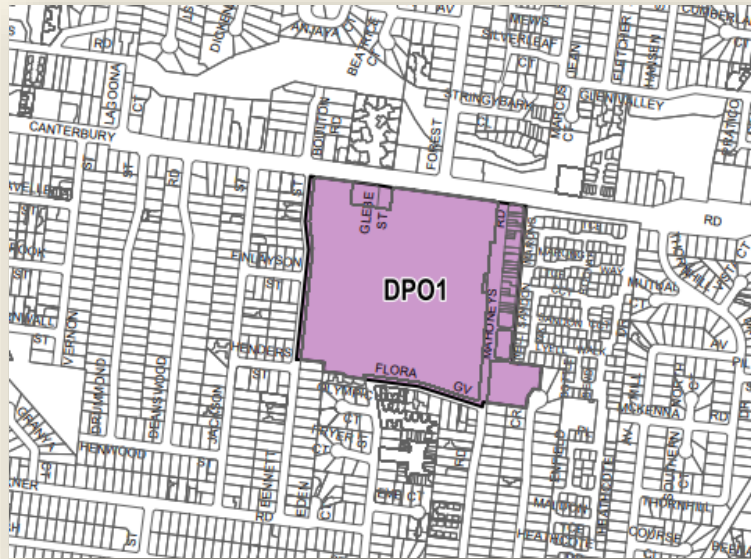


(Source: Metromap - <https://web.metromap.com.au/auth/login>)

The site is located in the Forest Hill Activity Centre and as such, is subject to the planning controls contained in Schedule 1 to Clause 43.04 Development Plan Overlay (DPO). The land subject to the DPO is reproduced below, noting that it includes the Shopping Centre proper and the two Mahoneys Road sites

10.6 – ATTACHMENT 3. Traffic Report

eukai



The controls relating to traffic and transport have also been reproduced below:

Traffic and transport

- Traffic management and traffic control works considered necessary on the land and impacted roads.
- Measures to help minimise after-hours use of Mahoneys Road by Centre patrons.
- Measures to appropriately provide for pedestrian movement along and across Pacific Way and Mahoneys Road north of Pacific Way.
- Adequate car parking which is easily accessible from the surrounding street network.
- Car parking in the ratio of at least 5 car spaces to each 100 square metres of leasable floor area for a shop and 4 car spaces to each 100 square metres of leasable floor area for all other uses, or a lesser figure to the satisfaction of the responsible authority.
- At least 130 car spaces to the east of Mahoneys Road.
- Adequate public transport and associated facilities for public transport users.
- Safe and efficient pedestrian and cyclist routes to, from and on the land and adjoining roads.

In order to divest the properties, a Planning Scheme Amendment is being sought to modify the existing planning controls for the two sites located on the east side of Mahoneys Road. Primarily, the Planning Scheme Amendment seeks to:

- Remove the following requirement from the DPO – “At least 130 car spaces to the east of Mahoneys Road.”
- Removal of the office and car park sites on the east side of Mahoneys Road from the DPO to facilitate the redevelopment of these sites independent of the Shopping Centre.
- Revert to the Commercial 1 Zone for the office and car park sites to the east of Mahoneys Road.
- Introduction of new built form controls for the office and car park sites to the east of Mahoneys Road.

10.6 – ATTACHMENT 3. Traffic Report



This report assesses the implications of removing the requirement to provide at least 130 car spaces for the Shopping Centre on the east side of Mahoneys Road, as well as confirming the appropriateness of the future car parking controls for the sites.

Eukai was engaged by Haben in September 2025 to prepare this assessment to accompany the Planning Scheme Amendment application.

1.2 Structure of this Report

This report sets out an assessment of the anticipated transport implications of the proposed Planning Scheme Amendment. It entails the following sections:

- **Section 2** presents an overview of relevant transport conditions for the site.
- **Section 3** presents an assessment of the adequacy of the proposed future car parking provision for the Shopping Centre and the future parking controls for the office and car park sites.
- **Section 4** provides a response to Council's request for further information.
- **Section 5** presents a summary of key findings and recommendations contained within this report.

1.3 References

The report has been prepared having reference to the following (amongst other documents):

- Whitehorse Planning Scheme (and Overlays)
- Car parking demand surveys of the Shopping Centre and on-street car parking
- Empirical evidence as sourced in this report
- Site inspection of the Shopping Centre and surrounds
- Other documents as identified.

10.6 – ATTACHMENT 3. Traffic Report



2 Existing Context

2.1 Site Location

The Forest Hill Chase Shopping Centre is located at 270 Canterbury Road in Forest Hill, approximately 18km east of the Melbourne CBD.

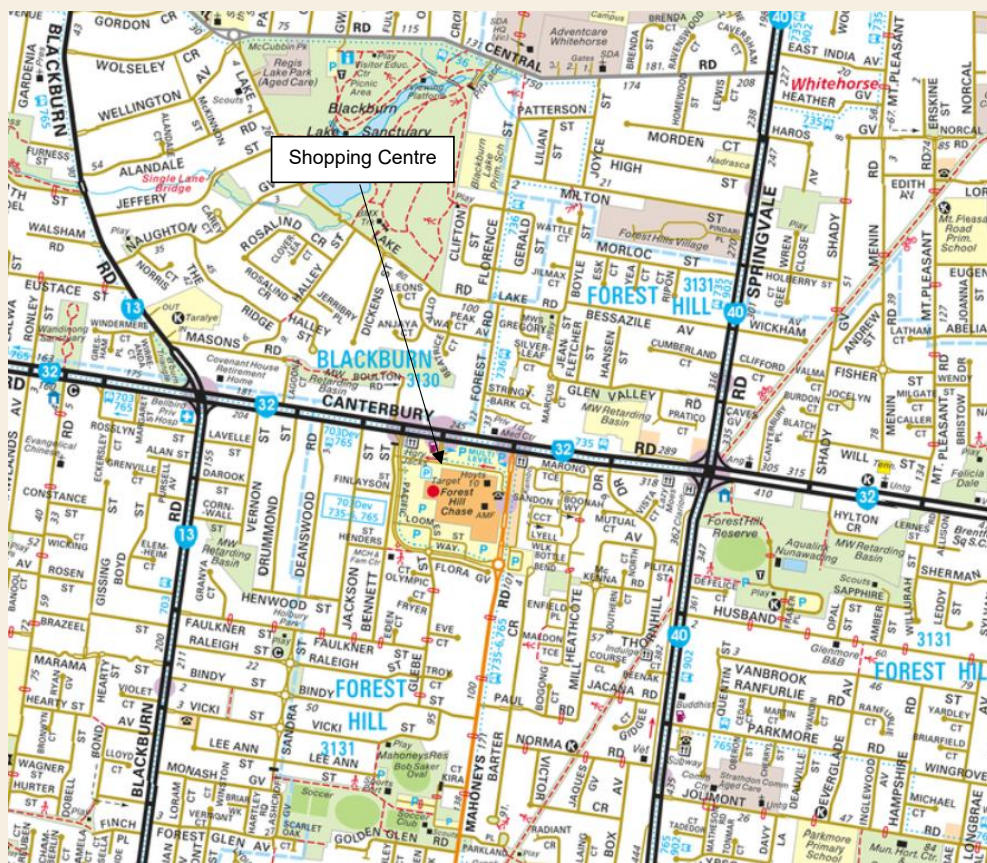
The Shopping Centre (proper) is bound by Canterbury Road to the north, Mahoneys Road to the east and Pacific Way to the south and west. The Centre has a frontage of approximately 270m to Canterbury Road, which is zoned as Transport Zone 2 (i.e. an arterial road under the management of the Department of Transport & Planning (DTP)).

The Mahoneys Road sites are located at 69-79 Mahoneys Road (existing office building) and the at-grade car park located to the immediate south of 69-79 Mahoneys Road.

The surrounding properties are predominantly low density residential, with commercial properties located on the east side of Mahoneys Road opposite the Shopping Centre.

The location of the Shopping Centre and the surrounding transport network is shown in **Figure 2.1**.

Figure 2.1: Subject Site and Surrounds



(Source: Melways Online - <https://online.melway.com.au/melway/>)

10.6 – ATTACHMENT 3. Traffic Report






2.2 Transport Network

The Centre is located on canterbury Road which forms a key east-west arterial road through the eastern suburbs of Melbourne. Springvale Road is a key north-south arterial road located approximately 600m to the east of the Centre.

A summary of the characteristics of the transport network in the immediate vicinity of the site is provided in **Table 2.1**.

Table 2.1: Existing Transport Network Characteristics

| Mode | Description |
|---|--|
| Active Travel  | <ul style="list-style-type: none"> • Pedestrian footpaths are provided on both sides of the frontage roads. • Pedestrian connectivity is provided in all directions from the Centre via the existing footpath network. • Signalised pedestrian crossings across Canterbury Road are provided at the 3 signalised intersections along the site's frontage. • Pedestrian Operates Signals (POS) are provided on Mahoneys Road at the main pedestrian access to the Centre. • The Centre has an overall walk score of 91 out of 100 which indicates that the site is a "walker's paradise" (69-79 Mahoneys Road has a score of 90 out of 100). • There are limited dedicated bike facilities in the vicinity of the Centre. |
| Public Transport  | <ul style="list-style-type: none"> • A dedicated bus interchange is provided at the Centre and services the following bus routes: <ul style="list-style-type: none"> – Bus Route 703 – Blackburn to Middle Brighton – Bus Route 735 – Nunawading to Box Hill – Bus Route 736 Blackburn to Mitcham – Bus Route 765 – Box Hill to Mitcham • Blackburn and Nunawading Railway Stations are approximately 1.8km to the north of the Centre. • The Centre has an overall transit score of 48 out of 100 which indicates that the site has "a few nearby public transport options" (69-79 Mahoneys Road also has a score of 90 out of 100).. |
| Private Vehicle  | <ul style="list-style-type: none"> • Canterbury Road is primary arterial road forming a key east-west link through the eastern suburbs of Melbourne and carries approximately 31,000vpd (under VicRoads/DTP management)^[1]. • Mahoneys Road is classified as a Major Council Road, whilst Pacific Way is classified as a local road (both under Council management). |

[1] Traffic volumes sourced from <https://vicroadsopendata-vicroadsmaps.opendata.arcgis.com/datasets/traffic-volume>

10.6 – ATTACHMENT 3. Traffic Report

2.3 Car Parking

2.3.1 Supply

The Shopping Centre has a current leasable floor area of approximately 60,500sqm, including 43,935sqm of shop area and 16,552sqm of other area.

There are a total 3,114 car parking spaces provided at the Shopping Centre, equivalent to a rate of 5.15 car spaces per 100sqm. Car parking for the Shopping Centre is currently provided as follows:

- Main shopping centre car park: 2,937 spaces (which reduces to 2,793 post the sports courts)
- Mahoneys Road car park: 177 spaces (including 20 spaces reserved for dialysis parking)

In addition to the above, there are currently 28 publicly accessible on-street car parking spaces (excluding loading and taxi zones) provided on Mahoneys Road between Canterbury Road and Pacific Way.

A breakdown of the car parking spaces by restriction and level for the main shopping centre car park is presented in **Table 2.2**. Plans showing each of the car parking levels at the Centre are provided in **Appendix A**.

Table 2.2: Car Parking Summary

| Restriction | Southern Car Parks | Blue / B1 | Yellow / B | Red / Ground | Green / L1 | Level 2 | Level 3 | Total |
|--------------------|--------------------|------------|------------|--------------|------------|------------|------------|--------------|
| Unrestricted | 320 | 312 | 318 | 877 | - | 235 | 153 [1] | 2,215 |
| 3P | - | - | - | - | 219 | - | - | 219 |
| 2P | 152 | - | - | - | - | - | - | 152 |
| ≤30 minutes | 9 | - | - | - | - | - | 2 | 11 |
| Disabled Parking | 9 | 4 | 4 | 26 | 7 | 9 | 10 | 69 |
| Parents with Prams | 4 | - | - | 13 | 27 | 6 | 11 | 61 |
| Customer Pick Up | 2 | - | - | - | 3 | 1 | - | 6 |
| Car Wash | - | - | - | 21 | - | 6 | - | 27 |
| Electric Vehicle | - | - | - | - | 2 | - | - | 2 |
| Other [2] | 25 | - | - | - | 3 | 1 | 2 | 31 |
| Total | 521 | 316 | 322 | 937 | 261 | 258 | 178 | 2,793 |

[1] Includes the sports courts development which reduces the Level 3 provision from 297 to 153 spaces.

[2] Includes Australia Post (1), Maintenance (3), Green Leaves (21), Salvation Army (3), loading zone (2), and Emergency Vehicle parking spaces (1).

10.6 – ATTACHMENT 3. Traffic Report



2.3.2 Surveys

Car parking demand surveys of the main shopping centre car park, Mahoneys Road car park and on-street car parking on Mahoneys Road between Canterbury Road and Pacific Way, were commissioned by Eukai. The car parking demand surveys were undertaken at hourly intervals during the following periods:

- Friday 23 August 2024 10:00am to 9:00pm
- Saturday 24 August 2024 10:00am to 5:00pm

It is noted that the surveyed days (Friday and Saturday) were chosen as they corresponded to the busiest days at the Centre.

2.3.3 Car Parking Demands

A summary of the surveyed car parking demands for the main shopping centre car park, Mahoneys Road car park and on-street car parking are presented below, with full results of the car parking demand surveys provided in **Appendix B**.

It is noted that visitation to the Shopping Centre fluctuates throughout the year, noting that the winter months (including August when the car parking surveys were undertaken) typically have lower visitation than other months. Accordingly, the car parking demands surveyed have been factored to represent the 85th percentile demand, as follows:

- Friday demands x 1.13
- Saturday demands x 1.22

Further details regarding how these factors were calculated is provided in **Appendix C**. It is noted that the factors have only been applied to the surveyed Shopping Centre car park demands (and not to the Mahoneys Road off-street car parking or Mahoneys Road on-street car parking).

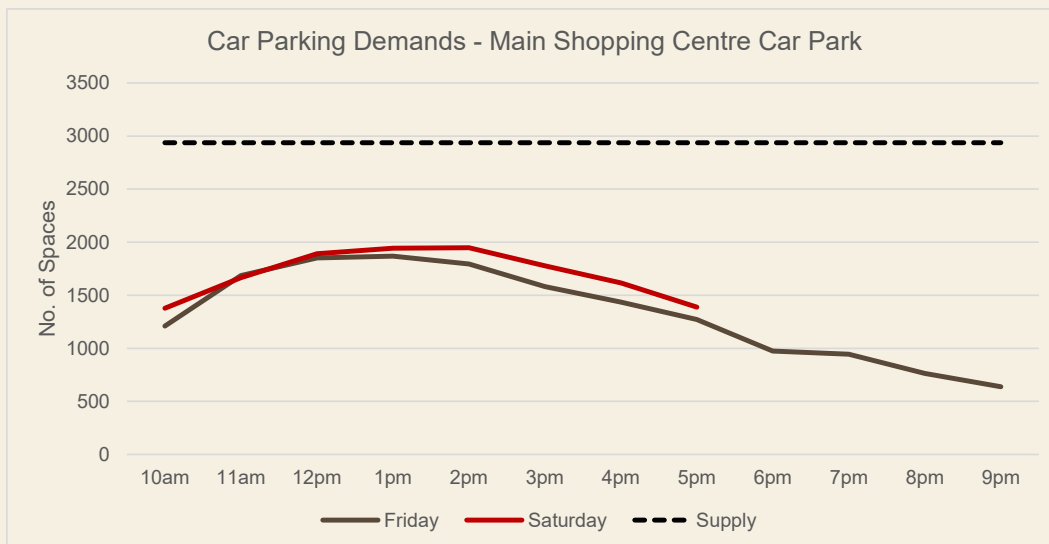
Main Shopping Centre Car Park

The 85th percentile car parking demands for the main shopping centre car park (excluding the Mahoneys Road car park) is presented in **Figure 2.2**. The data indicates a peak car parking demand of 1,948 spaces at 2:00pm on the Saturday, which equates to a peak occupancy of 66%.

10.6 – ATTACHMENT 3. Traffic Report



Figure 2.2: Car Parking Demand Survey Results – Main Shopping Centre Car Park



The surveys of the main centre car park indicate that there are approximately 1,000 vacant parking spaces at peak times on the 85th percentile design day. Further interrogation of the car parking surveys indicates that the following car parking areas experience the highest level of vacancies:

- Yellow car park basement level 2 – circa 250 spaces
- Blue car park basement level 1 – circa 250 spaces
- Red car park ground level – circa 150 spaces (generally located in the northeast corner of the site)
- Level 3 rooftop car park – circa 150 spaces
- Mycar car park – circa 70 spaces

Note: vacancies associated with expected 85th percentile demand. The Level 3 rooftop vacancies are expected to be used by the proposed basketball court development and as such, are unlikely to be available in the future.

It is observed that the yellow, blue and red car parks are located at the northern end of the Shopping Centre (multi-deck car park), with the spaces on the eastern side of these levels proximate to the existing Mahoneys Road strip shopping areas.

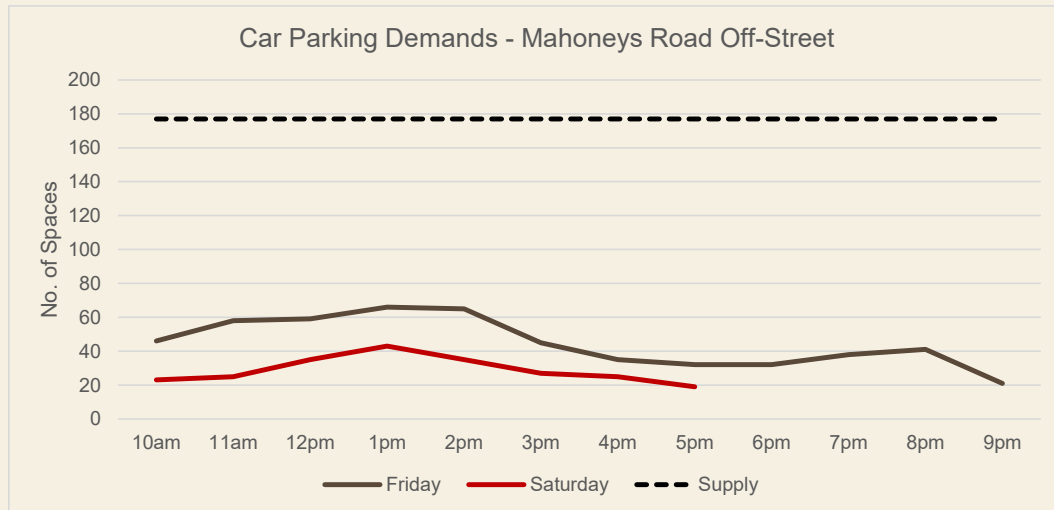
Mahoneys Road Car Park

The existing surveyed car parking demands for the Mahoneys Road car park are presented in **Figure 2.3**. The data indicates a peak car parking demand of 66 spaces at 1:00pm on the Friday, which equates to a peak occupancy of 37%. For reference, a peak car parking demand of 35 spaces was observed at 2pm on the Saturday.

10.6 – ATTACHMENT 3. Traffic Report



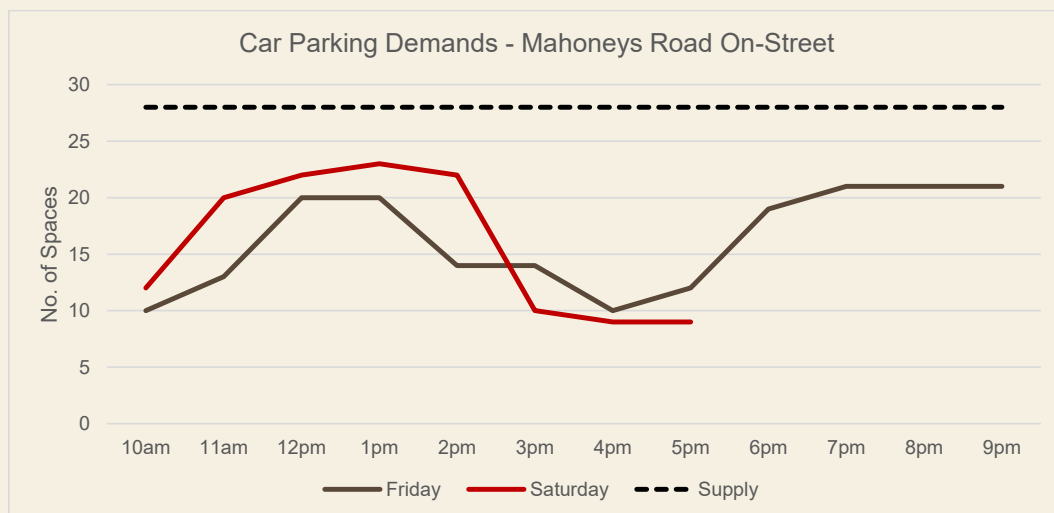
Figure 2.3: Car Parking Demand Survey Results – Mahoneys Road Car Park



On-Street Car Parking

The existing surveyed on-street car parking demands for Mahoneys Road between Canterbury Road and Pacific Way is presented in Figure 2.4. The data indicates a peak car parking demand of 23 spaces at 1:00pm on the Saturday, which equates to a peak occupancy of 82%.

Figure 2.4: Car Parking Demand Survey Results – Mahoneys Road On-Street Car Parking



Summary

A summary of the existing car parking supplies, peak demands, minimum vacancies and occupancies of the main centre, Mahoneys Road off-street car park and Mahoneys Road on-street parking is presented in Table 2.3.

10.6 – ATTACHMENT 3. Traffic Report



Table 2.3: Summary Car Parking Supplies, Demands and Occupancies

| Car Park | Supply | Peak Demand | Minimum Vacancies | Occupancy | Day / Time |
|-------------------------------|--------------|--------------|-------------------|-----------|-----------------|
| Main Shopping Centre Car Park | 2,937 spaces | 1,948 spaces | 989 spaces | 66% | Saturday at 2pm |
| Mahoneys Road Car Park | 177 spaces | 66 spaces | 111 spaces | 37% | Friday at 1pm |
| Mahoneys Road On-Street | 28 spaces | 23 spaces | 5 spaces | 82% | Friday at 1pm |

Typically, an occupancy of 85% is considered the effective capacity of a car parking area, with occupancies above 85% resulting in vehicles having to circulate through car parking areas looking for a vacant space (a higher occupancy threshold is typically adopted where car parking guidance measures are implemented).

In this instance the main centre and Mahoneys Road off-street car parks are operating well below their effective capacities of 85%. The on-street car parking is operating near its effective capacity of 85%.

Key Findings

It is evident that car parking demands and occupancies within the existing Mahoneys Road car park are low. Furthermore, it is observed that car parking demands and occupancies within the main shopping centre car park are moderate.

Given that customers are able to park at the main shopping centre car park (which offers greater level of convenience and accessibility to the Centre itself), it is considered reasonable to assume that the majority of existing car parking demands within the Mahoneys Road car park are associated with the office use at 69-79 Mahoneys Road and/or the strip shopping on Mahoneys Road and not the Centre itself. This is further highlighted by 20 of the spaces within the Mahoneys Road car park being allocated for dialysis patients use only, noting that the Forest Hill Dialysis Clinic is located at 69-79 Mahoneys Road and not within the Shopping Centre proper.

In summary, it is evident that the Mahoneys Road car park is unlikely to be used to accommodate Shopping Centre car parking demands.

10.6 – ATTACHMENT 3. Traffic Report

3 Future Parking Situation

3.1 Preamble

As identified in **Section 1.1**, the DPO currently has a requirement that “at least 130 car spaces to the east of Mahoneys Road” are provided for the Shopping Centre. As part of this application approval is being sought to remove this requirement from the DPO, as well as remove the two sites on Mahoneys Road from the DPO.

In this respect an assessment is presented below, including:

- The impact on car parking availability for the Centre and surrounding areas should the at-grade car parking spaces be removed (refer **Section 3.2**).
- An assessment of the Centre statutory car parking requirements against the reduced car parking provision without the Mahoneys Road car park (refer **Section 3.3**).
- The alternative car parking controls for the two sites on Mahoneys Road when they are removed from the DPO (refer **Section 3.4**).

The above matters are discussed below.

3.2 Car Parking Availability

As identified earlier, the main centre car park currently operates with a peak occupancy of 66%, with a minimum 993 vacancies (85th percentile design day). The analysis also concludes that the Mahoneys Road off-street car park likely caters for car parking demands generated by the office building located at 69-79 Mahoneys Road and the strip shopping on Mahoneys Road and it is unlikely to cater for car parking demands generated by the Shopping Centre.

Notwithstanding, a conservative assessment (on the high side) is presented below which assumes that all of the existing car parking demands in the Mahoneys Road off-street car park are redistributed to the main centre car park.

The assessment also includes consideration of the approved basketball courts' development at the Shopping Centre¹, which is:

- Forecast to increase car parking demands by 69 spaces.
- Reduce the available car park supply by 144 spaces (2,937 spaces to 2,793 spaces)

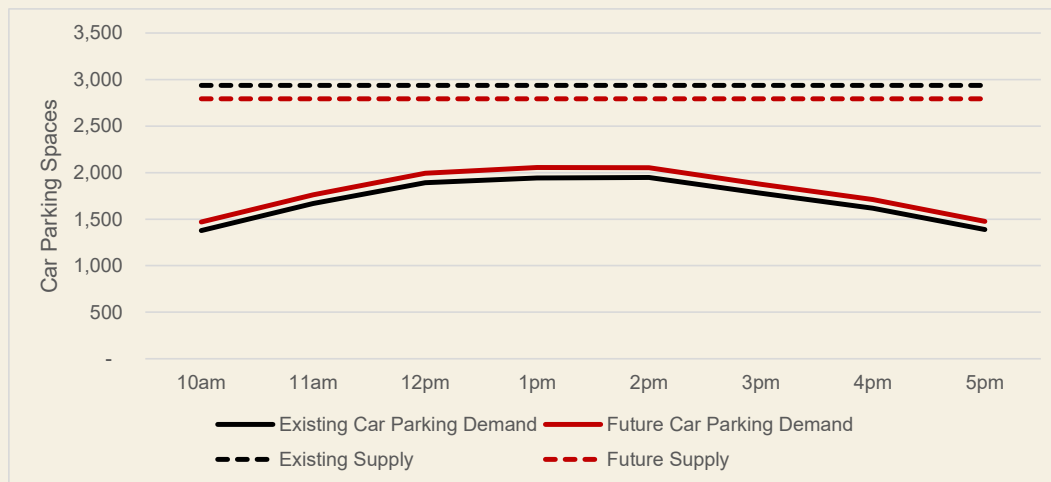
The forecast future car parking supply and demand across the course of a typical Saturday (peak day) is presented in **Figure 3.1**.

¹ Planning Permit WH/2025/71 for the use of the land for a minor sports and recreation facility.

10.6 – ATTACHMENT 3. Traffic Report



Figure 3.1: Forecast Car Parking Supply and Demand on the 85th Percentile Saturday



A summary of the future car park supply, peak car park demand, minimum vacancies and occupancy at the peak time (Saturday at 2pm) is presented in Table 3.1.

Table 3.1: Summary Car Parking Supplies, Demands and Occupancies

| Scenario | Supply | Peak Demand | Minimum Vacancies | Occupancy |
|--------------------------------|------------------------------|-----------------------------|-------------------|-----------|
| Existing Main Car Park | 2,937 spaces | 1,948 spaces | 989 spaces | 66% |
| + Basketball Court Development | 2,793 spaces (=2,937-144) | 2,017 spaces (=1,948+69) | 776 spaces | 72% |
| + Mahoneys Road Car Park | 2,793 spaces | 2,052 spaces (=2,017+35) | 741 spaces | 73% |

It is evident that ample vacancies exist at the existing main centre car park to accommodate the future demands associated with the basketball court development and any redistributed demands from the Mahoneys Road car park. It is noted that the Mahoneys Road car parking demands are not considered to be associated with the Shopping Centre and as such, the above assessment is considered conservative on the high side.

Given the proximity of the Mahoneys Road off-street car park to the strip shopping on Mahoneys Road it is expected that some of the displaced car parking could be associated with the strip shopping uses. In this respect, it is observed that the bulk of existing Shopping Centre vacancies were observed in the multi-deck car park at the northern end of the site in close proximity to the strip shopping uses.

Based on the above, it is considered appropriate from a car parking perspective to remove the “at least 130 car spaces to the east of Mahoneys Road” requirement from the DPO.

10.6 – ATTACHMENT 3. Traffic Report

3.3 Shopping Centre Car Parking Requirement

3.3.1 Preamble

The existing statutory car parking requirements for the site are set out in Schedule 1 to Clause 43.04 Development Plan Overlay 'Forest Hill Activity Centre' of the Whitehorse Planning Scheme. The car parking rates are reproduced below:

“Car parking in the ratio of at least 5 car spaces to each 100 square metres of leasable floor area for a shop and 4 car spaces to each 100 square metres of leasable floor area for all other uses, or a lesser figure to the satisfaction of the responsible authority.”

It is understood that when the Development Plan Overlay rates were implemented they were lower than the then minimum Clause 52.06 statutory car parking requirements (i.e. a minimum 'shop' statutory car parking rate of 8 spaces per 100sqm was applicable). At the time, the Development Plan Overlay implemented rates (which were lower than the equivalent Clause 52.06 rates) that took into consideration the sites comparatively higher public transport accessibility and strategic desire to reduce car usage at the site.

Given that the currently applicable statutory rates for shop (3.5 vs 5 spaces per 100sqm) and other uses (3.5 vs 4 spaces per 100sqm)² are lower than the Development Plan Overlay rates, the current Development Plan Overlay rates are considered outdated and overly conservative.

An assessment of the statutory car parking requirements for the Shopping Centre adopting the DPO rates and the Clause 52.06 rates are presented below.

3.3.2 DPO Rates Assessment

An assessment of the statutory car parking requirements (adopting the DPO car parking rates) of the overall development is set out in **Table 3.2**.

Table 3.2: DPO Car Parking Requirements

| Use | Size | Statutory Parking Rate | Statutory Parking Requirement |
|--------------------------------------|-----------|------------------------|--------------------------------|
| Shop | 43,935sqm | 5 spaces per 100sqm | 2,197 spaces |
| Other (existing) | 16,552sqm | 4 spaces per 100sqm | 662 spaces |
| Other (additional) | 2,203sqm | | 88 spaces |
| Total | | | 2,947 spaces |
| Supply (Main Centre + Mahoneys Road) | | | 2,970 spaces (surplus = 23) |
| Future Supply (Main Centre Only) | | | 2,793 spaces (shortfall = 154) |

Note: includes consideration of the proposed basketball court development.

² Based on the statutory car parking requirement for bar, food & drink, restaurant, hotel and medical centre land uses.

10.6 – ATTACHMENT 3. Traffic Report



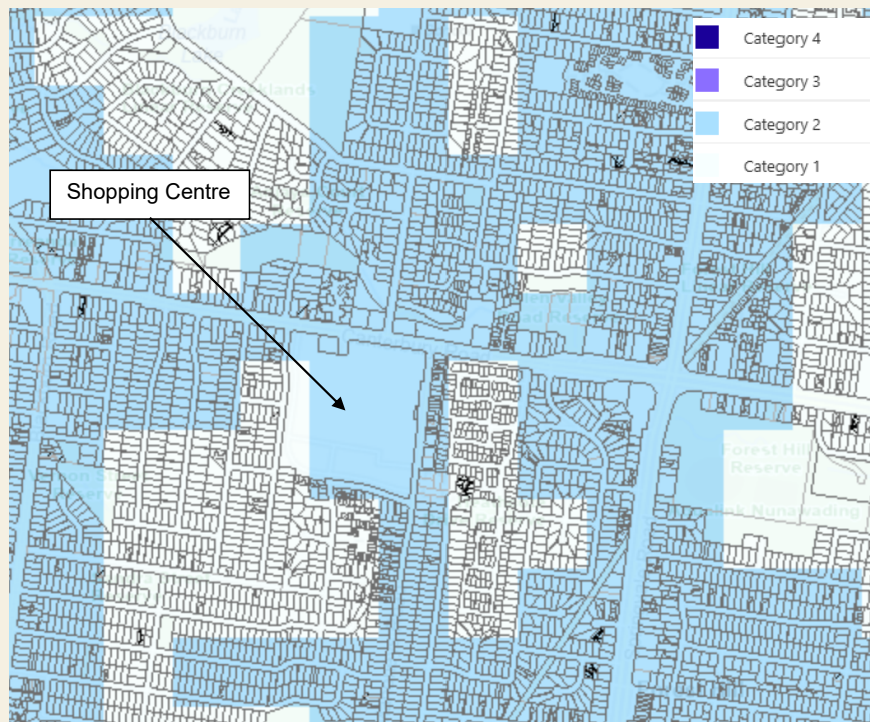
Table 3.2 indicates that the removal of the Mahoneys Road off-street car park will result in a shortfall of 150 car parking spaces for the Shopping Centre against the DPO car parking requirements.

3.3.3 Clause 52.06 Assessment

The typical car parking requirements for the developments located in Whitehorse LGA are set out in Clause 52.06 of the Planning Scheme.

A recent Planning Scheme Amendment (VC277), which was gazetted late December 2025, has resulted in updated statutory parking rates. The parking rates are informed by public transport accessibility levels (PTAL) with the subject site located within Category 2 (i.e., moderate PTAL) of the Car Parking Requirements Maps as shown in **Figure 3.2**. Category 2 zones are subject to minimum statutory car parking requirements.

Figure 3.2: Subject Site related to CPR Maps – Category 1



An assessment of the statutory car parking requirement for the proposed development is set out in **Table 3.3**³.

³ In comparison to the previous statutory car parking rates, the updated car parking rates result in a lower statutory car parking requirement and as such, in accordance with the transition requirements of Clause 52.06 the updated statutory car parking requirement for the development have been presented.

10.6 – ATTACHMENT 3. Traffic Report



Table 3.3: Statutory Car Parking Requirements (Updated Clause 52.06 Rates)

| Land Use | Size | Statutory Car Parking Rate | Statutory Car Parking Requirement |
|--------------------------------------|-------------------------------|--------------------------------------|-----------------------------------|
| Supermarket | 9,627sqm | 2.5 spaces per 100sqm | 240 spaces |
| Shop | 34,308sqm | 2 spaces per 100sqm | 686 spaces |
| Medical Centre | 1,136sqm | 3.5 spaces per 100sqm | 39 spaces |
| Childcare Centre | 2,400sqm (assume 30 staff) | 0.5 spaces per employee | 15 spaces |
| Other (F&B, etc.) | 13,016sqm | 2 spaces per 100sqm | 260 spaces |
| Basketball Courts | 3 courts | 23 spaces per court (empirical rate) | 69 spaces |
| Total | | | 1,309 spaces |
| Supply (Main Centre + Mahoneys Road) | | | 2,970 spaces (surplus = 1,661) |
| Future Supply (Main Centre Only) | | | 2,793 spaces (surplus = 1,484) |

Table 3.3 indicates that the removal of the Mahoneys Road off-street car park will result in a continued surplus of parking spaces for the Shopping Centre against Clause 52.06 car parking requirements.

Adoption of the updated statutory car parking rates indicates a minimum statutory car parking requirement of 1,309 spaces. For references application of the now superseded statutory car parking rates indicated a minimum statutory car parking requirement of 2,329 spaces (including 69 spaces for the basketball courts).

3.3.4 Summary

The DPO states that car parking can be provided at *“a lesser figure to the satisfaction of the responsible authority”*. As described above the DPO parking rates are considered to be conservative on the high side and not reflective of contemporary parking requirements for the Shopping Centre (and surrounding areas).

Given that the car parking surveys indicate that there are ample vacancies in the main centre car park (minimum 745 spaces) and that an assessment against the standard statutory car parking requirements indicates a surplus (1,484 spaces), the proposed parking arrangements for the Shopping Centre without the Mahoneys Road off-street car park are considered to be appropriate.

3.4 Mahoneys Road Site Car Parking Controls

It is proposed to remove the Mahoneys Road properties from the DPO zoning and adopt a standard Commercial 1 zoning. As such, the associated car parking controls for the Mahoneys Road sites would be modified from the DPO rates:

10.6 – ATTACHMENT 3. Traffic Report

“Car parking in the ratio of at least 5 car spaces to each 100 square metres of leasable floor area for a shop and 4 car spaces to each 100 square metres of leasable floor area for all other uses, or a lesser figure to the satisfaction of the responsible authority.”

To the Category 2 car parking rates in Clause 52.06 of the Whitehorse Planning Scheme, noting that both Mahoneys Road sites sit within the Category 2 car parking requirements (refer **Figure 3.2** above).

As described above, the current DPO rates were implemented when the default statutory car parking rates were much higher. The current rates are not considered to reflect a contemporary approach to car parking and as such, the adoption of the default statutory car parking rates for the properties on the east side of Mahoneys Road is considered appropriate. Indeed, whilst not part of this Planning Scheme Amendment the removal of the car parking controls from the DPO for the Shopping Centre proper would be considered to be an appropriate car parking and transport planning outcome.

Key Findings

There are ample vacancies at the main centre car park to accommodate the existing Shopping Centre demands, the approved basketball courts development demands and any displaced demands from the removal of the Mahoneys Road off-street car park. It is observed that the majority of the future vacancies are expected to be in the multi-deck car park at the northern end of the Shopping Centre.

The removal of the Mahoneys Road off-street car park results in a shortfall of car parking when assessed against the DPO car parking rates, however, the DPO car parking rates are not representative of contemporary car parking practice. When assessed against the updated Clause 52.06 statutory car parking rates the Centre has a 1,484 space surplus following the removal of the Mahoneys Road off-street car park.

Based on the analysis presented in this section it is considered appropriate to remove the requirement to provide 130 car spaces for the Centre on the east side of Mahoneys Road and to adopt the default Clause 52.06 car parking rates for the rezoning of the two sites on the east side of Mahoneys Road.

10.6 – ATTACHMENT 3. Traffic Report

4 Council Request for Further Information

4.1 Preamble

Whitehorse City Council undertook a pre development application review of the proposal and issued a transport specific request for further information on 22 January 2026. Each of the Council items is reproduced below in bold with a response provided thereafter.

4.2 Car Parking Supply

Please provide a car parking plan generally in accordance with Clause 52.06 requirements, showing all car spaces provided for Forest Hill Chase Shopping Centre (whether on the land or other land) to verify the numbers in the report. Information on any restrictions relating to the use of car parking spaces (such as the proportion of car spaces allocated to staff parking, pick up areas, specific uses, car wash areas, long term parking for commuters etc) should also be provided.

The provision of current car parking plans was raised in my email of 22/12/25 and I now consider this to be essential information for Council to consider the amendment proposal.

The architectural plans showing each of the car parking levels is provided in Appendix A, with a breakdown of car parking spaces by restriction and car park level provided in Section 2.2.

4.3 Traffic Survey Methodology

We request that car parking surveys are undertaken over 2 days for each carpark originally surveyed. We are also aware that the dates the survey was taken (August 2025) may have coincided with significant renovations to the centre which could affect the results. Any new surveys should be undertaken outside of school holidays. Following the completion of the survey, the report should be updated, including the key findings, to reflect the results.

Car parking surveys were conducted over 2 days, including a weekday and a Saturday in August 2024. In order to account for any seasonal fluctuations the surveyed day was benchmarked against 12 months of doorcount data with the 85th percentile Friday and Saturday demand determined. The data indicates that there are significant vacancies within the existing car park.

Rather than undertaking additional car parking surveys (at considerable expense to the Applicant) Eukai has undertaken a review of available aerial photography of the Centre which demonstrates the available vacancies in the at-grade and rooftop car parks. It is noted that the car parking surveys indicate that the majority of vacancies are located in the basement car parks at the northern end of the Centre, however, it is reasonable to assume that if there are vacancies in the open air car park (which are have higher occupancies than the basements at the northern end of the Centre) that there would continue to be vacancies in the basement car parks.

The aerial photography has been sourced from MetroMap and includes captures from January 2026 (most recent) and for days in December 2022, 2024 and 2025, noting that historically December represents the busiest trading month for shopping centres.

The aerial photos indicate that there continue to be ample vacancies in the at-grade and rooftop car parks and by extension it is reasonable to assume that there are ample vacancies in the basement car parking areas at the northern end of the Centre.

10.6 – ATTACHMENT 3. Traffic Report



Figure 4.1: Monday 5 January 2026



Figure 4.2: Wednesday 17 December 2025

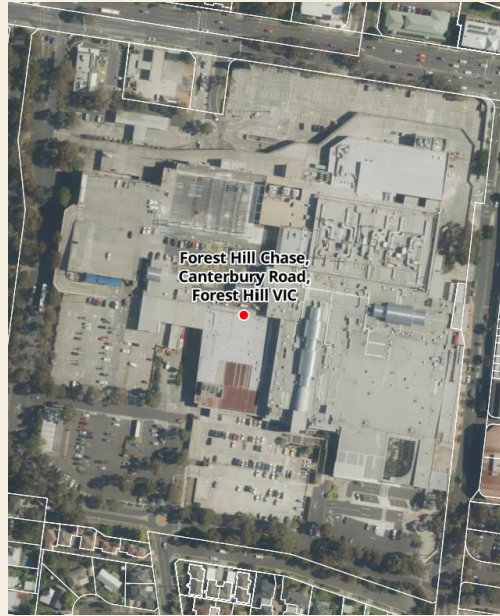


Figure 4.3: Thursday 5 December 2024

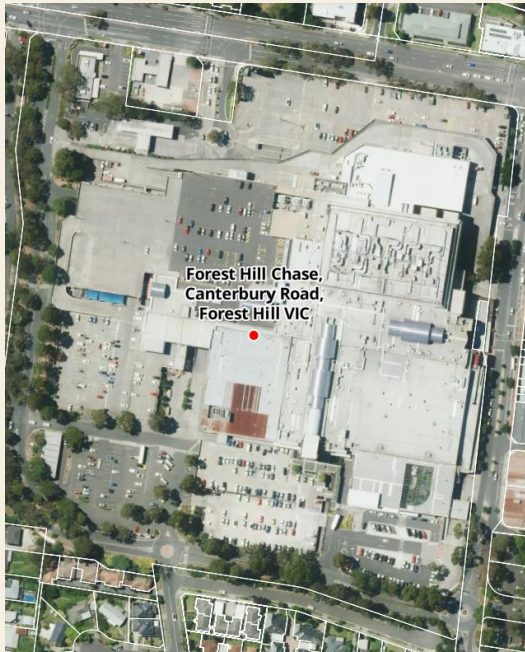
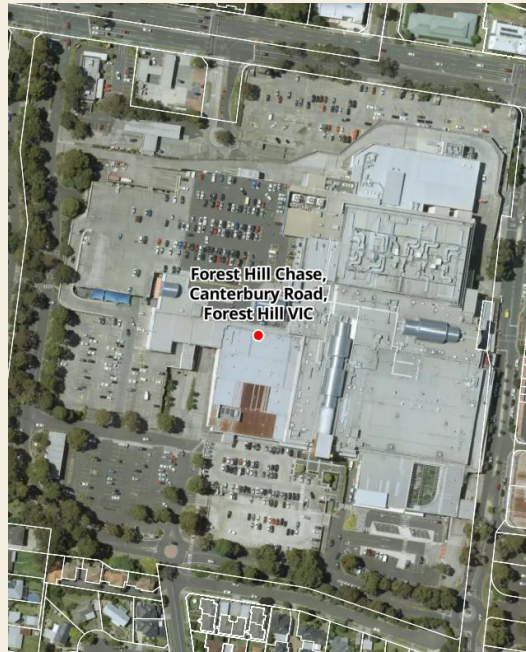


Figure 4.4: Sunday 18 December 2022



10.6 – ATTACHMENT 3. Traffic Report



4.4 Future vehicular and pedestrian access

We previously provided advice that the Design and Development Overlay (see my email of 12/12/26) and that it include a requirement for safe and appropriate access to the sites from surrounding streets and laneways. These are additional comments from this round of feedback:

- *Vehicular access from Mahoneys Road should be maintained at the northern end of the car park site, or the furthest point from the roundabout.*
- *No commercial vehicular access from Barter Crescent (given the land is zoned C1Z)*
- *Maintaining the east-west pedestrian access along the southern boundary of the carpark land, and the north-south footpath within the land nearby the western frontage to Mahoneys Road.*

Some commentary from Eukai on the management of safe and appropriate vehicular (including loading) access to the subject sites with any redevelopment, given their location, the existing conditions and site constraints, would be useful to determine whether any site-specific requirements should be added to the DDO schedule in this regard.

Vehicle, loading cycling and walking access to any future development of the Mahoneys Road sites will be assessed as part of any future planning permit application and will need to be assessed against the relevant access requirements of the Whitehorse Planning Scheme.

In respect to the specific commentary from Council, the following is noted:

- Primary vehicle access to any future commercial development is recommended to be derived from Mahoneys Road (i.e. to the north of the existing roundabout).
- Any potential secondary vehicle access to Barter Crescent would need to be assessed against the requirements of the Planning Scheme, including consideration of any impacts to the residential properties fronting Barter Crescent.
- Any future development will maintain an east-west pedestrian link connecting Barter Crescent to Mahoneys Road.

4.5 Shopping Centre Car Parking Requirement

As advised in my previous email (22/12/25), please update the report to provide an assessment of the car parking requirements under the new Clause 52.06 provisions in the Whitehorse Planning Scheme.

An assessment of the statutory car parking requirements adopting the updated Clause 52.06 rates (gazetted in December 2025) is presented in Section 3.3. The assessment indicates a statutory car parking requirement of 1,309 spaces for the shopping centre. This is less than the previously calculated statutory car parking requirement of 2,329 spaces calculated using the now superseded Clause 52.06 rates.

Importantly, it is highlighted that the shopping centre car parking provision (2,793 spaces), without the Mahoneys Road car park, still exceeds the minimum statutory car parking requirement.

10.6 – ATTACHMENT 3. Traffic Report

5 Summary

The following conclusions are reached based on the discussions and analysis contained in this report:

- There are ample vacancies in the existing main centre car park to accommodate any displaced car parking demands associated with the removal of the Mahoneys Road off-street car park.
- Despite there being a car parking shortfall against the DPO car parking requirement for the main centre car park following the removal of the Mahoneys Road car park, there is a surplus of car parking against Clause 52.06 car parking requirements.
- Accordingly, it is considered appropriate to remove the *“at least 130 car spaces to the east of Mahoneys Road”* car parking requirement from the DPO.
- The adoption of the default Clause 52.06 car parking rates for the two sites to the east side of Mahoneys Road is considered appropriate.

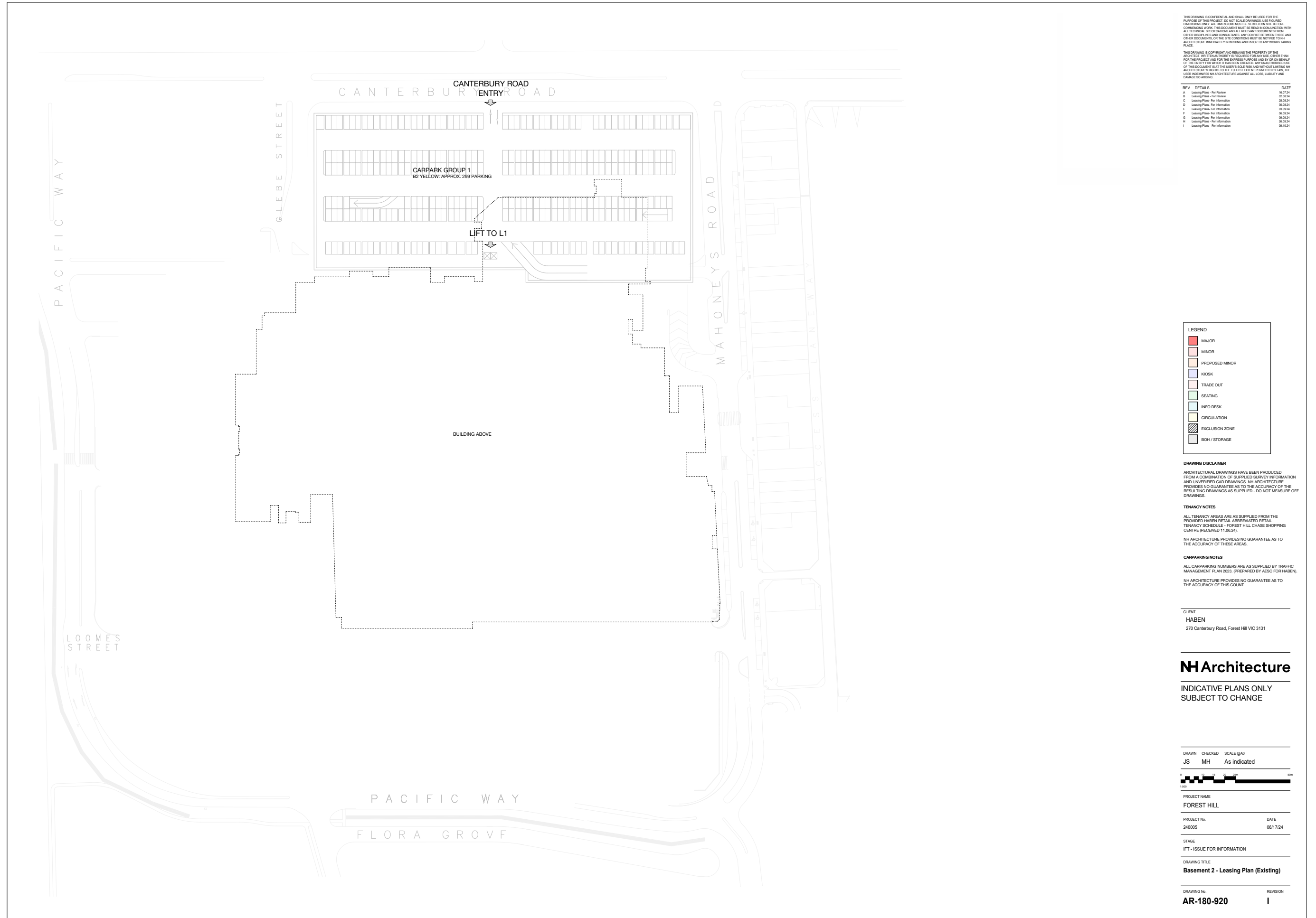
10.6 – ATTACHMENT 3. Traffic Report

eukai

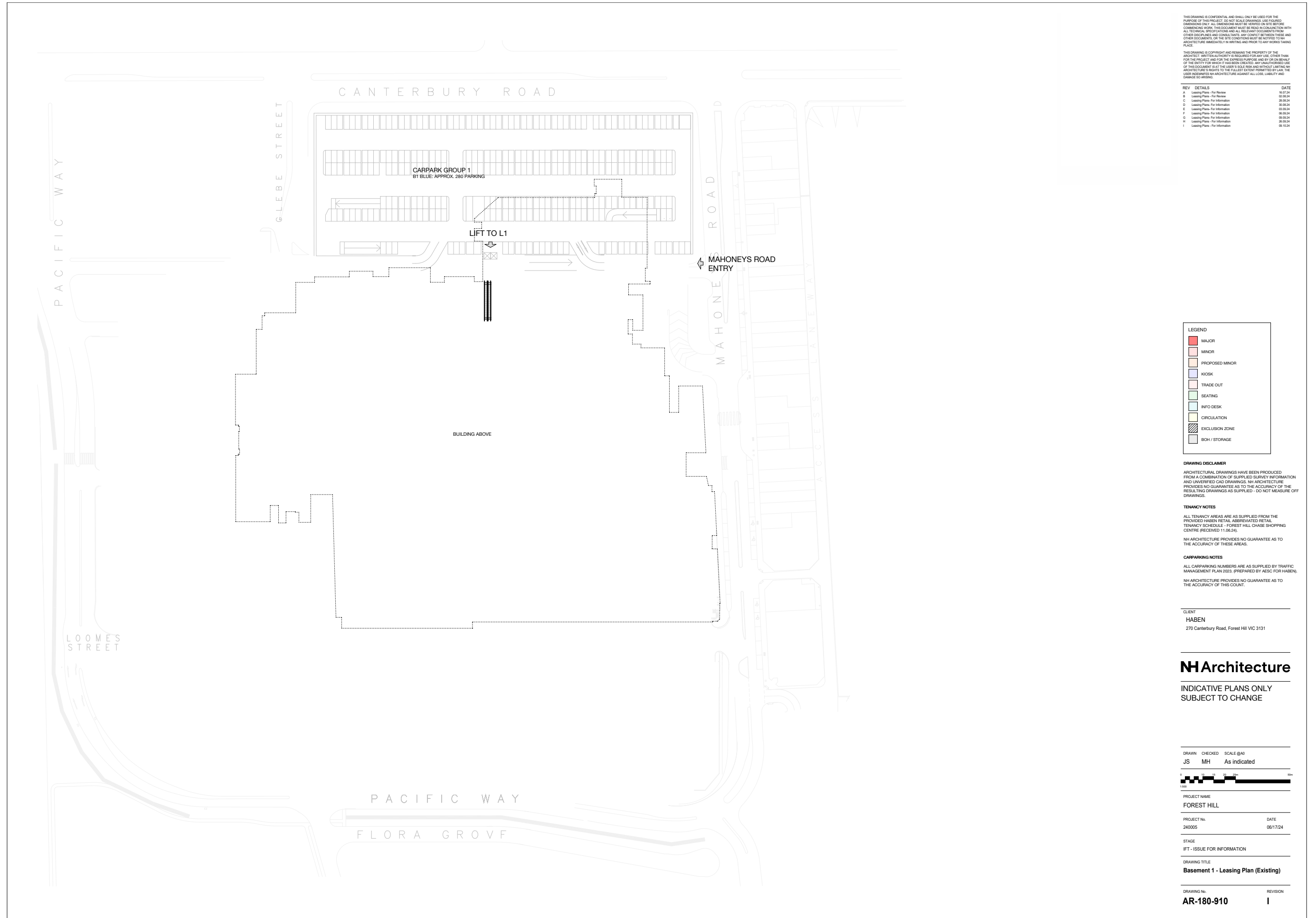
Appendix A

Existing Car Parking Plans

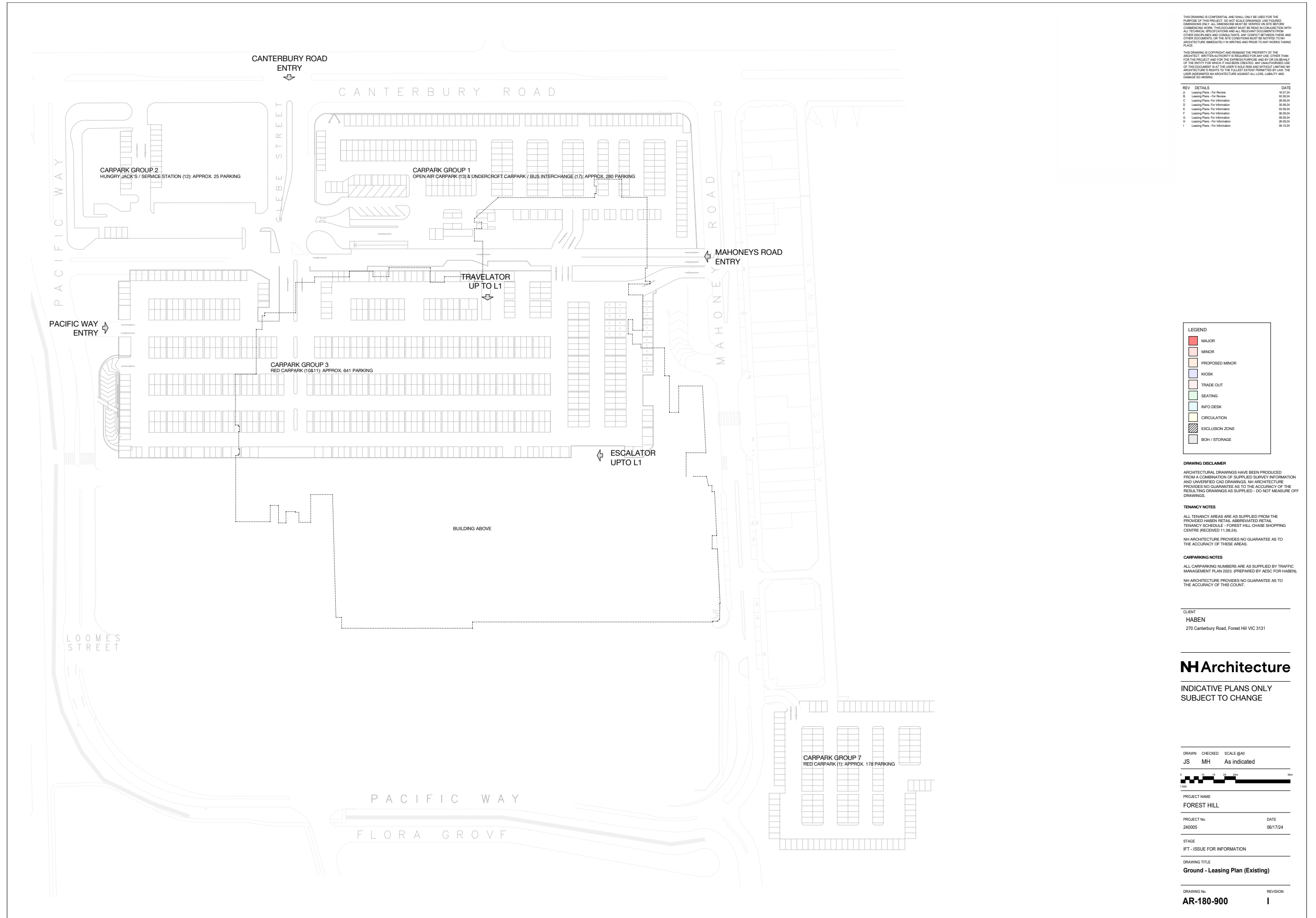
10.6 – ATTACHMENT 3. Traffic Report



10.6 – ATTACHMENT 3. Traffic Report



10.6 – ATTACHMENT 3. Traffic Report



THIS DRAWING IS CONFIDENTIAL AND SHALL ONLY BE USED FOR THE PURPOSE OF THIS PROJECT. DO NOT SCALE DRAWINGS. USE FIGURED DIMENSIONS ONLY. ALL DIMENSIONS MUST BE VERIFIED ON SITE BEFORE COMMENCING WORK. THIS DOCUMENT MUST BE READ IN CONJUNCTION WITH ALL TECHNICAL SPECIFICATIONS AND ALL RELEVANT DOCUMENTS FROM OTHER DISCIPLINES AND CONSULTANTS ANY CONFLICT BETWEEN THESE AND OTHER DOCUMENTS OR THE SITE CONDITIONS MUST BE NOTIFIED TO NH ARCHITECTURE IMMEDIATELY IN WRITING AND PRIOR TO ANY WORKS TAKING PLACE.

THIS DRAWING IS COPYRIGHT AND REMAINS THE PROPERTY OF THE ARCHITECT. WRITTEN AUTHORITY IS REQUIRED FOR ANY USE, OTHER THAN FOR THE PROJECT AND FOR THE EXPRESS PURPOSE AND BY OR ON BEHALF OF THE CLIENT FOR WHICH IT HAS BEEN CREATED. ANY UNAUTHORIZED USE OF THIS DOCUMENT IS AT THE USER'S SOLE RISK AND WITHOUT LIMITING NH ARCHITECTURE'S RIGHTS TO THE FULLEST EXTENT PERMITTED BY LAW, THE USER HEREBY RELEASES NH ARCHITECTURE AGAINST ALL LOSS, LIABILITY AND DAMAGE TO ARISING.

| REV | DETAILS | DATE |
|-----|---------------------------------|----------|
| A | Leasing Plans - For Review | 10/27/24 |
| B | Leasing Plans - For Review | 02/28/24 |
| C | Leasing Plans - For Information | 20/08/24 |
| D | Leasing Plans - For Information | 20/08/24 |
| E | Leasing Plans - For Information | 03/09/24 |
| F | Leasing Plans - For Information | 03/09/24 |
| G | Leasing Plans - For Information | 03/09/24 |
| H | Leasing Plans - For Information | 20/09/24 |
| I | Leasing Plans - For Information | 08/10/24 |

| LEGEND | |
|-------------------|----------------|
| [Red Box] | MAJOR |
| [Light Red Box] | MINOR |
| [Lighter Red Box] | PROPOSED MINOR |
| [Blue Box] | KIOSK |
| [Light Blue Box] | TRADE OUT |
| [Green Box] | SEATING |
| [Light Green Box] | INFO DESK |
| [Yellow Box] | CIRCULATION |
| [Hatched Box] | EXCLUSION ZONE |
| [Grey Box] | BOH / STORAGE |

DRAWING DISCLAIMER
 ARCHITECTURAL DRAWINGS HAVE BEEN PRODUCED FROM A COMBINATION OF SUPPLIED SURVEY INFORMATION AND UNVERIFIED CAD DRAWINGS. NH ARCHITECTURE PROVIDES NO GUARANTEE AS TO THE ACCURACY OF THE RESULTING DRAWINGS AS SUPPLIED. DO NOT MEASURE OFF DRAWINGS.

TENANCY NOTES
 ALL TENANCY AREAS ARE AS SUPPLIED FROM THE PROVIDED HABEN RETAIL ABBREVIATED RETAIL TENANCY SCHEDULE - FOREST HILL CHASE SHOPPING CENTRE (RECEIVED 11.06.24).
 NH ARCHITECTURE PROVIDES NO GUARANTEE AS TO THE ACCURACY OF THESE AREAS.

CARPARKING NOTES
 ALL CARPARKING NUMBERS ARE AS SUPPLIED BY TRAFFIC MANAGEMENT PLAN 2023. (PREPARED BY AESC FOR HABEN).
 NH ARCHITECTURE PROVIDES NO GUARANTEE AS TO THE ACCURACY OF THIS COUNT.

CLIENT
HABEN
 270 Canterbury Road, Forest Hill VIC 3131

NHArchitecture
 INDICATIVE PLANS ONLY
 SUBJECT TO CHANGE

DRAWN: JS CHECKED: MH SCALE: @A0
 As indicated



PROJECT NAME
FOREST HILL

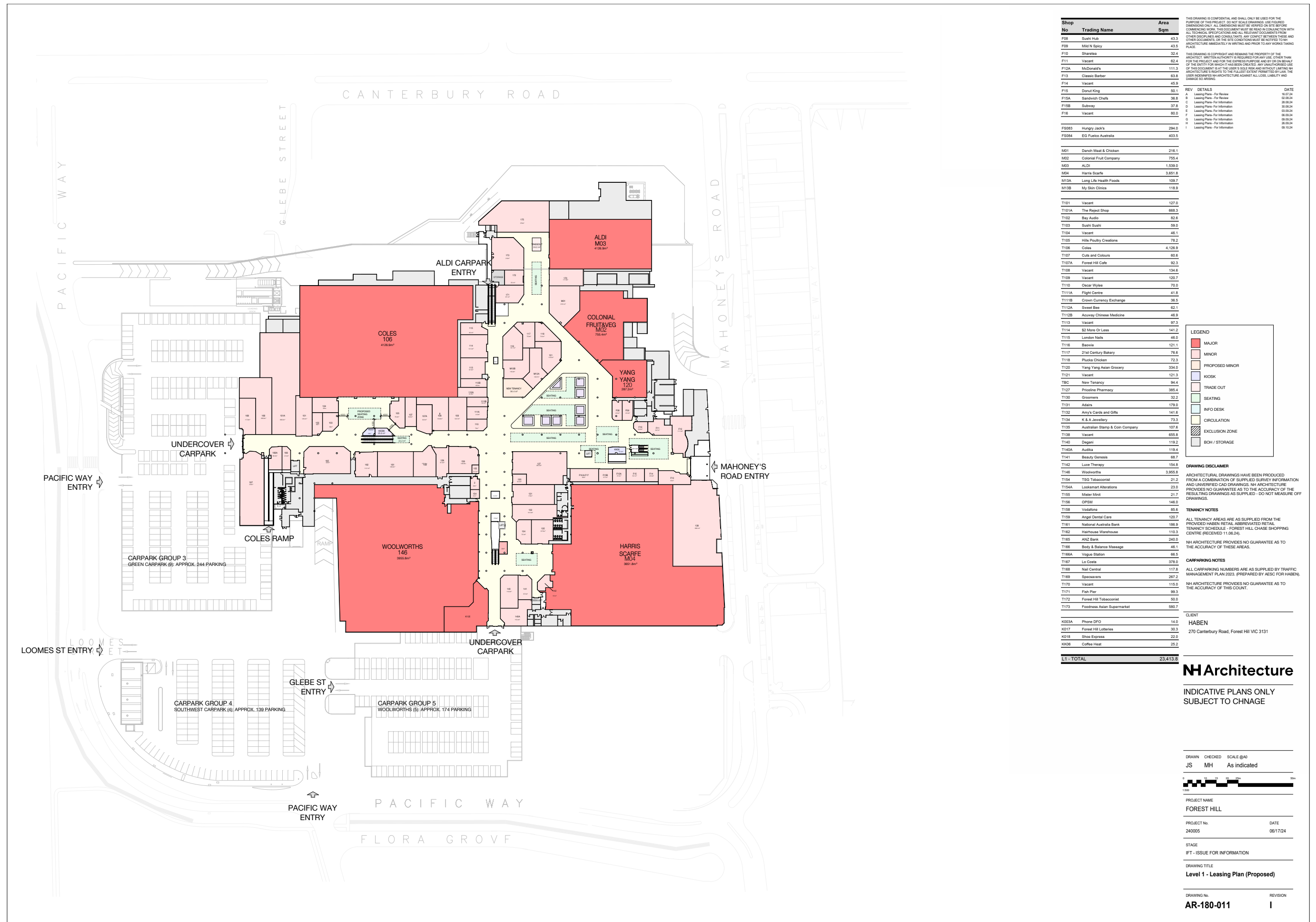
PROJECT No: 240005 DATE: 06/17/24

STAGE
 IFT - ISSUE FOR INFORMATION

DRAWING TITLE
Ground - Leasing Plan (Existing)

DRAWING No: **AR-180-900** REVISION: **I**

10.6 – ATTACHMENT 3. Traffic Report



| Shop No | Trading Name | Area Sqm |
|-------------------|---------------------------------|-----------------|
| F08 | Sushi Hub | 43.3 |
| F09 | Mild N Spicy | 43.5 |
| F10 | Sharetea | 32.4 |
| F11 | Vacant | 62.4 |
| F12A | McDonald's | 111.3 |
| F13 | Classic Barber | 63.6 |
| F14 | Vacant | 23.0 |
| F15 | David King | 50.1 |
| F15A | Sandwich Chefs | 36.8 |
| F15B | Subway | 37.8 |
| F16 | Vacant | 60.0 |
| F003 | Hungry Jack's | 294.0 |
| F004 | EG Fuelco Australia | 403.5 |
| M01 | Dench Meat & Chicken | 216.1 |
| M02 | Colonial Fruit Company | 755.4 |
| M03 | ALDI | 1,539.0 |
| M04 | Harris Scarfe | 3,651.8 |
| M13A | Long Life Health Foods | 109.7 |
| M13B | My Skin Clinics | 118.9 |
| T101 | Vacant | 127.0 |
| T101A | The Reject Shop | 668.3 |
| T102 | Bay Audio | 82.8 |
| T103 | Sushi Sushi | 99.0 |
| T104 | Vacant | 46.1 |
| T105 | Hills Poultry Creations | 78.2 |
| T106 | Coles | 4,128.9 |
| T107 | Cuts and Colours | 66.6 |
| T107A | Forest Hill Cafe | 92.3 |
| T109 | Vacant | 134.6 |
| T109 | Vacant | 120.7 |
| T110 | Ocean Wines | 70.6 |
| T111A | Flight Centre | 41.8 |
| T111B | Green Currency Exchange | 36.5 |
| T112A | Sweet Bee | 62.1 |
| T112B | Acunay Chinese Medicine | 48.9 |
| T113 | Vacant | 97.3 |
| T114 | \$2 More Or Less | 141.2 |
| T115 | London Nails | 46.0 |
| T116 | Bacole | 121.1 |
| T117 | 21st Century Bakery | 76.6 |
| T118 | Plucka Chicken | 72.3 |
| T120 | Yang Yang Asian Grocery | 334.0 |
| T121 | Vacant | 121.3 |
| T122 | New Tenancy | 84.4 |
| T127 | Phosline Pharmacy | 385.4 |
| T130 | Groomers | 32.2 |
| T131 | Adidas | 179.0 |
| T132 | Amy's Cards and Gifts | 141.8 |
| T134 | K & A Jewellery | 73.3 |
| T135 | Australian Stamp & Coin Company | 107.6 |
| T138 | Vacant | 658.8 |
| T140 | Degani | 119.2 |
| T140A | Audika | 119.4 |
| T141 | Beauty Genesis | 68.7 |
| T142 | Luxe Therapy | 154.8 |
| T146 | Woolworths | 3,925.9 |
| T154 | TSD Telecomart | 21.2 |
| T154A | Locksmart Alterations | 23.0 |
| T155 | Mister Mint | 21.7 |
| T156 | OPSM | 146.0 |
| T156 | Vodafone | 85.6 |
| T159 | Angel Dental Care | 120.7 |
| T161 | National Australia Bank | 186.9 |
| T162 | Hairhouse Warehouse | 113.3 |
| T165 | ANZ Bank | 240.0 |
| T166 | Body & Balance Massage | 46.1 |
| T166A | Vogue Station | 66.5 |
| T167 | Lo Costa | 379.0 |
| T168 | Nail Central | 117.8 |
| T169 | Speakeasy | 267.7 |
| T170 | Vacant | 115.0 |
| T171 | Fish Pier | 96.3 |
| T172 | Forest Hill Telecomart | 60.0 |
| T173 | Foodies Asian Supermarket | 580.7 |
| L1 - TOTAL | | 23,413.8 |

| Shop No | Trading Name | Area Sqm |
|---------|-----------------------|----------|
| K003A | Phone DFO | 14.0 |
| K017 | Forest Hill Lotteries | 30.3 |
| K018 | Shoe Express | 22.0 |
| K008 | Coffee Heat | 25.2 |

| REV | DETAILS | DATE |
|-----|---------------------------------|----------|
| A | Leasing Plans - For Review | 10/27/24 |
| B | Leasing Plans - For Review | 05/28/24 |
| C | Leasing Plans - For Information | 20/08/24 |
| D | Leasing Plans - For Information | 20/08/24 |
| E | Leasing Plans - For Information | 08/09/24 |
| F | Leasing Plans - For Information | 08/09/24 |
| G | Leasing Plans - For Information | 08/09/24 |
| H | Leasing Plans - For Information | 20/09/24 |
| I | Leasing Plans - For Information | 08/10/24 |

LEGEND

- MAJOR
- MINOR
- PROPOSED MINOR
- KIOSK
- TRADE OUT
- SEATING
- INFO DESK
- CIRCULATION
- EXCLUSION ZONE
- BOH / STORAGE

DRAWING DISCLAIMER

ARCHITECTURAL DRAWINGS HAVE BEEN PRODUCED FROM A COMBINATION OF SUPPLIED SURVEY INFORMATION AND UNVERIFIED CAD DRAWINGS. NH ARCHITECTURE PROVIDES NO GUARANTEE AS TO THE ACCURACY OF THE RESULTING DRAWINGS AS SUPPLIED - DO NOT MEASURE OFF DRAWINGS.

TENANCY NOTES

ALL TENANCY AREAS ARE AS SUPPLIED FROM THE PROVIDED HABEN RETAIL ABBREVIATED RETAIL TENANCY SCHEDULE - FOREST HILL CHASE SHOPPING CENTRE (RECEIVED 11.06.24).

NH ARCHITECTURE PROVIDES NO GUARANTEE AS TO THE ACCURACY OF THESE AREAS.

CARPARK NOTES

ALL CARPARKING NUMBERS ARE AS SUPPLIED BY TRAFFIC MANAGEMENT PLAN 2023. (PREPARED BY ASC FOR HABEN).

NH ARCHITECTURE PROVIDES NO GUARANTEE AS TO THE ACCURACY OF THIS COUNT.

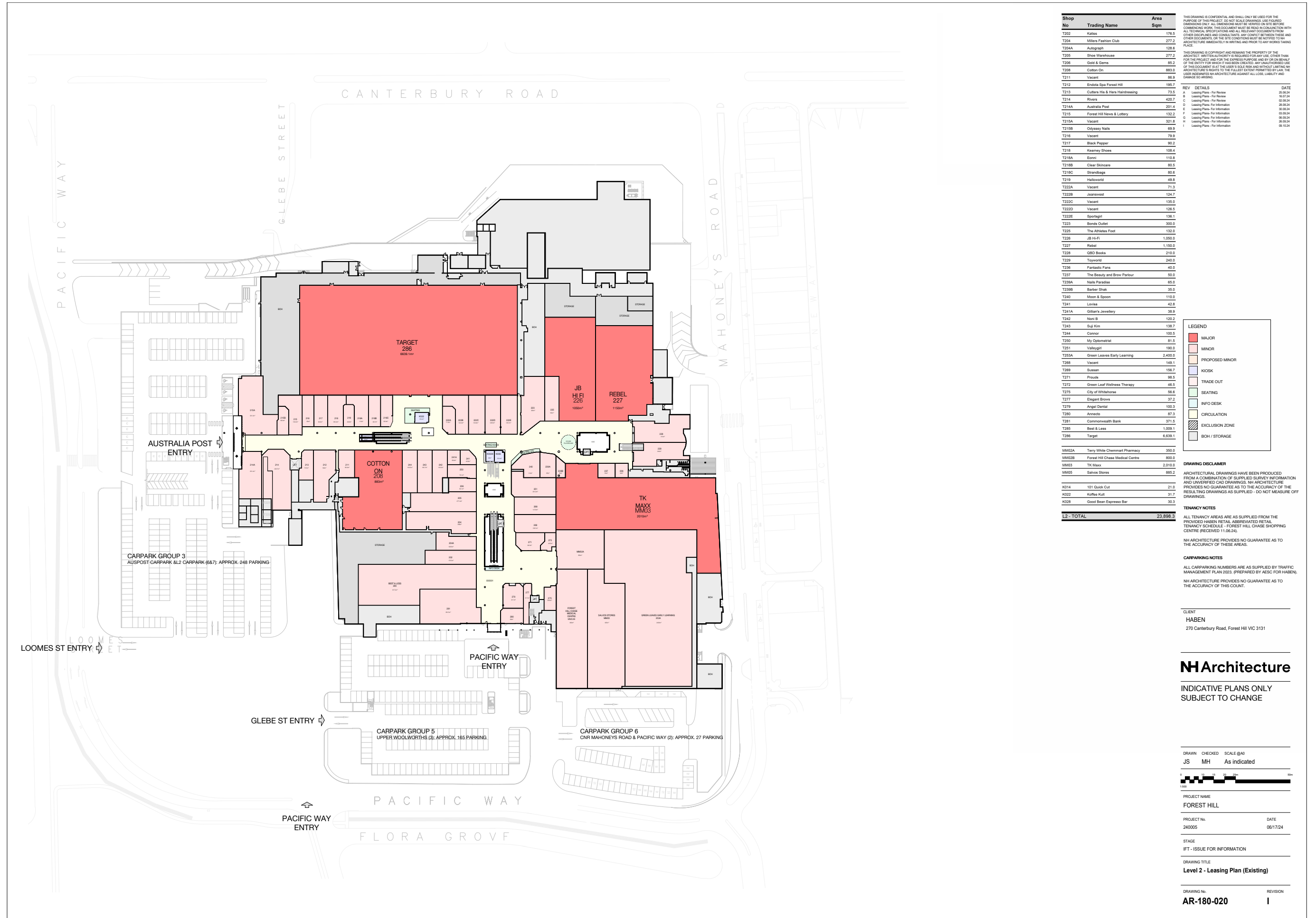
CLIENT

HABEN
270 Canterbury Road, Forest Hill VIC 3131

NH Architecture
INDICATIVE PLANS ONLY
SUBJECT TO CHNAGE

| | | |
|-----------------------------------|----------|--------------|
| DRAWN | CHECKED | SCALE @A0 |
| JS | MH | As indicated |
| | | |
| PROJECT NAME | | |
| FOREST HILL | | |
| PROJECT No. | DATE | |
| 240005 | 06/17/24 | |
| STAGE | | |
| IFT - ISSUE FOR INFORMATION | | |
| DRAWING TITLE | | |
| Level 1 - Leasing Plan (Proposed) | | |
| DRAWING No. | REVISION | |
| AR-180-011 | I | |

10.6 – ATTACHMENT 3. Traffic Report



| Shop No | Trading Name | Area Sqm |
|-------------------|----------------------------------|-----------------|
| T202 | Kalbarri | 178.9 |
| T204 | Millem Fashion Club | 277.2 |
| T204A | Autograph | 128.8 |
| T205 | Shoe Warehouse | 277.2 |
| T206 | Gold & Gems | 85.2 |
| T208 | Cotton On | 883.0 |
| T211 | Vacant | 86.8 |
| T212 | Endota Spa Forest Hill | 195.7 |
| T213 | Cutlers Hair & Hair Dressing | 73.5 |
| T214 | Rivers | 420.7 |
| T214A | Australia Post | 201.4 |
| T215 | Forest Hill News & Lottery | 132.2 |
| T215A | Vacant | 321.8 |
| T215B | Odyssey Halls | 83.8 |
| T216 | Vacant | 79.9 |
| T217 | Black Pepper | 90.2 |
| T218 | Kearney Shoes | 108.4 |
| T218A | Eonni | 118.8 |
| T218B | Clear Skincare | 80.5 |
| T218C | Stranbags | 80.6 |
| T218 | Halloworld | 49.8 |
| T222A | Vacant | 71.3 |
| T222B | Japinewest | 124.7 |
| T222C | Vacant | 135.0 |
| T222D | Vacant | 128.5 |
| T222E | Spotlight | 136.1 |
| T223 | Bonds Outlet | 300.0 |
| T225 | The Athletics Foot | 132.0 |
| T226 | JB Hi-Fi | 1,056.0 |
| T227 | Rebel | 1,158.0 |
| T228 | QBC Books | 210.0 |
| T229 | Toyworld | 240.0 |
| T230 | Fantasia Flats | 42.0 |
| T237 | The Beauty and Brew Parlour | 152.8 |
| T238A | Nails Paradise | 65.0 |
| T238B | Barber Shak | 35.0 |
| T240 | Moon & Spoon | 110.0 |
| T241 | Lovisa | 42.8 |
| T241A | Gillar's Jewellery | 38.9 |
| T242 | Nori B | 120.2 |
| T243 | Bug Kim | 138.7 |
| T244 | Connor | 100.0 |
| T250 | My Optometrist | 81.5 |
| T251 | Volleyball | 150.0 |
| T253A | Green Leaves Early Learning | 2,400.0 |
| T266 | Vacant | 148.1 |
| T269 | Susann | 156.7 |
| T271 | Prudis | 98.5 |
| T272 | Green Leaf Wellness Therapy | 46.5 |
| T275 | City of Whitehorse | 56.6 |
| T277 | Elegant Brows | 37.2 |
| T279 | Angel Dental | 100.3 |
| T280 | Annetto | 87.3 |
| T281 | Commonwealth Bank | 371.5 |
| T285 | Best & Less | 1,008.1 |
| T286 | Target | 6,639.1 |
| MM02A | Tony White Chemmart Pharmacy | 350.0 |
| MM02B | Forest Hill Chase Medical Centre | 800.0 |
| MM03 | TK Maxx | 2,010.0 |
| MM05 | Salvos Stores | 885.2 |
| X014 | 101 Quik Cut | 21.0 |
| X022 | Koffee Kult | 31.7 |
| X028 | Good Bean Espresso Bar | 35.3 |
| L2 - TOTAL | | 23,898.3 |

THIS DRAWING IS CONFIDENTIAL AND SHALL ONLY BE USED FOR THE PURPOSE OF THIS PROJECT. DO NOT SCALE DRAWINGS. USE FIGURED DIMENSIONS ONLY. ALL DIMENSIONS MUST BE APPROVED ON SITE BEFORE COMMENCING WORK. THIS DOCUMENT MUST BE READ IN CONJUNCTION WITH ALL TECHNICAL SPECIFICATIONS AND ALL RELEVANT DOCUMENTS FROM OTHER DISCIPLINES AND CONSULTANTS ANY CONFLICT BETWEEN THESE AND OTHER DOCUMENTS, OR THE SITE CONDITIONS MUST BE NOTIFIED TO NH ARCHITECTURE IMMEDIATELY IN WRITING AND PRIOR TO ANY WORKS TAKING PLACE.

THIS DRAWING IS COPYRIGHT AND REMAINS THE PROPERTY OF THE ARCHITECT. WRITTEN AUTHORITY IS REQUIRED FOR ANY USE, OTHER THAN FOR THE PROJECT AND FOR THE EXPRESS PURPOSE AND BY OR ON BEHALF OF THE CLIENT FOR WHICH IT HAS BEEN CREATED. ANY UNAUTHORIZED USE OF THIS DOCUMENT IS AT THE USER'S SOLE RISK AND WITHOUT LIMITING NH ARCHITECTURE'S LIABILITY TO THE FULLEST EXTENT PERMITTED BY LAW, THE USER ACCEPTS RESPONSIBILITY FOR ANY LOSS, LIABILITY AND DAMAGE TO ARISING.

| REV | DETAILS | DATE |
|-----|---------------------------------|----------|
| A | Leasing Plans - For Review | 20.08.24 |
| B | Leasing Plans - For Review | 05.07.24 |
| C | Leasing Plans - For Review | 02.08.24 |
| D | Leasing Plans - For Information | 28.08.24 |
| E | Leasing Plans - For Information | 30.08.24 |
| F | Leasing Plans - For Information | 03.09.24 |
| G | Leasing Plans - For Information | 06.09.24 |
| H | Leasing Plans - For Information | 20.09.24 |
| I | Leasing Plans - For Information | 08.10.24 |

LEGEND

- MAJOR
- MINOR
- PROPOSED MINOR
- KIOSK
- TRADE OUT
- SEATING
- INFO DESK
- CIRCULATION
- EXCLUSION ZONE
- BOH / STORAGE

DRAWING DISCLAIMER

ARCHITECTURAL DRAWINGS HAVE BEEN PRODUCED FROM A COMBINATION OF SUPPLIED SURVEY INFORMATION AND UNVERIFIED CAD DRAWINGS. NH ARCHITECTURE PROVIDES NO GUARANTEE AS TO THE ACCURACY OF THE RESULTING DRAWINGS AS SUPPLIED - DO NOT MEASURE OFF DRAWINGS.

TENANCY NOTES

ALL TENANCY AREAS ARE AS SUPPLIED FROM THE PROVIDED HABEN RETAIL ABBREVIATED RETAIL TENANCY SCHEDULE - FOREST HILL CHASE SHOPPING CENTRE RECEIVED 11.06.24.

NH ARCHITECTURE PROVIDES NO GUARANTEE AS TO THE ACCURACY OF THESE AREAS.

CARPARKING NOTES

ALL CARPARKING NUMBERS ARE AS SUPPLIED BY TRAFFIC MANAGEMENT PLAN 0003, PREPARED BY ASC FOR HABEN.

NH ARCHITECTURE PROVIDES NO GUARANTEE AS TO THE ACCURACY OF THIS COUNT.

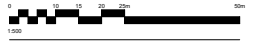
CLIENT

HABEN
270 Canterbury Road, Forest Hill VIC 3131

NH Architecture

INDICATIVE PLANS ONLY
SUBJECT TO CHANGE

DRAWN: JS CHECKED: MH SCALE: @A0
As indicated



PROJECT NAME
FOREST HILL

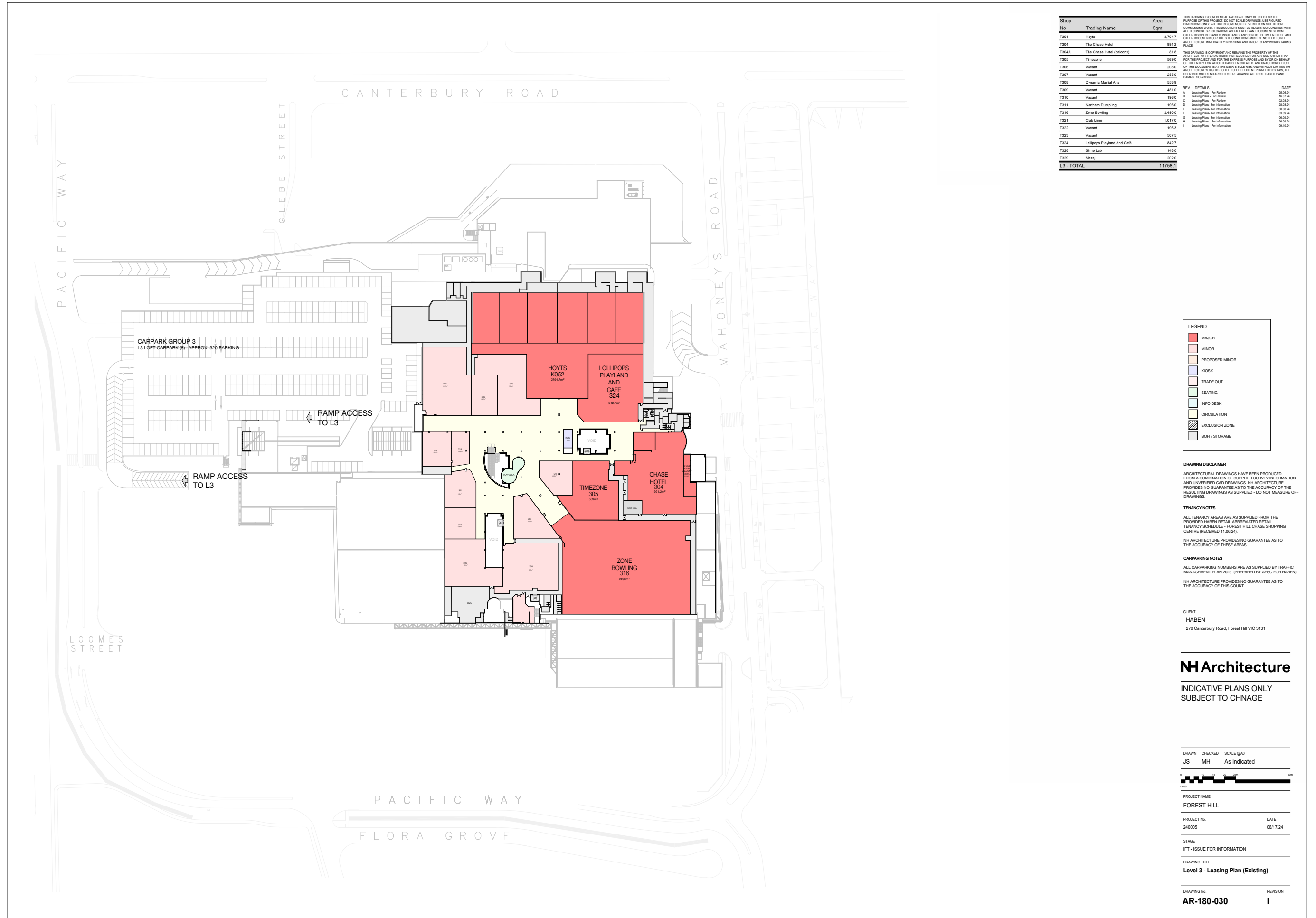
PROJECT No. 240005 **DATE** 06/17/24

STAGE
IFT - ISSUE FOR INFORMATION

DRAWING TITLE
Level 2 - Leasing Plan (Existing)

DRAWING No. AR-180-020 **REVISION** I

10.6 – ATTACHMENT 3. Traffic Report



10.6 – ATTACHMENT 3. Traffic Report

eukai

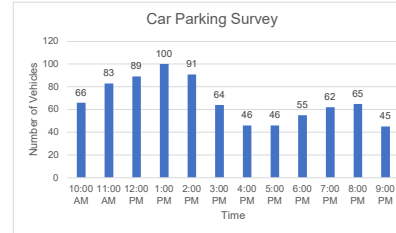
Appendix B

Car Parking Survey Data

10.6 – ATTACHMENT 3. Traffic Report



Job: Mahoneys Road - Car Parking Survey
 Date: Friday, 23 August 2024
 Survey Period: 10am to 9pm

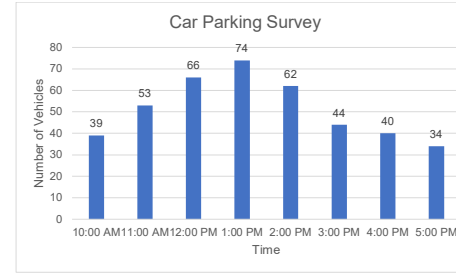


| Location | Side | Between | Restriction | Time | Supply | 10:00 AM | 11:00 AM | 12:00 PM | 1:00 PM | 2:00 PM | 3:00 PM | 4:00 PM | 5:00 PM | 6:00 PM | 7:00 PM | 8:00 PM | 9:00 PM | Average | | |
|--------------------------------|------|--|--|-----------------|------------|-----------|-----------|-----------|------------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|----|---|
| Mahoneys Road | East | Between Canterbury Road and Pedestrian Crossing Signal | 1/2 P | 9am-9pm Mon-Sat | 5 | 2 | 2 | 3 | 4 | | 2 | 1 | 2 | 3 | 4 | 5 | 5 | 3 | | |
| | | | Loading Zone | 9am-9pm Mon-Sat | 1 | | 1 | | 1 | | | | | | | | 1 | 1 | 1 | |
| | | | 1/4 P | 9am-9pm Mon-Sat | 5 | | 4 | 5 | 4 | 5 | 4 | | 2 | 3 | 4 | 5 | 5 | 5 | 5 | |
| | | | 1P Disabled Only | 9am-9pm Mon-Sat | 2 | 1 | 1 | 2 | 2 | | 1 | 1 | 1 | 1 | 1 | | 1 | 2 | 1 | |
| | | Between Pedestrian Crossing Signal and Lane Way | Loading Zone | 9am-9pm Mon-Sat | 1 | | 1 | 1 | | 1 | | | | | | 1 | 1 | 1 | | 1 |
| | | | 1/2 P | 9am-9pm Mon-Sat | 5 | 2 | 2 | 3 | 5 | 3 | 3 | 3 | 3 | 3 | 4 | 4 | 5 | 4 | 4 | 3 |
| | | | 1/2 P Disabled Only | 9am-9pm Mon-Sat | 2 | | 1 | 1 | | 1 | 1 | | | | 1 | 1 | | | | 1 |
| | | | 1/2 P | 9am-9pm Mon-Sat | 3 | | 1 | 1 | 1 | 3 | 1 | 1 | 1 | 1 | 1 | 2 | 1 | 1 | 1 | 1 |
| | West | Between Pacific Way and Bus Stop (113 Mahoneys Road) | Unrestricted | | | 13 | | | | | | | | | | | | | 0 | |
| | | | 1/4 P | 9am-9pm Mon-Sat | 6 | 5 | 2 | 5 | 4 | 2 | 2 | 2 | 2 | 2 | 5 | 5 | 4 | 4 | 4 | |
| Taxi Zone | | | | 6 | 5 | 5 | 3 | 5 | 2 | 3 | | | 1 | 2 | 1 | | 1 | 3 | | |
| No Standing | | | 9am-9pm Mon-Fri; 9am-1pm Sat | 9 | | | | | | | | | | | | | | | 0 | |
| Mahoneys Road Loading Zone | West | Between Canterbury Road and Pacific Way | Loading Zone 1 | | | 4 | 4 | 5 | 5 | 6 | 1 | 1 | 1 | 1 | 1 | 1 | 1 | 3 | | |
| | | | Target/JbHIFI Customer Pickup | | | 1 | 1 | 2 | 2 | 1 | | | | | | | | | 1 | |
| | | | Coles Loading Dock | | 1 | | | 1 | | | | | | | | | | | | 1 |
| | | | Harris Scarfe Loading Dock Mon-Fri 7am-3pm | | | | | | 1 | | | | | | | | | | | 1 |
| Car Park East of Mahoneys Road | | | Dialysis Parking | | 20 | 3 | 3 | 3 | 9 | 11 | 9 | 7 | 7 | 7 | 11 | 13 | 3 | 7 | | |
| | | | Unrestricted | | 157 | 43 | 55 | 56 | 57 | 54 | 36 | 28 | 25 | 25 | 27 | 28 | 18 | 18 | 38 | |
| Total | | | | | 235 | 66 | 83 | 89 | 100 | 91 | 64 | 46 | 46 | 55 | 62 | 65 | 45 | 68 | | |

10.6 – ATTACHMENT 3. Traffic Report



Job: Mahoneys Road - Car Parking Survey
 Date: Saturday, 24 August 2024
 Survey Period: 10am to 5pm



| Location | Side | Between | Restriction | Time | Supply | 10:00 AM | 11:00 AM | 12:00 PM | 1:00 PM | 2:00 PM | 3:00 PM | 4:00 PM | 5:00 PM | Average | |
|---|--|--|--|-----------------|------------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|---|
| Mahoneys Road | East | Between Canterbury Road and Pedestrian Crossing Signal | 1/2 P | 9am-9pm Mon-Sat | 5 | 2 | 3 | 5 | 3 | 2 | 2 | 1 | 1 | 2 | |
| | | | Loading Zone | 9am-9pm Mon-Sat | 1 | | 1 | | | | | | 1 | 1 | |
| | | | 1/4 P | 9am-9pm Mon-Sat | 5 | 3 | 5 | 5 | 5 | 5 | 1 | 1 | 2 | 3 | |
| | | | 1P Disabled Only | 9am-9pm Mon-Sat | 2 | | 2 | 1 | 2 | 2 | 2 | | | | 2 |
| | | Between Pedestrian Crossing Signal and Lane Way | Loading Zone | 9am-9pm Mon-Sat | 1 | | 1 | | | | | | | | 1 |
| | | | 1/2 P | 9am-9pm Mon-Sat | 5 | 5 | 5 | 5 | 5 | 5 | 2 | 1 | 1 | 4 | |
| | | Between Lane Way and Pacific Way | 1/2 P Disabled Only | 9am-9pm Mon-Sat | 2 | | | 1 | 1 | | | | | | 1 |
| | | | 1/2 P | 9am-9pm Mon-Sat | 3 | | 2 | 1 | 1 | 2 | 1 | 1 | 1 | 1 | 1 |
| | Between Pacific Way and Bus Stop (113 Mahoneys Road) | Unrestricted | | 13 | | | | | | | | | | 0 | |
| | West | Between Canterbury Road and Pedestrian Crossing Signal | 1/4 P | 9am-9pm Mon-Sat | 6 | 2 | 3 | 4 | 6 | 6 | 2 | 5 | 4 | 4 | |
| Between Pedestrian Crossing Signal and Pacific Way | | Taxi Zone | | 6 | 1 | 5 | 5 | 4 | 2 | 4 | 2 | 2 | 3 | | |
| Between Pacific Way and Bus Stop (76 Mahoneys Road) | | No Standing | 9am-9pm Mon-Fri; 9am-1pm Sat | 9 | | | | | | | | | | 0 | |
| Mahoneys Road Loading Zone | West | Between Canterbury Road and Pacific Way | Loading Zone 1 | | | 2 | 1 | 4 | 4 | 3 | 2 | 3 | 2 | 3 | |
| | | | Target/lbHIFI Customer Pickup | | | 1 | | | | 1 | | | | 1 | |
| | | | Coles Loading Dock | | | | | | | | | | | | 0 |
| | | | Harris Scarfe Loading Dock Mon-Fri 7am-3pm | | | | | | | | | 1 | 1 | 1 | |
| Car Park East of Mahoneys Road | | | Dialysis Parking | | 20 | 4 | 3 | 4 | 1 | 2 | 1 | 3 | 3 | 3 | |
| | | | Unrestricted | | 157 | 19 | 22 | 31 | 42 | 33 | 26 | 22 | 16 | 26 | |
| Total | | | | | 235 | 39 | 53 | 66 | 74 | 62 | 44 | 40 | 34 | 52 | |

10.6 – ATTACHMENT 3. Traffic Report



Appendix C

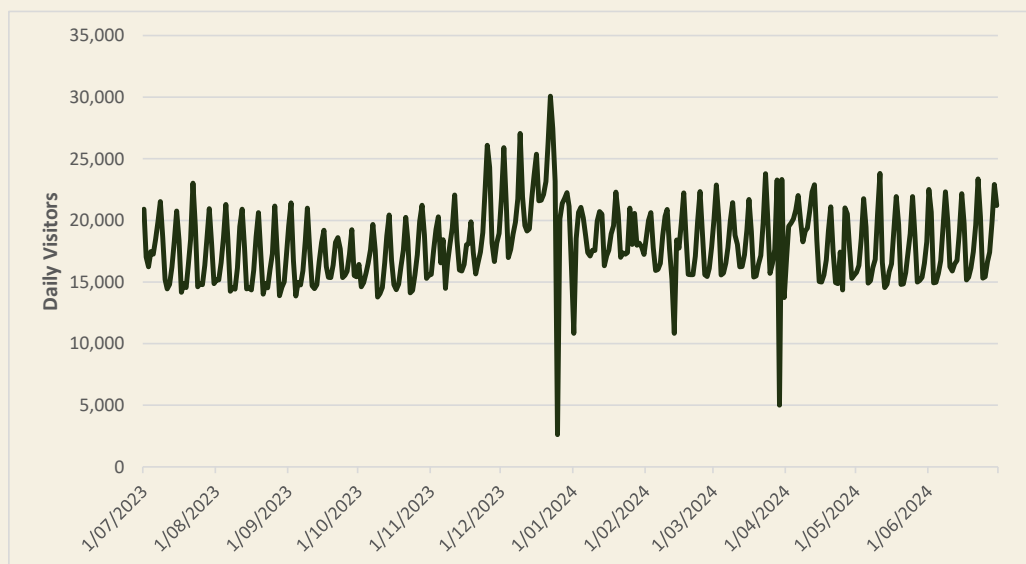
Shopping Centre Door Count Data

Door count data for the Centre for the 2023/2024 financial year has been sourced from Centre Management (the data captures the daily number of customers and staff to the Centre). The data indicates the following average daily demands:

- Monday: 15,424
- Tuesday: 15,927
- Wednesday: 16,674
- Thursday: 18,035
- Friday: 19,515 (and an 85th percentile demand of 21,269)
- Saturday: 21,728 (and an 85th percentile demand of 23,324)
- Sunday: 18,515

From the above data, it is evident that Saturdays are typically the busiest day of the week and that Fridays are the busiest weekday at the Centre. A summary of the daily door count data across the year is presented in Figure A1. Consistent with most shopping centres, the data indicates that November and December are typically the busiest months of the year at the Centre.

Figure A1: Forest Hill Shopping Centre Door Count Data – FY2023/24



10.6 – ATTACHMENT 3. Traffic Report

It is noted that on the day of the car parking surveys (referred to earlier in this report) the following door count demands were recorded:

- Friday: 18,606
- Saturday: 19,101

As identified above, visitation to the Centre fluctuates throughout the year. It is observed that the winter months (including August when the car parking surveys were undertaken) typically have lower visitation compared to the other months and in particular November and December when demands typically peak.

A summary of the door count demands for the surveyed days and the 85th percentile day are presented in Table C1. The necessary factor to multiply the surveyed day to the 85th percentile day has been identified in the Table as well.

Table C1: Car Parking Demand Surveys – Factored to Represent 85th Percentile Day

| Day | Door Count Data | | Factor |
|----------|-----------------|-----------------------------|--------|
| | Surveyed Day | 85 th Percentile | |
| Friday | 18,606 | 21,269 | 1.13 |
| Saturday | 19,101 | 23,324 | 1.22 |

The surveyed car parking demand data has been factored by the above figures to determine the 85th percentile car parking demands for the Friday and Saturday survey periods. The data presented in Section 2 of this report represents the factored 85th percentile car parking demand data.

10.6 – ATTACHMENT 3. Traffic Report



eukai

© 2025 Eukai Pty Ltd.

www.eukai.com.au

10.7 **Records of Informal Meetings of Councillors**

Attachment 1 Pre Council Meeting Briefing - 20 April 2026

Attachment 2 Strategy Meeting - 27 April 2026

Attachment 3 Strategy Meeting - 4 May 2026

10.7 – ATTACHMENT 1. Pre Council Meeting Briefing - 20 April 2026



Record of Informal Councillor Meeting

Form HPCM 21/183353

| Pre Council Meeting Briefing – 20 April 2026 – 6.30pm – 6.42pm | | |
|--|-----------------------------------|-----------------------|
| Matter/s Discussed: | Councillors Present | Officers Present |
| <ul style="list-style-type: none"> 20 April 2026 Council Meeting Agenda | Cr Kirsten Langford Mayor | S McMillan |
| | Cr Kieran Simpson Deputy Mayor | L Letic S Sullivan |
| | Cr Peter Allan | J Green |
| | Cr Blair Barker | S Cann |
| | Cr Prue Cutts | S White |
| | Cr Andrew Davenport | V Ferlaino |
| | Cr Daniel Griffiths | A Wintle |
| | Cr Jarrod Gunn | J Moore |
| | Cr Jason Martin | K Marriott |
| | Cr Ben Stennett | C Clarke |
| | Cr Hayley Weller | |
| Others Present: Nil | | |
| Disclosures of Conflict of Interest: Nil | | |
| Councillor /Officer attendance following disclosure: Nil | | |



10.7 – ATTACHMENT 2. Strategy Meeting - 27 April 2026



Record of Informal Councillor Meeting

Form HPCM 21/183353

| Strategy Meeting – 27 April 2026: 6.30pm – 9.15pm | | |
|--|---|---|
| Matter/s Discussed: | Councillors Present | Officers Present |
| <ul style="list-style-type: none"> Library Future State Updates Strategic Property Program & Outcomes of Community Engagement (property sales) Draft Tree Management Policy Community Engagement (adoption) Draft Council Meeting Agenda | Cr Kirsten Langford Mayor Cr Kieran Simpson Deputy Mayor Cr Peter Allan Cr Blair Barker Cr Andrew Davenport Cr Jarrod Gunn Cr Jason Martin Cr Ben Stennett Cr Hayley Weller | S McMillan K Marriott L Letic J Green S White S Sullivan V Ferlaino S Candeland A Ghastine S Day |
| Others Present: Nil | | |
| Disclosures of Conflict of Interest: Nil | | |
| Councillor /Officer attendance following disclosure: N/A | | |
| Apologies: Cr Prue Cutts Absent: Cr Daniel Griffiths | | |

10.7 – ATTACHMENT 3. Strategy Meeting - 4 May 2026



WHITEHORSE
CITY COUNCIL

Record of Informal Councillor Meeting

Form HPCM 21/183353

| Strategy Meeting – 4 May 2026: 6.30pm – 9.30pm | | |
|--|--|---|
| Matter/s Discussed: | Councillors Present | Officers Present |
| <ul style="list-style-type: none"> • Eastern Region Group of Councils (ERG) Overview & Updates/ Video Booking • Open Space Strategy Implementation Update • Development of Council's Corporate Emission Reduction Plan • Directorate Update - City Development • Draft Council Meeting Agenda | Cr Kirsten Langford Mayor Cr Kieran Simpson Deputy Mayor Cr Peter Allan Cr Blair Barker Cr Prue Cutts Cr Andrew Davenport Cr Daniel Griffiths Cr Jarrod Gunn Cr Jason Martin Cr Hayley Weller | S McMillan K Marriott S Cann J Green S White S Sullivan V Ferlaino S Candeland S Kinsey T Gledhill I Wang A Egan C Clarke M Kagawa |
| Others Present: Liz Johnstone, Executive Officer - Eastern Region Group of Councils (virtual) | | |
| Apologies: Cr Ben Stennett | | |
| Disclosures of Conflict of Interest: | | |
| Name: | Conflict Details: | |
| Mayor Cr Kirsten Langford | Material Conflict in item 9.1 of the Draft Council Agenda | |
| | | |
| Councillor /Officer attendance following disclosure: | | |
| Cr Langford left the room at 8.54pm during discussion on Item 9.1 of the Draft Council Agenda and returned at 8.58pm. | | |